

CATEGORICAL EXCLUSION LEVEL 1 FORM

GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 63 and County Road (CR) 1650 N / Vermillion County
Designation Number(s):	1700098
Project Description/Termini:	Reduced Conflict Intersection Project (installation of median U-Turn), located approximately 600 feet (ft.) (0.1 mile) north of Interstate (I)-74. The project termini will extend along SR 63 from 1,400 ft. south of eastbound I-74 ramp to 1,650 ft. north of westbound I-74 ramp.

CE Level 1 documentation for exempted projects

Additional Information to CE Level 1

Approval:

INDOT DE/ESD Signature and Date

Release for Public Involvement:

RZK September 12, 2022

INDOT DE/ESD Initials and Date

Certification of Public involvement:

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer:

Signature and Date

CE Preparer:

Susan Castle, Metric Environmental, LLC

Name and Organization

Indiana Department of Transportation

County Vermillion Route SR 63 and CR 1650 N Des. No. 1700098

GENERAL PROJECT INFORMATION, DESCRIPTION, AND DESIGN INFORMATION	
Purpose and Need:	<p>Need: The need for this project is a result of the crash history on SR 63 through the project area. Based on the crash data collected by INDOT from 2015 through 2018, this segment of SR 63 had a total of eighteen crashes, including a fatality. The existing SR 63 left-turn sight distance restrictions and low compliance with the posted speed limit of 45 miles per hour, result in frequent right-angle, failure to yield crashes (Appendix G, pages G-1 to G-6).</p> <p>Purpose: The purpose of this project is to reduce the crash rate on this segment of SR 63 by an alternative traffic operation for SR 63 that is efficient for trucks and that will result in high driver compliance.</p>
Project Description (Preferred Alternative):	<p>The Indiana Department of Transportation (INDOT) and the Federal Highway Administration (FHWA) intend to proceed with an intersection improvement project.</p> <p>Location The project is located on SR 63, approximately 0.1 mile north of I-74 in Highland Township, Vermillion County, Indiana (Appendix B, page B-1). Specifically, the project is located in Sections 4 and 5, Township 19 North, Range 9 West on the Perrysville, Indiana 7.5-minute United States Geological Survey (USGS) topographical map (Appendix B, page B-2).</p> <p>Existing Conditions SR 63 is classified as a Principal Arterial. The posted speed limit is 45 miles per hour (mph) within the project limits. CR 1650 N is a local road. The posted speed limit is 30 mph.</p> <p>SR 63 and CR 1650 N is a two-way stop-controlled intersection, with SR 63 operating under free-flowing conditions. Northbound SR 63 consists of two 12 ft. wide through lanes and one 12 ft. wide left-turn lane. Southbound SR 63 consists of two 12 ft. wide through lanes, one 12 ft. left-turn lane, and one 12 ft. wide right-turn lane. Eastbound CR 1650 N consists of one shared left, through and right-turn lane and one receiving lane. Westbound CR 1650 N consists of one right-turn lane and one receiving lane.</p> <p>SR 63 and north gas station drive is a tee intersection between the stop-controlled private driveway approach (which serves gas stations and drive-thru restaurants) and SR 63 is free-flowing. Northbound SR 63 consists of two 12 ft. wide through lanes and one 12 ft. wide right-turn lane. Southbound SR 63 consists of two 12 ft. wide through lanes. Westbound north gas station drive consists of one wide shared left and right-turn lane and one receiving lane.</p> <p>SR 63 and unnamed drive is a tee intersection between the stop-controlled private driveway approach (which serves two single-family residences) and SR 63 is free flowing. Northbound and southbound SR 63 each consist of two 12 ft. wide though lanes. Eastbound unnamed drive consists of one shared left and right-turn lane and one receiving lane.</p> <p>Shoulder widths vary throughout the project area. Land use in the vicinity of the project area consists of interstate interchange, fuel stations, restaurants, and</p>

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	<p>some residential nearby. The existing conditions are located in Appendix B, pages B-3 to B-6.</p> <p><u>Preferred Alternative</u></p> <p>The preferred alternative is to construct a new reduced conflict intersection (median opening) on SR 63 approximately 390 ft. north of the north gas station drive to the truck plaza; eliminating the existing median opening on SR 63 located across from the north gas station drive; and extending and raising splitter islands at CR 1650 N. New pavement markings, advanced street directional signs, and high lumen roadway luminaire lights will be installed. One stormwater pipe will be installed in the grass median at the north end of the project limits. The pipe will be 15 inches in diameter and 135 ft. long. Approximately 6 tons of revetment riprap on 12 square yards of geotextile will be placed at the outlet of the pipe (Appendix B, pages B-8 to B-22).</p> <p>The proposed improvements will be designed to minimize impacts to the surrounding environment while implementing the applicable design standards. The project termini are logical because they encompass only the area necessary to conduct the proposed intersection improvements. The project has independent utility as its construction is not dependent on another project. The project length is approximately 0.08 mile, not including incidental construction. Construction is currently scheduled to begin in Spring 2024 and continue through Summer 2024.</p> <p>Traffic will be maintained on SR 63 during construction and a detour will not be necessary. Additional information is provided in the Maintenance of Traffic (MOT) section of this document and Appendix B, pages B-11 to B-14.</p> <p>The preferred alternative will meet the purpose and need of the project by decreasing the number of crashes at the intersection of SR 63 and CR 1650 N.</p>
<p>Other Alternatives Considered:</p>	<p><u>Slotted left-turn lane and loon at north gas station drive</u></p> <p>Constructing a reduced conflict intersection at the north gas station drive would involve adding additional pavement along the west side of SR 63 for heavy vehicles to make a U-turn, as well as a deceleration lane along the west most northbound lane. The deceleration lane for northbound SR 63 at the gas station drive would reduce the gas station drive delay from E-36 seconds to D-28 seconds. This alternative eliminates right-angle crashes at CR 1650 N; however, it would allow for right-angle crashes to still occur at the north gas station drive between westbound and northbound or southbound traffic. Therefore, this alternative was discarded from further consideration.</p> <p><u>No Build Alternative</u></p> <p>The No Build alternative would allow the existing intersection to remain with no repair efforts, no expenditure of funds, and no impacts to the surrounding environment. Although this is a feasible option, it would not address the purpose and need of the project, and it would not be prudent to allow the current flow of traffic to persist given the number of dangerous crashes documented at this intersection. For these reasons, this alternative was discarded from further consideration.</p>

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Funding Source(s):	<input checked="" type="checkbox"/> Federal	<input checked="" type="checkbox"/> State	<input type="checkbox"/> Local	<input type="checkbox"/> Other
Project Sponsor:	INDOT, Crawfordsville District			
Estimated Cost:	\$813,286.00	Project Length:	0.24 Mile	
Public Involvement:			No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p>Notice of Entry letters were not mailed to property owners within the project area because the project will take place within the limits of the existing right-of-way (ROW) along SR 63 and CR 1650 N.</p> <p>The project will meet the requirements described in the current <i>Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual</i>, page 55, under the Public Involvement Criteria which states (b) the proposal substantially changes the layout or functions of connecting roadways or the facility being improved and (c) the proposal permanently alters an existing traffic pattern. Therefore, the project sponsor will offer the public an opportunity to submit comments and/or request a public hearing. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.</p>				
Right-of-Way:			No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p>The existing ROW limits along SR 63 extend approximately 120 ft. to 175 ft. along the west side of SR 63 and 145 ft. to 200 ft. along the east side of SR 63. The existing ROW consists of SR 63 and maintained grass.</p> <p>This project will occur within existing ROW. No permanent or temporary ROW will be required for this project.</p> <p>If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT Crawfordsville District Environmental Section will be contacted immediately.</p>				
Maintenance of Traffic (MOT) During Construction:			No: <input type="checkbox"/>	Yes: <input checked="" type="checkbox"/>
<p>The MOT for the project will require Phased construction. A detour will not be necessary to complete the project.</p> <p>Phase 1 will include outside construction. Temporary traffic control devices and temporary pavement markings will be placed to close the southbound outside lane of traffic. Erosion and sediment control measures will be installed. Ingress and egress to all properties will be maintained at all times during construction. The pavement bump-out for the reduced conflict intersection will be constructed. Upon completion of Phase 1, all temporary pavement markings and traffic control devices will be removed from the southbound outside lane closure (Appendix B, pages B-11 to B-12).</p> <p>Phase 2 will include inside construction. Temporary traffic control devices and temporary pavement markings will be placed to close the northbound and southbound inside lanes of traffic. Ingress and egress to all properties will be maintained at all times during construction. The median pavement for the reduced conflict intersection will be constructed and the median pavement between CR 1650 N and the reduced conflict intersection will be removed. Permanent signage and pavement markings will be installed. Seed will be placed at all disturbed areas and temporary erosion control measures will be removed. Upon completion of Phase 2, all temporary pavement markings and traffic control devices will be removed from the northbound and southbound inside lane closures (Appendix B, pages B-13 to B-14).</p>				

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The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

Bridge(s) and/or Small Structure(s) (include structure number(s)):	No: X	Yes:
No bridges or small structures are located within the project area.		

IDENTIFICATION AND EVALUATION OF IMPACTS

Early Coordination:

Early coordination letters were sent on March 17, 2022, Appendix C, pages C-1 to C-3.

Agency	Date Sent	Date Response Received	Appendix
Federal Highway Administration	March 17, 2022	No response received	N/A
Indiana Department of Environmental Management Wellhead Proximity Determinator	March 17, 2022	March 17, 2022	N/A
Midwest Regional Office National Park Service	March 17, 2022	No response received	N/A
Indiana Geological and Water Survey	March 17, 2022	March 17, 2022	Appendix C, pages C-31 to C-33
Indiana Department of Natural Resources Division of Fish and Wildlife	March 17, 2022	April 13, 2022	Appendix C, page C-4
US Department of Housing and Urban Development	March 17, 2022	No response received	N/A
Indiana Department of Transportation Crawfordsville District - Environmental	March 17, 2022	No response received	N/A
Indiana Department of Transportation Crawfordsville District – INDOT PM	March 17, 2022	No response received	N/A
Terre Haute Area Metropolitan Organization	March 17, 2022	No response received	N/A
Vermillion County Commissioners	March 17, 2022	No response received	N/A
Vermillion County Surveyor	March 17, 2022	March 17, 2022	Appendix C, page C-34
Vermillion County Emergency Management	March 17, 2022	No response received	N/A
Vermillion County Surveyor forwarded the letter to the Vermillion County Highway Supervisor	March 17, 2022	No response received	N/A
IDEM, Office of Land Quality, Petroleum Remediation Section	August 29 and September 6, 2022	September 6, 2022	Appendix C, pages C-35 to C-36

All applicable recommendations are included in the Environmental Commitments section of this CE document.

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Streams, Rivers, and Other Jurisdictional Features Impacted:	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B, page B-3), and review of the State of Indiana Geographic Information System (GIS) Library there are no streams, rivers, watercourse, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on May 2, 2022 by Metric Environmental. Therefore, no impacts are expected.		
Open Water Feature(s):	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B, page B-3), and review of the State of Indiana GIS Library there are no open water features within or adjacent to the project area, which was confirmed by the site visit on May 2, 2022 by Metric Environmental. Therefore, no impacts are expected.		
Wetlands:	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B, page B-3), and review of the State of Indiana GIS Library there are no wetlands within or adjacent to the project area, which was confirmed by the site visit on May 2, 2022 by Metric Environmental. Therefore, no impacts are expected.		
Terrestrial Habitat:	No: X	Yes:
Based on a desktop review, a site visit on May 2, 2022 by Metric Environmental, the aerial map of the project area (Appendix B, page B-3), there is grass located within and adjacent to the project area. Approximately 0.70 acre of grass will be impacted for the installation of the stormwater pipe and grading. No trees will be trimmed or removed.		
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) responded on April 13, 2022, with recommendations to revegetate all bare and disturbed areas with a mixture of grasses and to use appropriate measures for controlling erosion and sediment from leaving the construction site (Appendix C, page C-4). However, these recommendations are addressed by specifications and are not included in the Environmental Commitments section of this CE document.		
Protected Species:	No:	Yes: X
Based on a desktop review and the Limited Red Flag Investigation (RFI) report (Appendix E, page E-5), completed by Metric Environmental on March 7, 2022, the IDNR Vermillion Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated April 13, 2022 (Appendix C, page C-4), the Natural Heritage Program's Database has been checked. The American Badger (<i>Taxidea taxus</i>), a state species of special concern, has been documented within 0.5-mile of the project area. Badgers are a wide-ranging species that prefer an open, prairie-type habitat, with Indiana being at the eastern edge of their natural range. The range of the badger continues to expand as a result of land-use changes from forest to farmland and open pastureland. Impacts to the American Badger or its preferred habitat are unlikely as a result of this project. An INDOT 0.5-mile bat review occurred on March 7, 2022. The review of the US Fish and Wildlife Service (USFWS) GIS database for Indiana bat and Northern long-eared bat roosting, hibernacula and capture sites did not identify documented sites within 0.5-mile the project area.		
Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-5 to C-19). The project is within range of the federally endangered Indiana bat (<i>Myotis sodalis</i>) and the federally threatened northern long-eared bat (NLEB) (<i>Myotis septentrionalis</i>). No additional species were generated in the IPaC species list other than the Indiana bat and northern long-eared bat.		
The project qualifies for the <i>Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)</i> , dated May 2016 (revised February 2018), between FHWA, Federal		

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Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 8, 2022, and based on the responses provided, the project was found to "Not Likely to Adversely Affect" (NLAA) the Indiana bat and/or the NLEB (Appendix C, pages C-20 to C-30). INDOT reviewed and verified the effect finding on March 8, 2022, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. The effect finding is dependent on the implementation of specific Avoidance and Minimization Measures (AMMs) as project commitments. The AMMs include making all personnel working in areas of known or presumed bat habitat aware of all environmental commitments and directing temporary lighting away from suitable habitat during the active season. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources:	No: X	Yes:
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Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-2), there are no karst features identified within or adjacent to the project area. In the early coordination response dated March 17, 2022, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, pages C-31 to C-33). IGWS indicated that geological hazards include moderate liquefaction potential and 1% annual chance flood hazard; mineral resources include high potential bedrock resource and high potential sand and gravel resource; and there are no documented active or abandoned mineral resources extraction sites located within 0.5-mile of the project area. The features will not be affected because excavation is planned to be less than 10 ft. Response from IGWS has been communicated to the designer on March 17, 2022. No impacts are expected.

Drinking Water Resources:	No: X	Yes:
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The project is located in Vermillion County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 17, 2022 by Metric Environmental. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on April 22, 2022 by Metric Environmental. The nearest wells are located west of the project area. The features will not be affected because they are not located within the construction limits. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

Based on a desktop review of <https://entapps.indot.in.gov/MS4/> by Metric Environmental on March 17, 2022, this project is not located in an Urban Area Boundary. No impacts are expected.

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<u>Not in a Public Water System Location</u>		
Based on a desktop review, a site visit on May 2, 2022 by Metric Environmental, the aerial map of the project area (Appendix B, page B-3), and consultation with the designer, no public water systems were identified. Therefore, no impacts are expected.		
Floodplains:	No: X	Yes:
The Indiana Department of Natural Resources Indiana Floodway Information Portal website (https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e) was accessed on April 23, 2022 by Metric Environmental. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix B, page B-7). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.		
Farmland:	No: X	Yes:
Based on a desktop review, a site visit on May 2, 2022 by Metric Environmental, the aerial map of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.		
Cultural Resources:	No: X	Yes:
On April 8, 2022, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category A, Types A-2 and A-3 and Category B, Type B-2 under the Minor Projects Programmatic Agreement, (Appendix D, pages D-1 to D-4). Type A-2 includes all work within interchanges and within medians of divided highways in previously disturbed soils; type A-3 includes replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone or brick structures or parts therein and are in previously disturbed soils; and type B-2 includes installation of new lighting, signals, signage, and other traffic control devices. This project is occurring in previously disturbed soils. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.		
Section 4(f) and Section 6(f) Resources:	No: X	Yes:
Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.		
Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and review of the State of Indiana GIS Library there are no potential 4(f) resources located within the 0.5-mile search radius. According to the site visit on May 2, 2022 by Metric Environmental, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.		
The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.		
A review of 6(f) properties on the INDOT ESD website revealed a total of four properties in Vermillion County (Appendix G, page G-7). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.		

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Air Quality:	No: X	Yes:
<p>This project is included in the Fiscal Year (FY) 2022 - 2026 Statewide Transportation Improvement Program (STIP) (Appendix F, page F-1).</p> <p>This project is located in Vermillion County, which is currently in attainment for all criteria pollutants according to https://www.in.gov/idem/sips/nonattainment-status-of-counties/. Therefore, the conformity procedures of 40 CFR Part 93 do not apply.</p> <p>This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.</p>		
Community Impacts:	No: X	Yes:
<p>Under FHWA Order 6640.23 A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require no new, additional permanent right-of-way; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.</p>		
Public Facilities and Services (e.g. schools, emergency services):	No: X	Yes:
<p>Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and review of the State of Indiana GIS Library there are no public facilities within the 0.5-mile search radius. There are no public facilities within or adjacent to the project area, which was confirmed by the site visit on May 2, 2022 by Metric Environmental. Therefore, no impacts are expected. Access to all properties will be maintained during construction.</p> <p>The Vermillion County Surveyor responded on March 17, 2022 indicating if any Cornerstone Monuments are found, the accurate recording of such needs to be forwarded to their office. If new monuments are installed, make sure that the concrete base for those monuments have some rebar or other detectable metal in them (Appendix C, page C-34). All applicable recommendations are included in the Environmental Commitments section of this CE document.</p> <p>It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.</p>		
Hazardous Materials and Regulated Substances:	No:	Yes: X
<p>Based on coordination with INDOT ESD and INDOT SAM it was determined completing a Limited Red Flag Investigation (LRFI) was appropriate (Appendix E, pages E-1 to E-2). Only the hazardous material 0.5-mile radius search was reviewed for this LRFI. Three Leaking Underground Storage Tank (LUST) sites, one Institutional Control site, three National Pollutant Discharge Elimination System (NPDES) Pipe Locations, and three Notice of Contamination sites are located within 0.5-mile of the project area.</p> <p><u>LUST/Institutional Control Sites:</u> Pilot Travel Center #339, 16502 N. SR 63, AI ID 55323, located at the northeast corner of SR 63 and East CR 1650 N, adjacent east of the project area. Two of the three LUST sites are noted to occur here. The property owner submitted a Closure Strategy Report in May 2018, indicating their intent to achieve NFA status through the use of an ERC. Data provided in support of closure included soil sampling results from monitoring well installation and 16 quarters of groundwater sampling. The data showed all contamination remaining on the site is localized to an area southeast of the building, with samples closer to the project area reporting as non-detect. Groundwater flow is to the east-southeast, away from the project area. The facility submitted a draft ERC in August 2021 that IDEM approved in an e-mail on December 3, 2021. The ERC will allow</p>		

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groundwater extraction only from a specified potable water well, with other uses prohibited except for environmental investigation purposes. The ERC does not appear to have been recorded yet, so no NFA has been issued. Coordination will be conducted with the IDEM Project Manager, Andrew Sliker, asliker@idem.in.gov, before RFC.

- Based on coordination with the IDEM Project Manager, there is no impacted area near the proposed construction limits. IDEM does not have any further recommendation concerning the potential to encounter/handling contamination beyond proper handling, removal, and disposal. If impact is found during construction that is beyond the SR 63 & CR 1650 intersection, please notify Andrew Sliker at ASliker@idem.IN.gov (Appendix C, pages C-35 to C-36).

NPDES Facilities: Spring Creek Travel Plaza, which has an effective private permit, is adjacent to the project area. Coordination with Spring Creek Travel Plaza will occur.

- Based on the Geographic Information System (GIS), NPDES facility #INRA00634, mentioned in the LRFI, is apparently associated with Spring Creek Travel Plaza (also known as Pilot Travel Center #339); however, conducting a search on Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC) that permit number is not associated with Pilot Travel Center or with any other facility. The only NPDES permit associated with the Pilot Travel Center is the mixed ownership permit #IN0057151 which terminated April 29, 2012, as described in the LRFI. The address for Permit #INRA00634 is identified as 2001 W. I-74 which is actually Spring Creek Welcome Center (Rest Area), approximately 2.9 mile west of the Project area. The Rest Area is an NPDES Facility; however, there permit number is #IN0054208 and is too far from the project area to be a concern. There appears to be conflicting information; therefore, it has been deduced that no coordination will be required.

Permits:	No: X	Yes:
No permits will be required for this project.		
It is the responsibility of the project sponsor to identify and obtain all required permits.		

ENVIRONMENTAL COMMITMENTS:

- Firm:**
- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
 - 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
 - 3) Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
 - 4) If any hazardous material impacts are found during construction that are beyond the SR 63 & CR 1650 intersection, please notify Andrew Sliker at ASliker@idem.IN.gov.
 - 5) General AMM1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.
 - 6) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season.
 - 7) Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

APPENDICES

APPENDIX A: INDOT Supporting Documentation

- Threshold Chart A-1

APPENDIX B: Graphics

- Location Map..... B-1
- USGS Topographic Map B-2
- 2017 Aerial Photograph B-3
- Photograph Location Map B-4
- Site Photographs..... B-5
- IDNR Floodplain Map..... B-7
- Road Plans..... B-8

APPENDIX C: Early Coordination

- Sample Early Coordination Letter C-1
- Early Coordination Recipients..... C-3
- IDNR-DFW Response..... C-4
- USFWS Official Species List..... C-5
- USFWS Concurrence Verification Letter C-20
- IGWS Response C-31
- Vermilion County Surveyor Response C-34
- IDEM, Office of Land Quality, Petroleum Remediation Section Response C-35

APPENDIX D: Section 106 of the National Historic Preservation Act

- MPPA Project Assessment Form..... D-1

APPENDIX E: Red Flag and Hazardous Materials

- Red Flag Investigation E-1

APPENDIX F: Air Quality

- FY 2022-2026 STIP F-1

APPENDIX G: Additional Studies

- Crash Analysis 2015 to 2018 G-1
- LWCF County Property List for Indiana; Vermilion County..... G-7

**APPENDIX A:
INDOT Supporting Documentation**

Categorical Exclusion Level Thresholds

Note: This document is a CE Level 1 due to Public Involvement Requirements.

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁶)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B:

Graphics



ILLINOIS

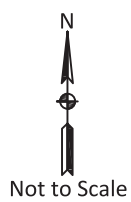


P:\2018\18-0056 - BFS - INDOT - 1803 Item 6 - Crawfordsville Dist 5 Bundled Intersection Projects\4 - Deliverables\Exhibits\Task 3\CAD Files\Location Map.dwg

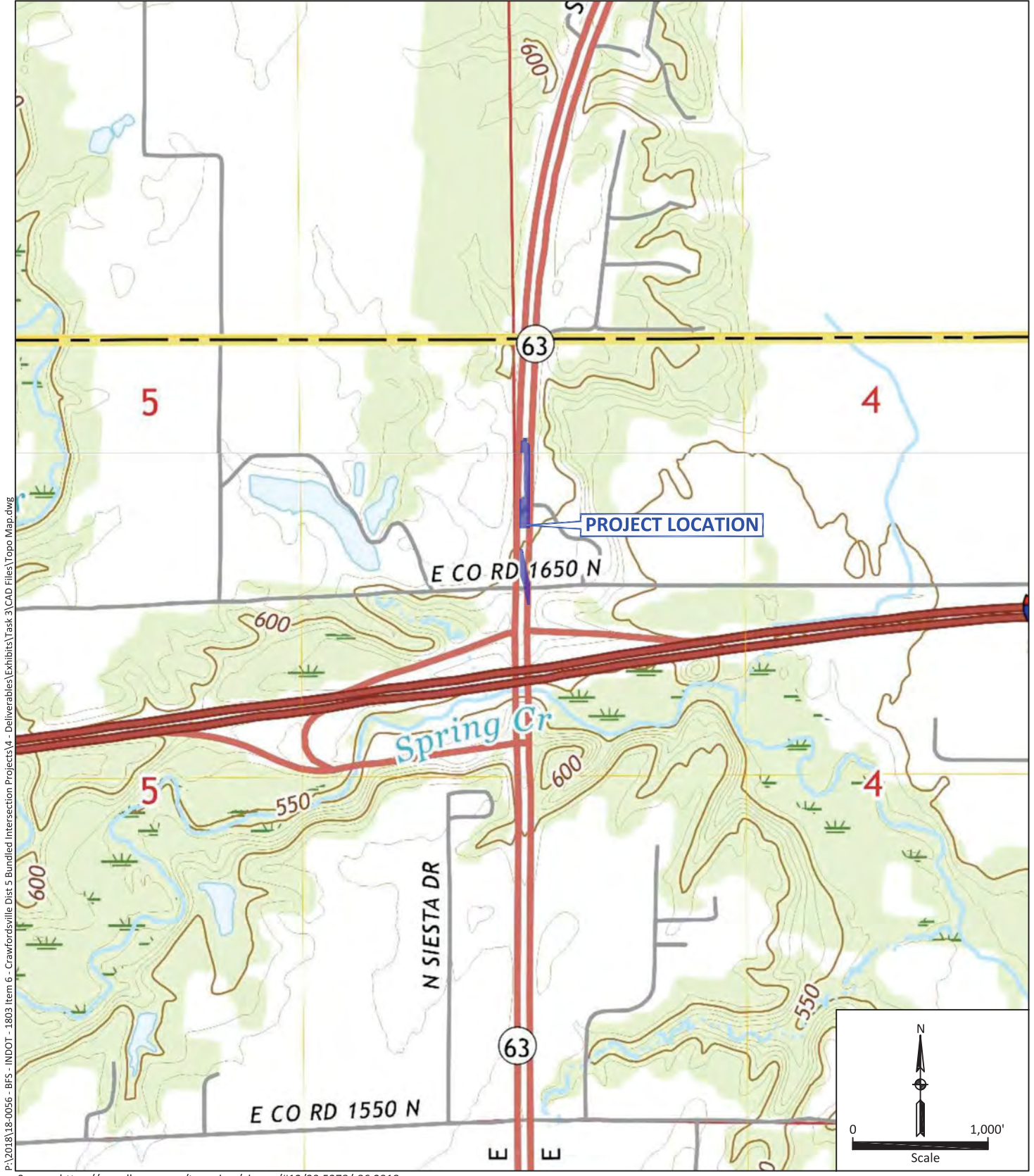
Source: <http://maps.indiana.edu/>

Location Map
 Intersection Improvement
 SR 63 from 1,400 feet South of Eastbound I-74
 Ramp to 1,650 feet North of Westbound I-74 Ramp,
 Vermillion County, Indiana
 Des. No. 1700098
 Metric Project #18-0056-3

All locations approximate



Drawn by: ILJ
 Checked by: SC
 Approved by: LBH
 Date: March, 2022



P:\2018\18-0056 - BFS - INDOT - 1803 Item 6 - Crawfordsville Dist 5 Bundled Intersection Projects\4 - Deliverables\Exhibits\Task 3\CAD Files\Topo Map.dwg

Source: <https://ngmdb.usgs.gov/topoview/viewer/#12/39.5378/-86.2918>

USGS Topographic Map
 Intersection Improvements
 SR 63 from 1,400 feet South of Eastbound I-74
 Ramp to 1,650 feet North of Westbound I-74 Ramp,
 Vermillion County, Indiana
 Des. No. 1700098
 Metric Project #18-0056-3

All locations approximate
 Base map:
 2019 Perrysville, IN
 7.5 Minute Quadrangle



Drawn by: ILJ
 Checked by: SC
 Approved by: LBH
 Date: March, 2022



Source: Google Earth Pro

2017 Aerial Photograph

Intersection Improvements
 SR 63 from 1,400 feet South of Eastbound I-74
 Ramp to 1,650 feet North of Westbound I-74 Ramp,
 Vermillion County, Indiana
 Des. No. 1700098
 Metric Project #18-0056-3

Note: All locations are approximate

— Construction Limits



Drawn by: ILJ
 Checked by: SC
 Approved by: LBH
 Date: March, 2022



Source: Google Earth Pro

Photograph Location Map
 Intersection Improvements
 SR 63 from 320 feet North of Westbound I-74 Ramp
 to 1,290 feet North of Westbound I-74 Ramp,
 Vermillion County, Indiana
 Des. No. 1700098
 Metric Project #18-0056-3

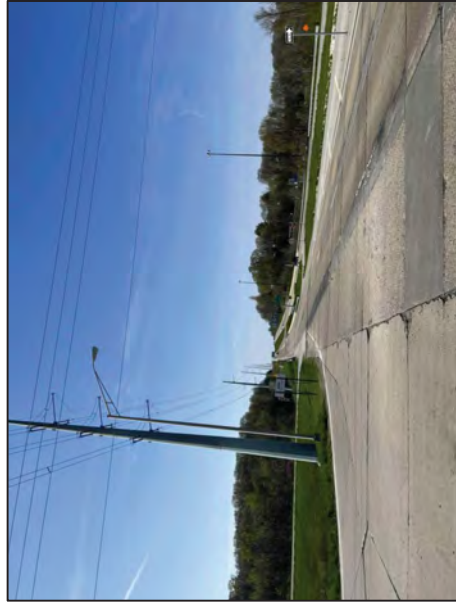
Note: All locations are approximate

— Construction Limits

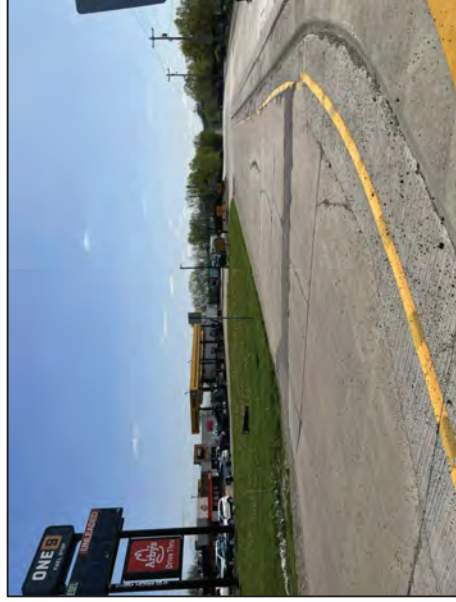


METRIC ENVIRONMENTAL

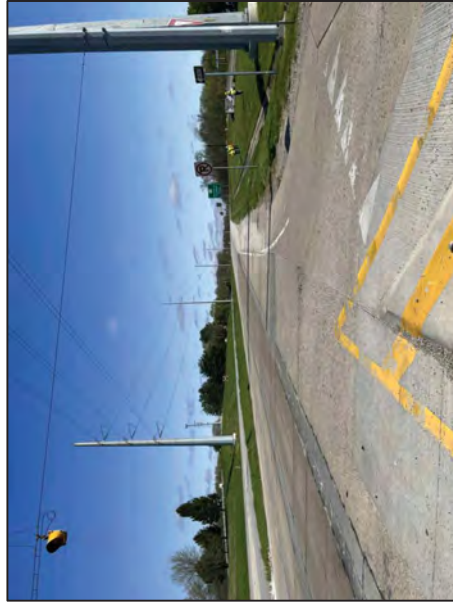
Drawn by: ILJ
 Checked by: SC
 Approved by: LBH
 Date: May, 2022



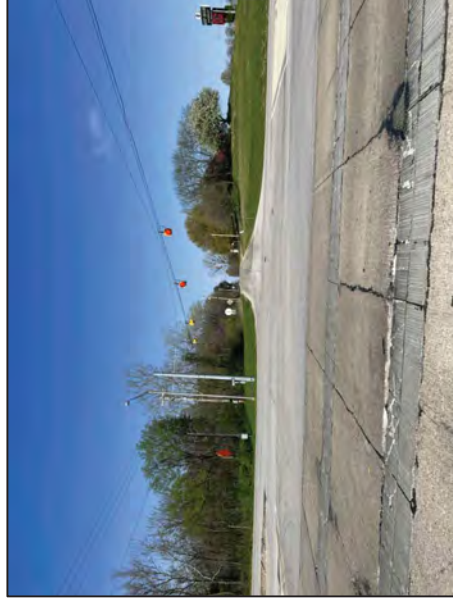
1. View of SR 63 at CR 1650, facing south.



2. View of CR 1650 at SR 63, facing east.



3. View of SR 63 at CR 1650, facing north.



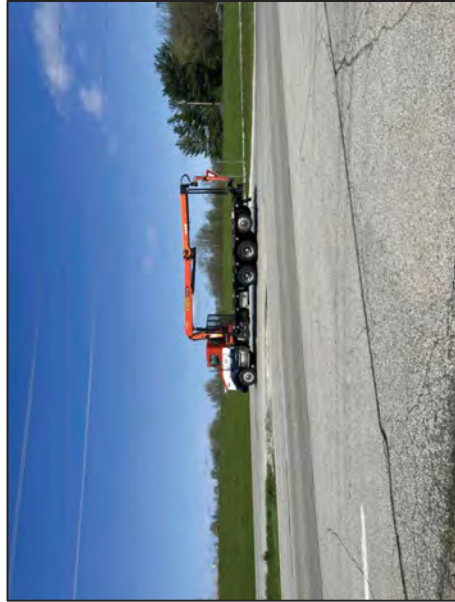
4. View of SR 63 at CR 1650, facing west.

Site Photographs

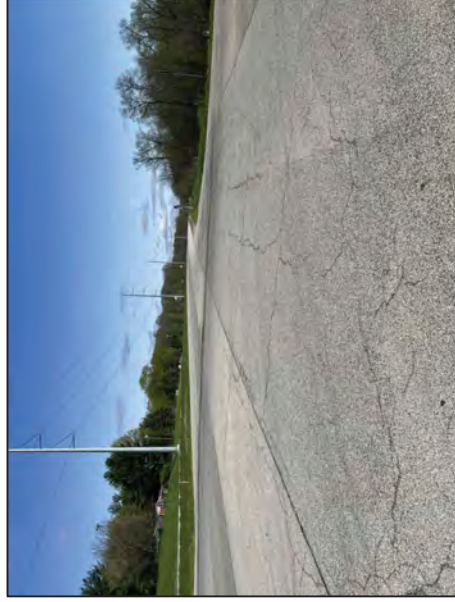
Intersection Improvements
SR 63 from 320 feet North of Westbound I-74 Ramp
to 1,290 feet North of Westbound I-74 Ramp,
Vermillion County, Indiana
Des. No. 1700098
Metric Project #18-0056-3



Drawn by: ILJ
Checked by: SC
Approved by: LBH
Date: May, 2022



5. View of North Gas Station Drive at SR 63, facing west at the left turn cross over.



6. View of SR 63 at North Gas Station Drive, facing north.



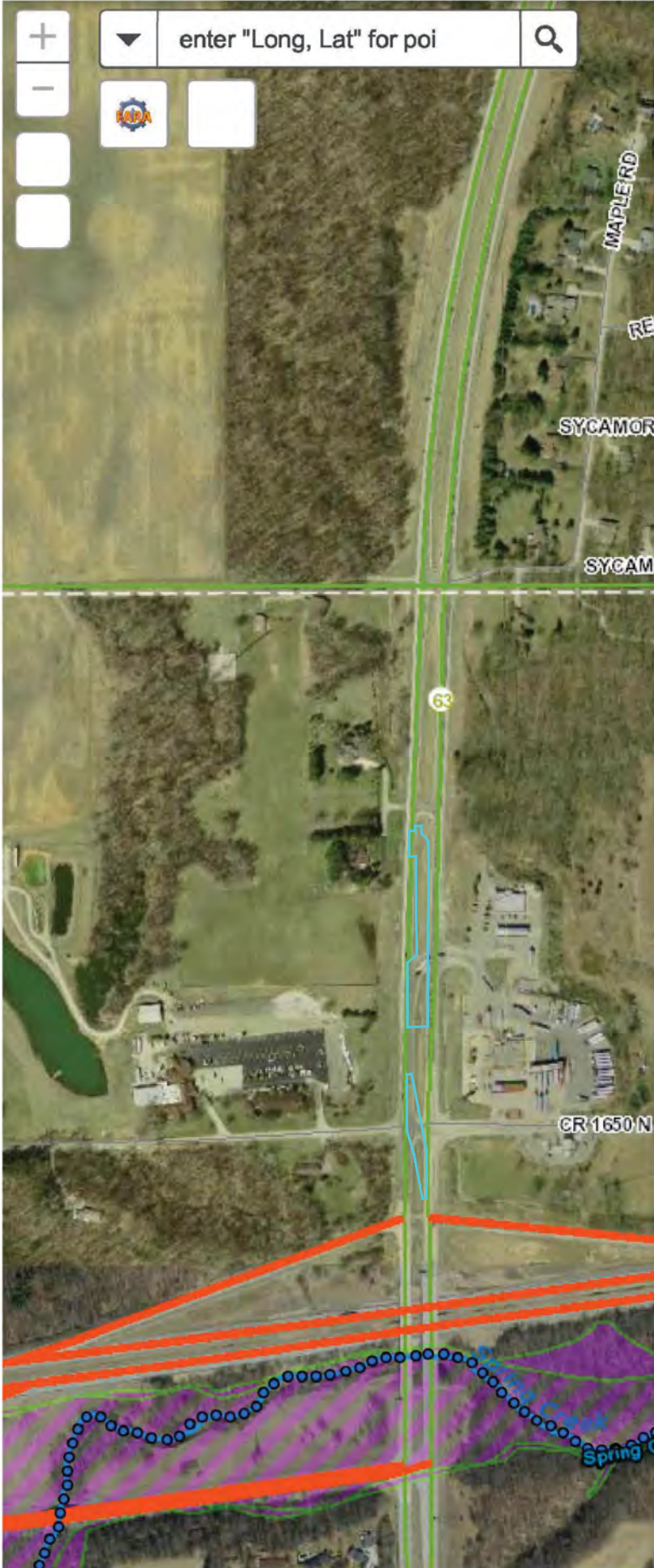
7. View of SR 63 facing northwesterly, where the J-turn will be constructed.

Site Photographs

Intersection Improvements
SR 63 from 320 feet North of Westbound I-74 Ramp
to 1,290 feet North of Westbound I-74 Ramp,
Vermillion County, Indiana
Des. No. 1700098
Metric Project #18-0056-3



Drawn by: ILJ
Checked by: SC
Approved by: LBH
Date: May, 2022



Legend

Local Flood Plain Administrator Jurisdiction



FLOOD_FEPs - Flood Elevation Points

- 1.0
- 1.5
- 1.75
- 2.0

Drainage Areas for Indiana NHD Flowlines

Rivers and Streams at least 1 square mile

- 1 - 10
- 10 - 100
- 100 - 500
- > 500

Best Available Flood Hazard Layer (BAFL)

Best Available Flood Hazard Layer

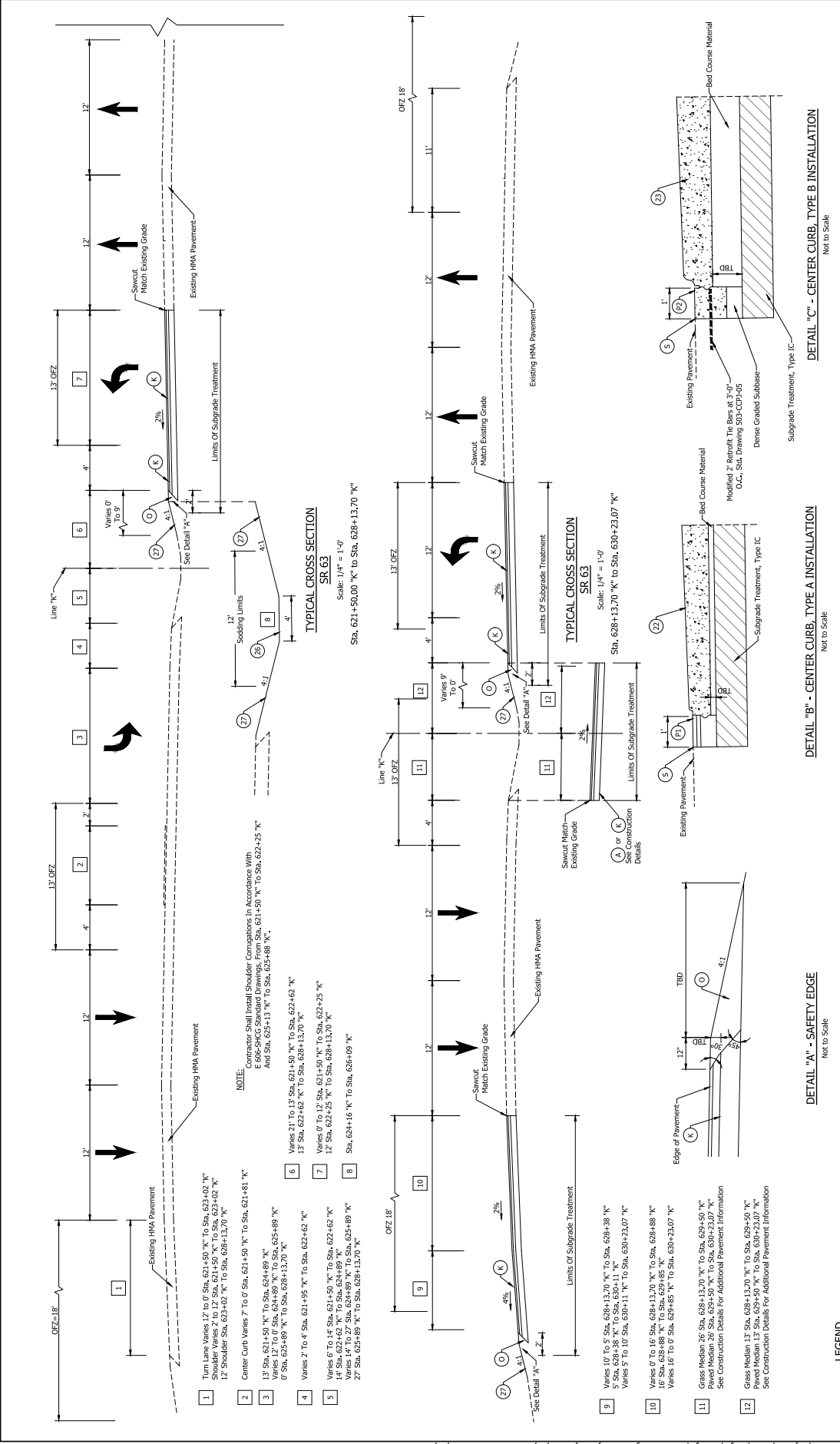
- FEMA Zone AE Floodway; FEMA Administrative Floodway
- DNR Detailed Floodway
- DNR Approximate Floodway
- FEMA Zone A
- FEMA Zone AE
- DNR Detailed Fringe
- DNR Approximate Fringe
- Additional Floodplain Area; DNR .2 Percent Flood Hazard
- FEMA Protected by Levee
- FEMA Floodplain - Ponding (Depth)
- FEMA Floodplain - Sheet Flow (Depth)
- Not Mapped

— Construction Limits

1:9028

-87.451 40.123 Degrees

B-7



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: _____	INR _____	DRAWN: _____	JMT _____	CHECKED: _____	NRG _____
CHECKED: _____	JCS _____	CHECKED: _____	NRG _____		

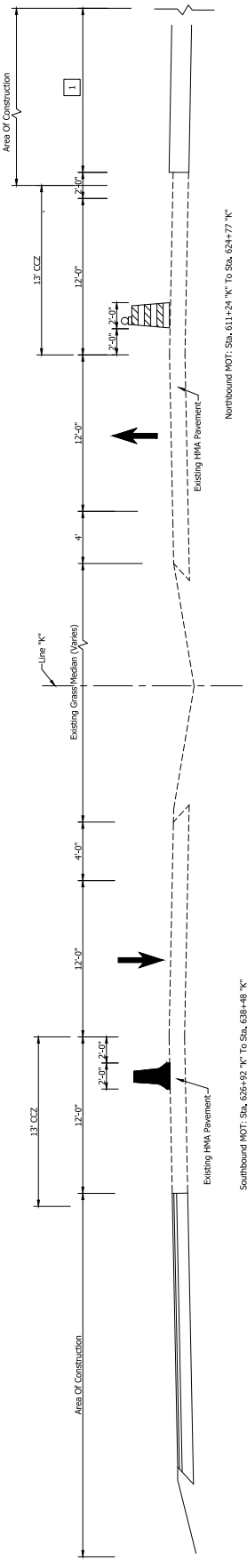
INDIANA DEPARTMENT OF TRANSPORTATION	BRIDGE FILE
TYPICAL CROSS SECTIONS	VERTICAL SCALE AS NOTED
	DESIGNATION N/A
	SHEET BOOK 3
	SHEET 33
	PROJECT 1700098
	CONTRACT R-6072
	PROJECT 1700098

PRELIMINARY PLANS

B-9

LEGEND	(A) PCP CONCRETE PAVEMENT (TBD)	(21) CONC. CENTER CURB TYPE B
(K) FULL DEPTH HMA PAVEMENT (TBD)	(22) NURSERY, SOODING	(26) MULCHED SEEDING R
(O) COMPACTED AGGREGATE NO. 53	(27) HMA FULL DEPTH PATCHING (TBD)	(28) SAW CUT
(2) CONC. CENTER CURB TYPE A		

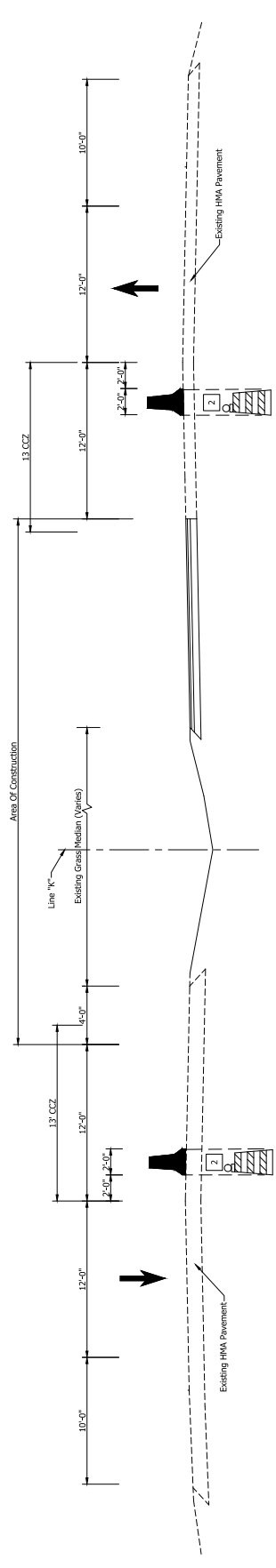
DETAIL "A" - SAFETY EDGE		DETAIL "B" - CENTER CURB, TYPE A INSTALLATION		DETAIL "C" - CENTER CURB, TYPE B INSTALLATION	
Not to Scale		Not to Scale		Not to Scale	



TYPICAL CROSS SECTION M.O.T. PHASE 1
SR 63

Scale: 1/4" = 1'-0"

Center Curb Varies 49' To 5' Sta. 619+99 "K" To Sta. 620+43 "K"



TYPICAL CROSS SECTION M.O.T. PHASE 2
SR 63

Scale: 1/4" = 1'-0"

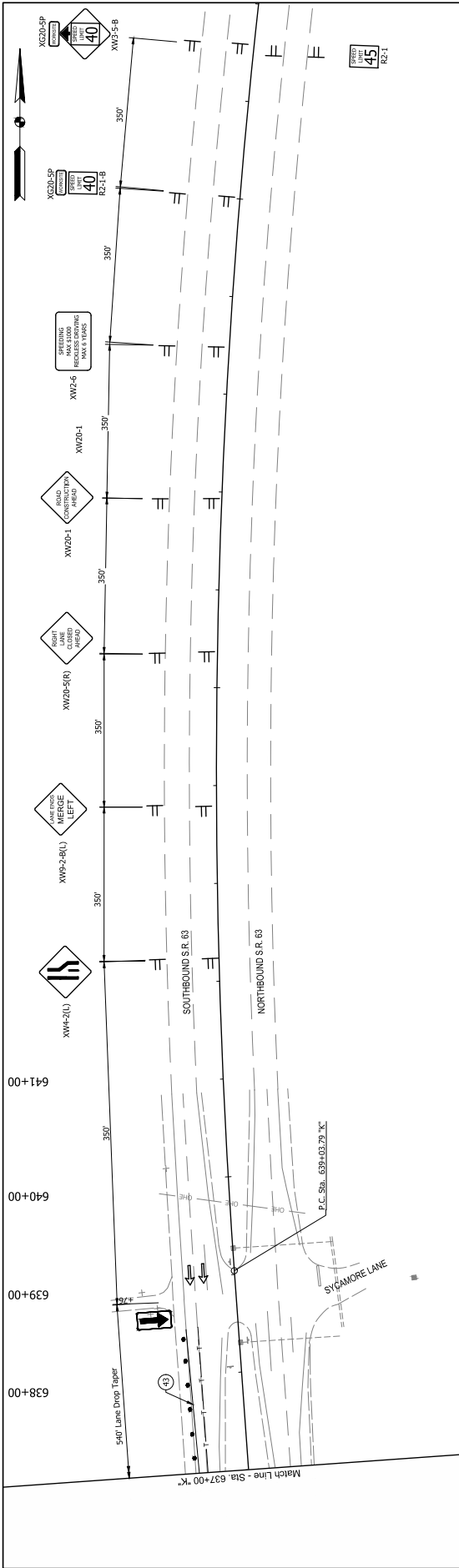
Sta. 626+92 "K" To Sta. 638+48 "K"

Barrels and Temporary Traffic Barrier Used At Various Locations. See Temporary Traffic Control Phase 2 Plan Sheet For More Information.

RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER:	DATE:	INDIANA DEPARTMENT OF TRANSPORTATION	RESIDENTIAL SCALE:	BRIDGE FILE:
DESIGNED:	INRO:	DRAWN:	MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS	VERTICAL SCALE:	DESIGNATION:
CHECKED:	JCS:	CHECKED:		SURVEY BOOK:	1700098
				SHEET:	33
				PROJECT:	4
				CONTRACT:	R-4072
					1700098

PRELIMINARY PLANS

B-10



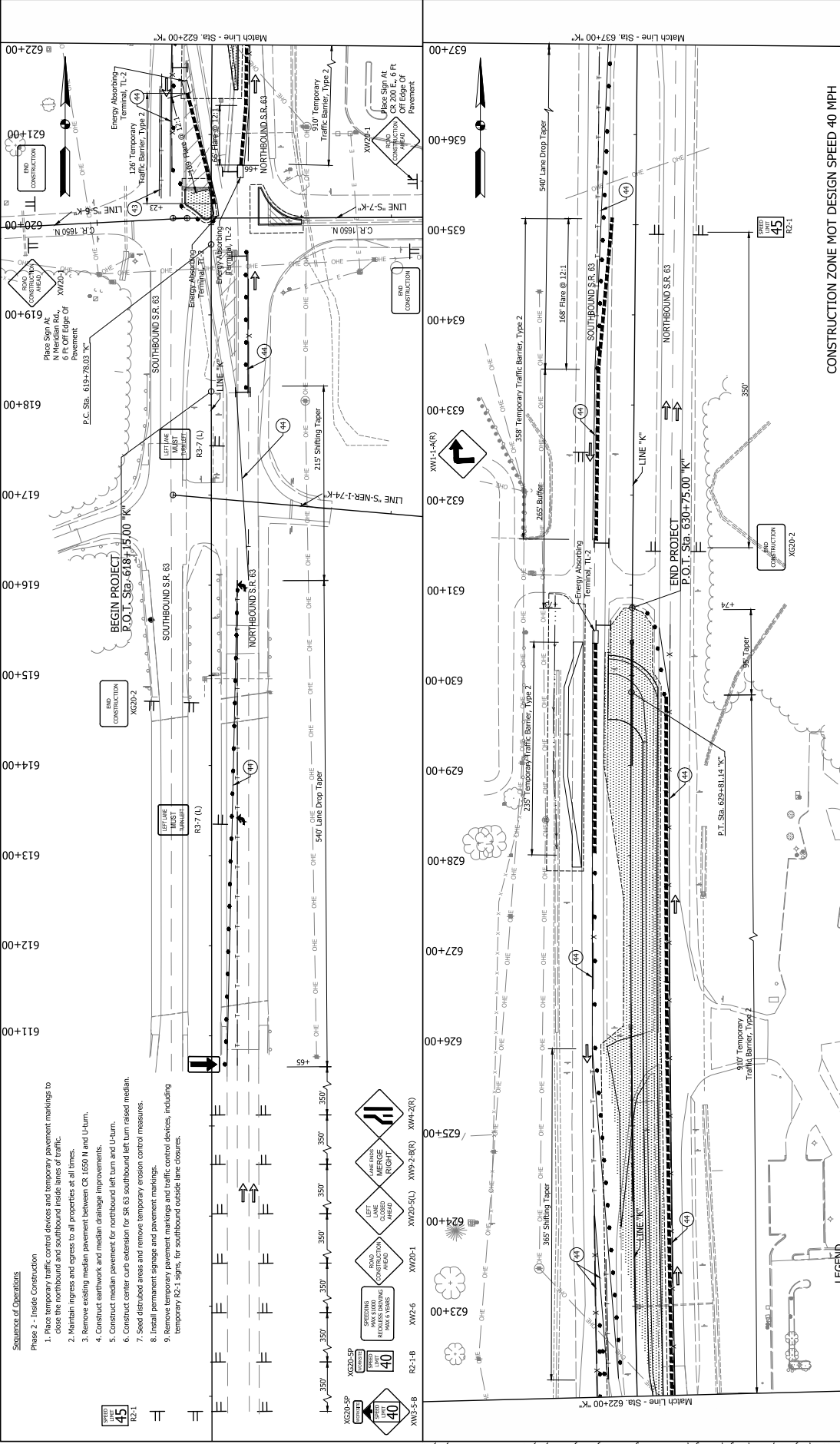
SIGN TYPE	MESSAGE	SIGN CODE	MOT PHASE	MAX (EACH)
B	SPEED LIMIT 45	R2-1	1	2
B	SPEED LIMIT 40	R2-1-B	4	4
A	LEFT LANE MUST TURN LEFT	R3-7	2	2
B	NO TRUCKS SYMBOL	R52	1	1
B	ROAD CONSTRUCTION	XG20-SP	8	8
B	WORKSITE	XG20-SP	8	8
A	ROAD CONSTRUCTION AHEAD	XW20-1	4	4
A	LEFT LANE CLOSED AHEAD	XW20-5(L)	4	4
A	RIGHT LANE CLOSED AHEAD	XW20-5(R)	4	4
A	40 MPH SPEED ZONE AHEAD	XW5-5-B	4	4
A	LANE MERGE SYMBOL (L)	XW4-2-A(L)	4	4
A	LANE MERGE SYMBOL (R)	XW4-2-A(R)	4	4
A	LANE ENDS MERGE LEFT	XW9-2A(L)	4	4
A	LANE ENDS MERGE RIGHT	XW9-2A(R)	4	4
TOTAL SIGN TYPE A			40	40
TOTAL SIGN TYPE B			4	23

- LEGEND**
- AREA OF CONSTRUCTION
 - TRAFFIC FLOW
 - CHANNELIZING DEVICE
 - TRAFFIC SIGNS
 - TEMP. PAVEMENT MARKING, REMOVABLE SOLID WHITE, 5"
 - FLASHING ARROW SIGN
 - BLACK TEMPORARY TAPE TYPE L, 5"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED BY	INRO	DRAWN BY
CHECKED BY	JCS	CHECKED BY
INDIANA DEPARTMENT OF TRANSPORTATION		
MAINTENANCE OF TRAFFIC PHASE 1		
BRIDGE FILE	DESIGNATION	PROJECT
VERTICAL SCALE	SURVEY BOOK	SHEET
1"=50'	N/A	1700098
CONTRACT	PROJECT	DATE
R-4072	1700098	3/23

PRELIMINARY PLANS

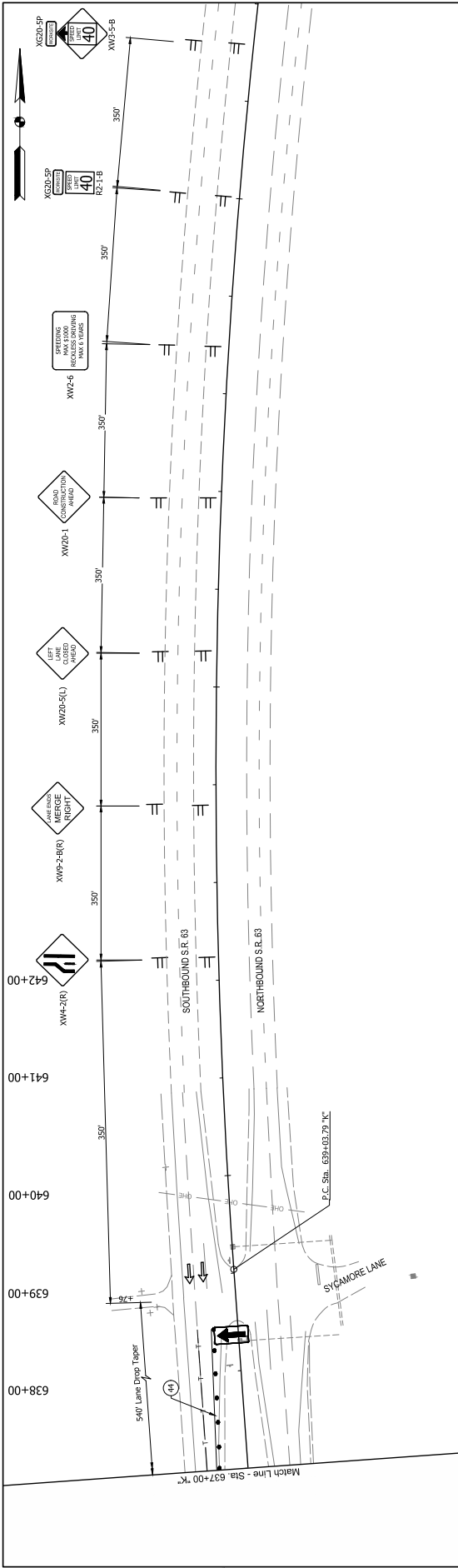
B-12



SCIENCE OF OPERATIONS Phase 2 - Inside Construction 1. Place temporary traffic control devices and temporary pavement markings to close the northbound and southbound inside lanes of traffic. 2. Maintain ingress and egress to all properties at all times. 3. Remove existing median pavement between CR 1650 N and U-turn. 4. Construct earthwork and median drainage improvements. 5. Construct median pavement for northbound left turn and U-turn. 6. Construct center curb extension for SR 63 southbound left turn raised median. 7. Seed disturbed areas and remove temporary erosion control measures. 8. Install permanent signage and pavement markings. 9. Remove temporary pavement markings and traffic control devices, including temporary R2.1 signs, for southbound outside lane closures.		RECOMMENDED FOR APPROVAL: DESIGNER: _____ DATE: _____ CHECKED: _____ JCS DESIGNED: _____ NRO DRAWN: _____ JMT CHECKED: _____ NRO	
BRIDGE FILE BRIDGE NO. 1700098 DESIGNATION: N/A SURVEY BOOK: 7 SHEET NO. 33 CONTRACT: R-4072 PROJECT: 1700098		INDIANA DEPARTMENT OF TRANSPORTATION MAINTENANCE OF TRAFFIC PHASE 2	
CONSTRUCTION ZONE MOT DESIGN SPEED 40 MPH		LEGEND [Hatched] AREA OF CONSTRUCTION [Arrow] TRAFFIC FLOW [Arrow] TEMP. LANE INDICATION ARROW [Symbol] CHANNELLING DEVICE [Symbol] TYPE III-B BARRICADE [Symbol] FLASHING ARROW SIGN [Symbol] TEMP. PAVEMENT MARKING, REMOVABLE SOLID WHITE, 5" [Symbol] TEMP. PAVEMENT MARKING, REMOVABLE SOLID YELLOW, 5" [Symbol] BLACK TEMPORARY TAPE TYPE 1, 5" [Symbol] TEMP. TRAFFIC BARRIER, TYPE 2	

PRELIMINARY PLANS

B-13



\\s161241\pos5\625100.000\Development\Design\Drawings\6251\6251 R21.dwg Zohary Sindy Plot:7/19/2022 3:24 PM Scale: 7/13/2022 1:54 PM

LEGEND

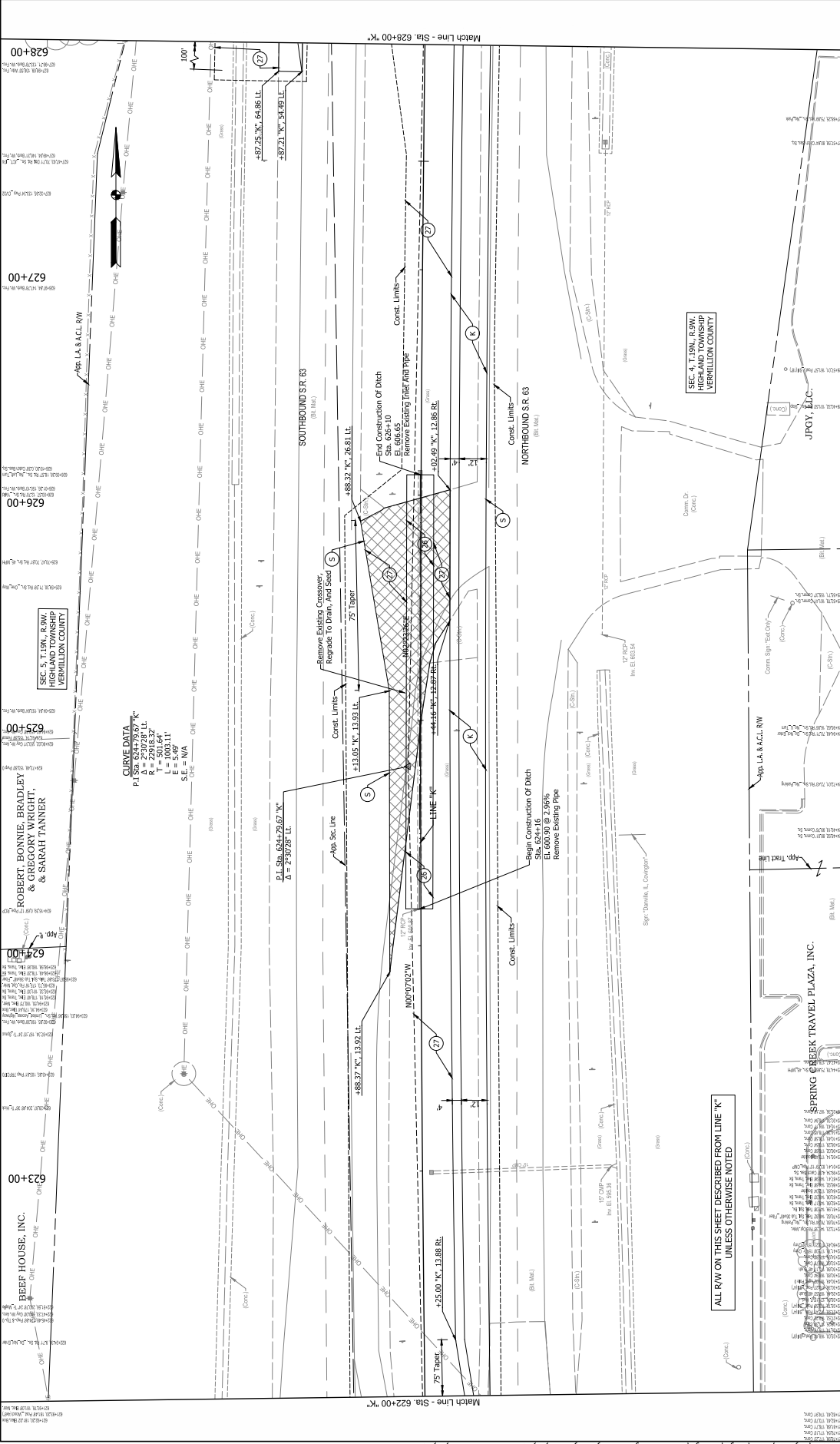
- ↑ TRAFFIC FLOW
- ▬ CHANNELIZING DEVICE
- CHANNELIZING DEVICE
- ▬ TRAFFIC SIGNS
- TYPE III-B BARRICADE
- ▬ FLASHING ARROW SIGN
- Ⓢ TEMP. PAVEMENT MARKING, REMOVABLE SOLID YELLOW, 5"

RECOMMENDED FOR APPROVAL:		DESIGN ENGINEER:	DATE:
DESIGNED:		INR:	JMT
CHECKED:		JCS	DRG
INDIANA DEPARTMENT OF TRANSPORTATION MAINTENANCE OF TRAFFIC PHASE 2			
RESIDENTIAL SCALE	BRIDGE FILE	DESIGNATION	1700098
VERTICAL SCALE	N/A	SHEET	6
SURVEY BOOK	CONTRACT	PROJECT	33
	R-4072		1700098

PRELIMINARY PLANS

B-14

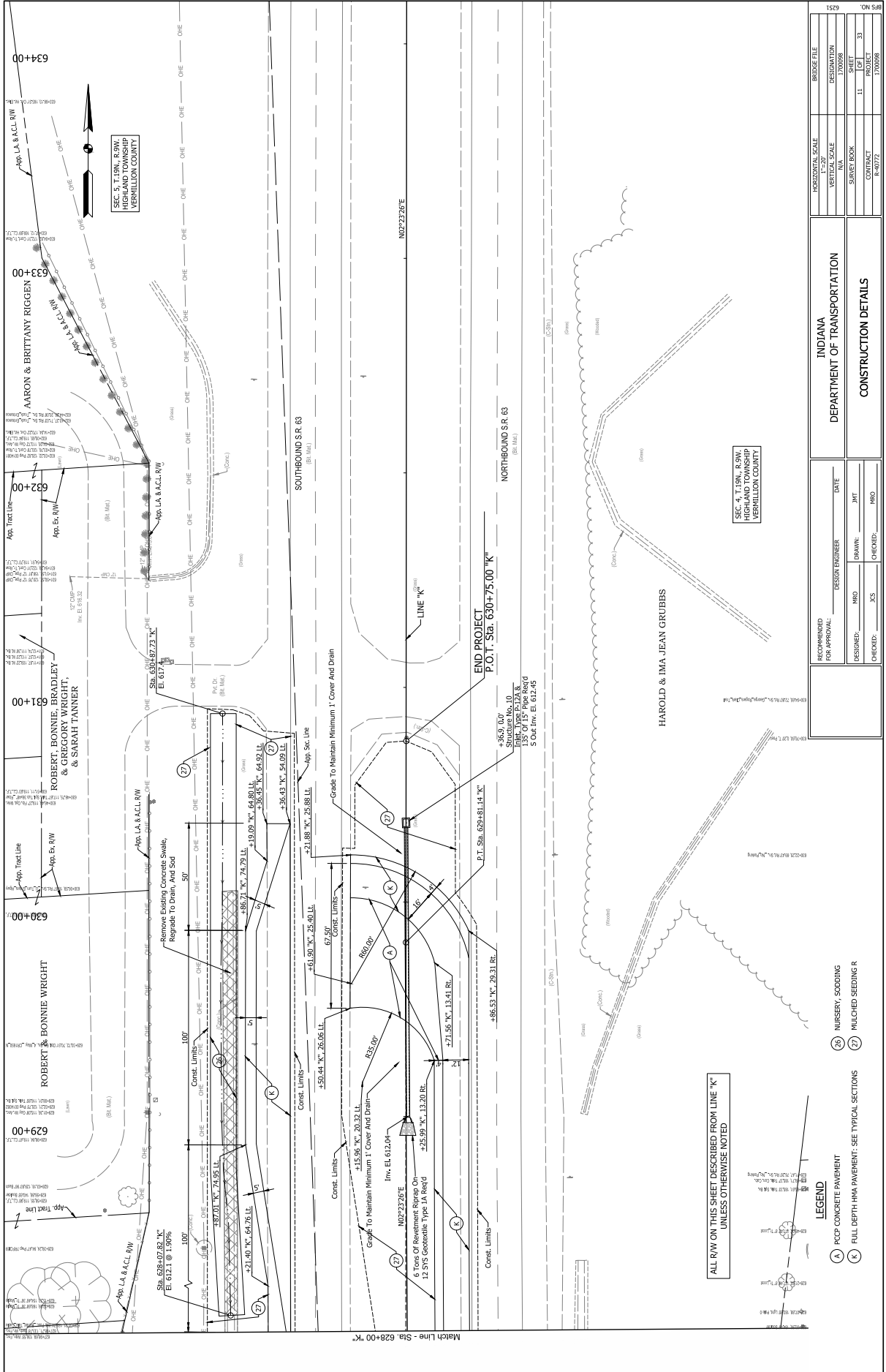
1529 ON 5/8/8



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED		INRO	JMT
CHECKED		JCS	INRO
INDIANA DEPARTMENT OF TRANSPORTATION			
CONSTRUCTION DETAILS			
BRIDGE FILE	RESIDENTIAL SCALE	VERTICAL SCALE	DESIGNATION
	1"=20'	1"=20'	1700098
			SURVEY BOOK
			10
			SHEET PROJECT
			33
			CONTRACT
			R-6072
			PROJECT
			1700098

PRELIMINARY PLANS

B-16



BRIDGE FILE	RESIDENTIAL SCALE	VERTICAL SCALE	DESIGNATION
	1"=20'	1"=20'	1700098
			SHEET
			11
			PROJECT
			1700098

INDIANA	
DEPARTMENT OF TRANSPORTATION	
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER
DATE	
DESIGNED BY	DRAWN BY
INR	JMT
CHECKED BY	INR
JCS	INR
CONSTRUCTION DETAILS	

SEC. 4, T. 19N., R. 9W., HIGHLAND TOWNSHIP VERMILION COUNTY

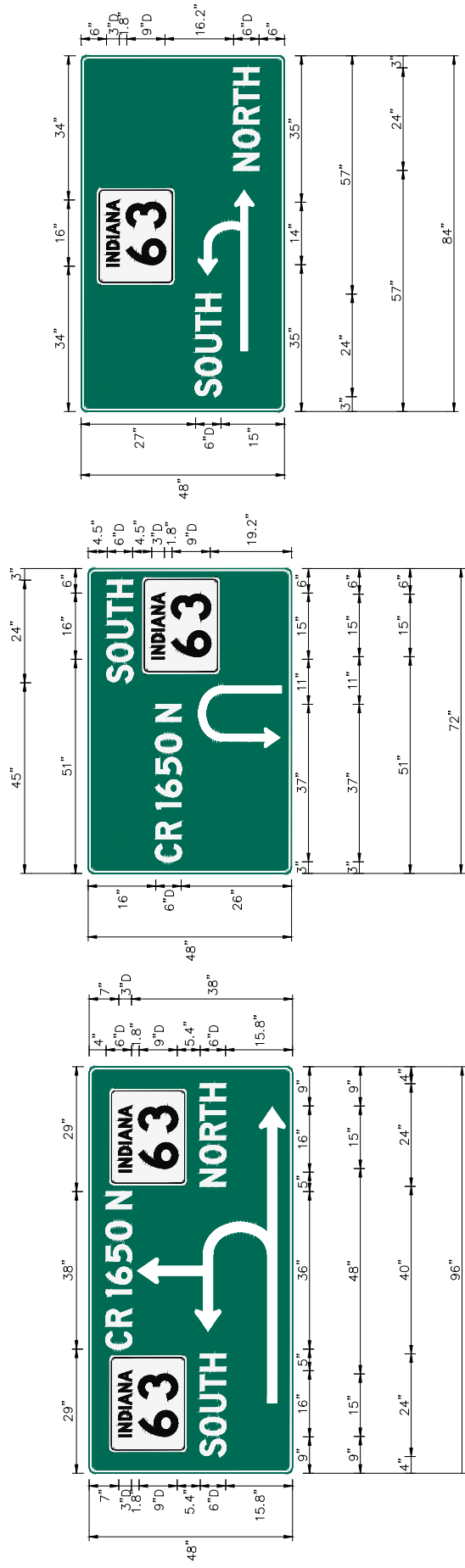
SEC. 5, T. 19N., R. 9W., HIGHLAND TOWNSHIP VERMILION COUNTY

LEGEND
(A) PC/P CONCRETE PAVEMENT
(K) FULL DEPTH HMA PAVEMENT - SEE TYPICAL SECTIONS
(26) NURSERY, SOODING
(27) MULCHED SEEDING R

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "K" UNLESS OTHERWISE NOTED

PRELIMINARY PLANS

B-17



SIGN NUMBER	D3-2a
WIDTH x HGHT.	8'-0" x 4'-0"
BORDER WIDTH	0.5"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
LEGEND/BORDER	COLOR: Green
	TYPE: Reflective
	COLOR: Black/White

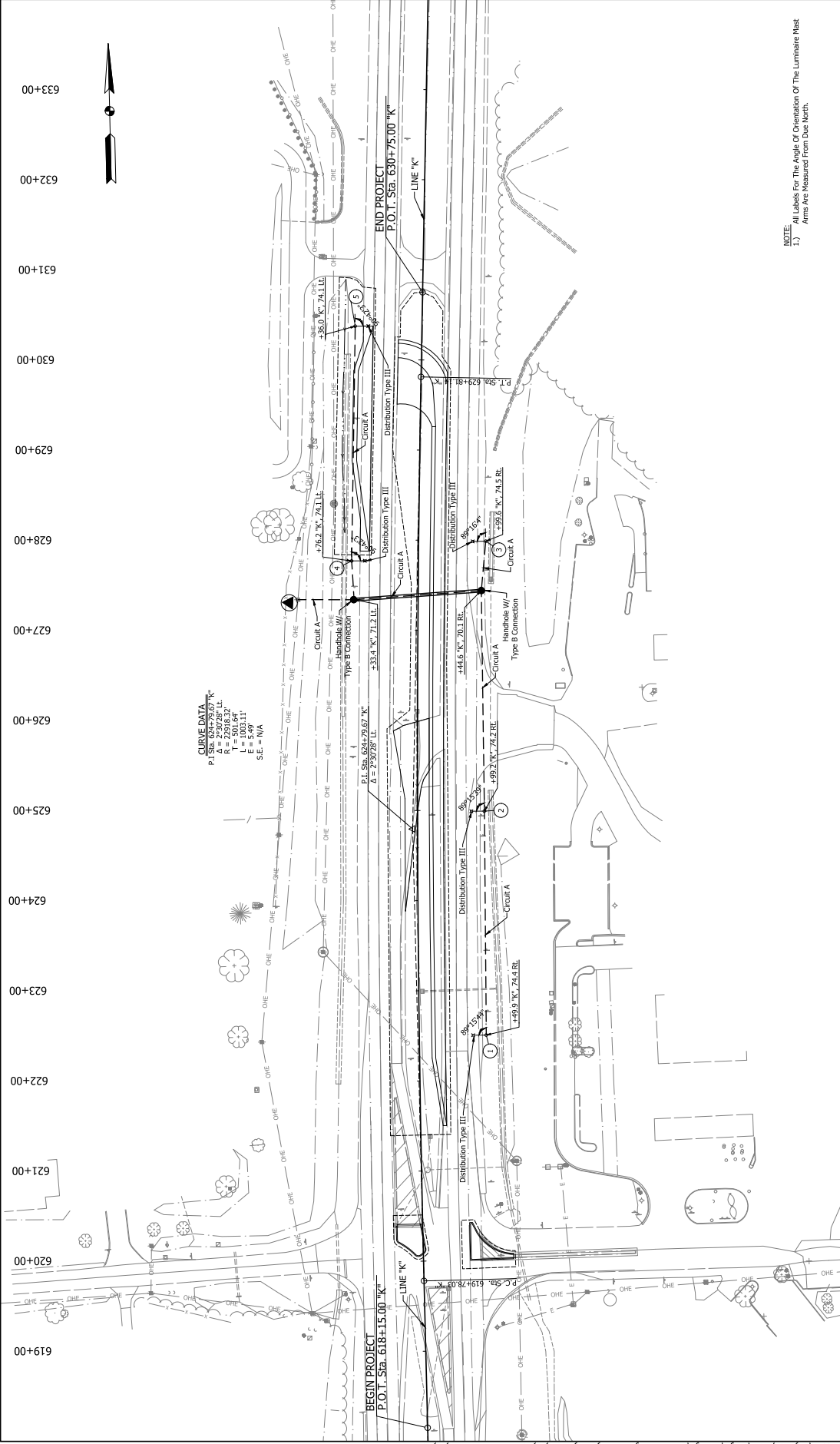
SIGN NUMBER	D3-2b
WIDTH x HGHT.	6'-0" x 4'-0"
BORDER WIDTH	0.5"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
LEGEND/BORDER	COLOR: Green
	TYPE: Reflective
	COLOR: White/Black

SIGN NUMBER	D3-2c
WIDTH x HGHT.	7'-0" x 4'-0"
BORDER WIDTH	0.5"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
LEGEND/BORDER	COLOR: Green
	TYPE: Reflective
	COLOR: White/Black

RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	RESIDENTIAL SCALE	BRIDGE FILE
	DESIGNED:	DRAWN:		PROJECT	VERTICAL SCALE
CHECKED:	JCS	CHECKED:	NR0	SURVEY BOOK	1700098
			JMT	SHEET	33
			NR0	CONTRACT	R-6072
				PROJECT	1700098
				ON SHEET	

PRELIMINARY PLANS

B-18



CURVE DATA
 P.C. Sta. 6247.97' K
 A = 2307.28' LL
 R = 74.11'
 L = 1003.11'
 S.E. = 54.9'
 S.E. = N49'

BEGIN PROJECT
 P.O.T. Sta. 618+15.00 "K"

END PROJECT
 P.O.T. Sta. 630+75.00 "K"

LEGEND
 ○ PROPOSED HANDHOLE, TYPE II POLYMER CONCRETE, 2'x3'
 X LUMINAIRE NO. 4
 --- 4-1C NO. 4 CU. IN PLASTIC DUCT IN 2" CONDUIT
 --- 4-1C NO. 4 CU. IN PLASTIC DUCT IN TRENCH
 ○ SERVICE POINT, TYPE I (120/240)
 --- INSTALL HIGH LUMEN ROADWAY LUMINAIRE AT 40 FT M.H. ON 12' FT MAST ARM ON LIGHTPOLE W/ NON-BREAKAWAY BASE ON LIGHT STANDARD FOUNDATION

NOTE:
 1.) All Labels For The Angle Of Orientation Of The Luminaire Mast Arms Are Measured From Due North.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED	INR	DRAWN: JMT
CHECKED	JCS	CHECKED: INR

INDIANA	BRIDGE FILE
DEPARTMENT OF TRANSPORTATION	DESIGNATION
LIGHTING DETAILS	SHEET
	PROJECT
	CONTRACT

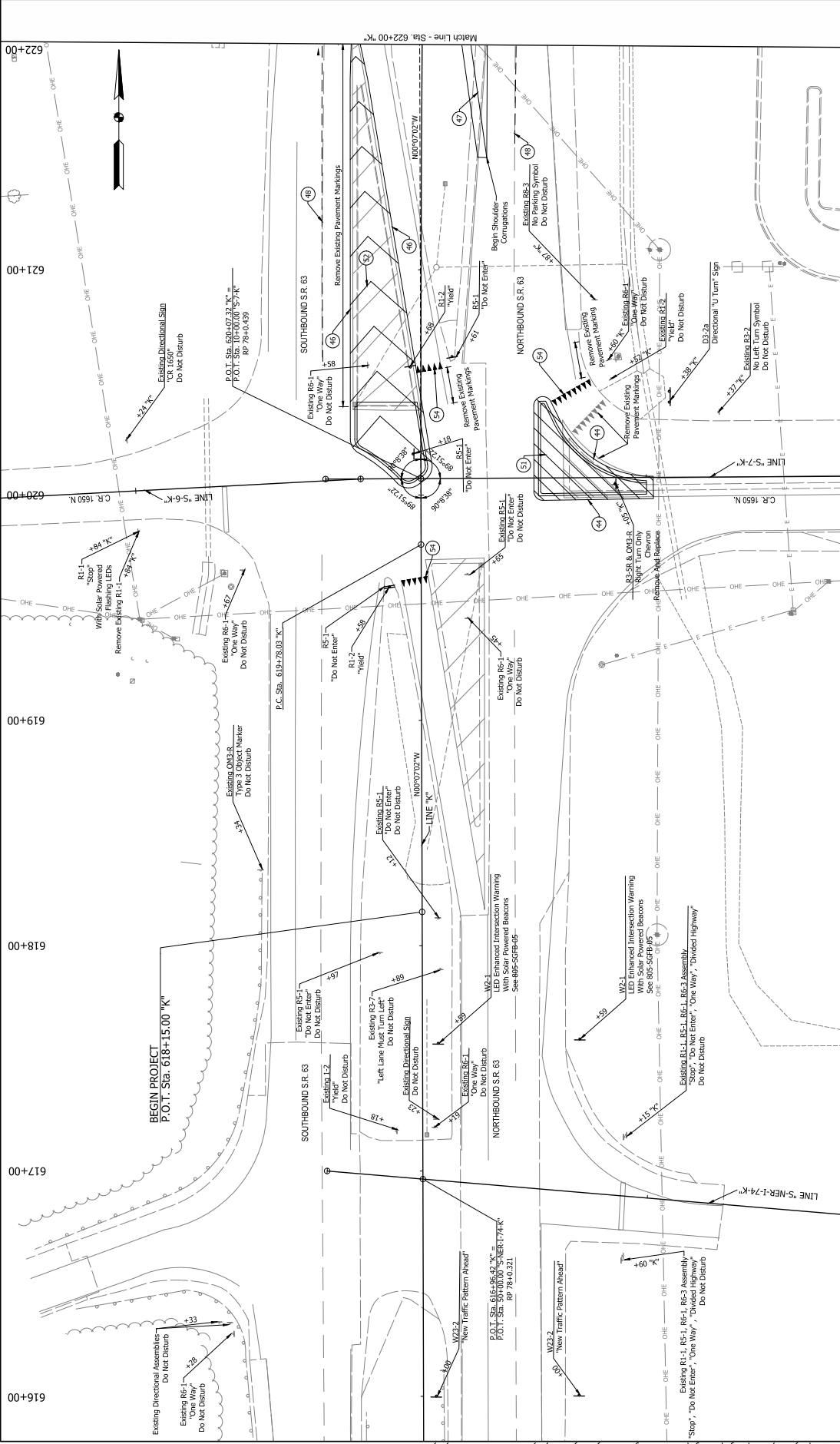
ON 5/8/17 1700098

PRELIMINARY PLANS

B-19

619+00 620+00 621+00 622+00 623+00 624+00 625+00 626+00 627+00 628+00 629+00 630+00 631+00 632+00 633+00

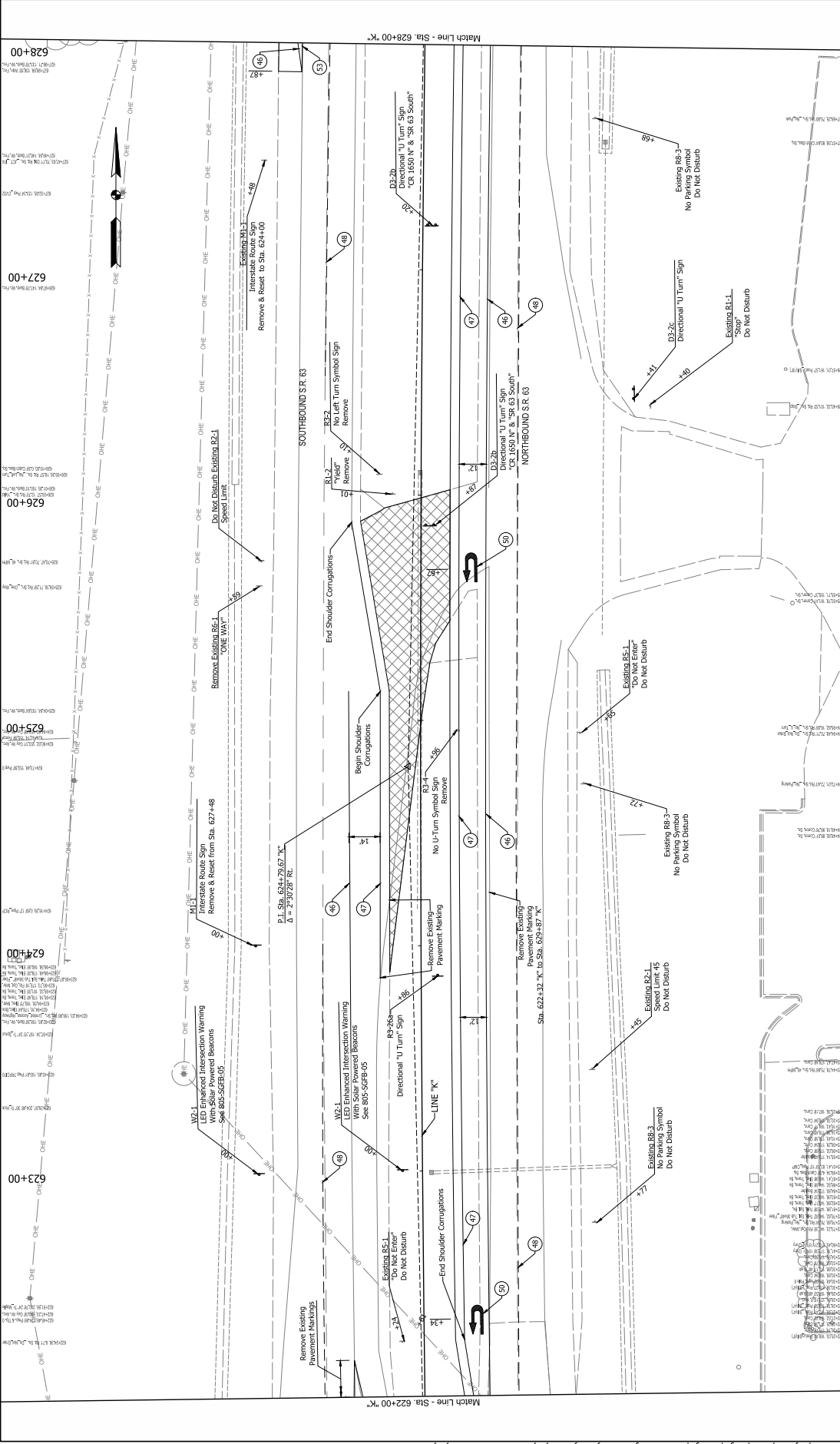
1/6/12/17 16:55:00 0000\Development\Design Drawings\6251\651.dwg Zachary Smiley Plot: 7/19/2022 3:24 PM Scale: 1/16"=1'-0"



LEGEND LINE, MULTI-COMPONENT, SOLID YELLOW, 6" TRANSVERSE MARKING, MULTI-COMPONENT, WHITE, SOLID, CROSS HATCH LINE, 12', 5" O.C. PROPOSED SHEET SIGN LINE, THERMOPLASTIC SOLID WHITE, 6" TRANSVERSE MARKING, MULTI-COMPONENT, YELLOW, SOLID, CROSS HATCH LINE, 24', 20" O.C. PROPOSED PANEL SIGN LINE, THERMOPLASTIC SOLID YELLOW, 6" TRANSVERSE MARKING, MULTI-COMPONENT, WHITE, SOLID, CROSS HATCH LINE, 24', 20" O.C. YIELD TRIANGLE, MULTI-COMPONENT, WHITE (2'-3" X 1'-6", 1" SPA.)	EXISTING SHEET SIGN PROPOSED SHEET SIGN	RECOMMENDED FOR APPROVAL: _____ DATE: _____ DESIGNER: _____ JMT DRAWN: _____ CHECKED: _____	INDIANA DEPARTMENT OF TRANSPORTATION PAVEMENT MARKINGS & SIGNAGE	BRIDGE FILE: _____ VERTICAL SCALE: 1"=20' DESIGNATION: 1700098 SHEET: 33 SURVEY BOOK: _____ CONTRACT: R-6072 PROJECT: 1700098
--	--	--	--	---

PRELIMINARY PLANS

B-20



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
APPROVED	DATE	DESIGNED	DATE	CHECKED	DATE
DESIGNED	DATE	DRAWN	DATE	IN CHARGE	DATE

INDIANA DEPARTMENT OF TRANSPORTATION	
BRIDGE FILE	170098
DESIGNATION	170098
SHEET NO.	33
SHEET TOTAL	16
PROJECT	R-6072
CONTRACT	170098

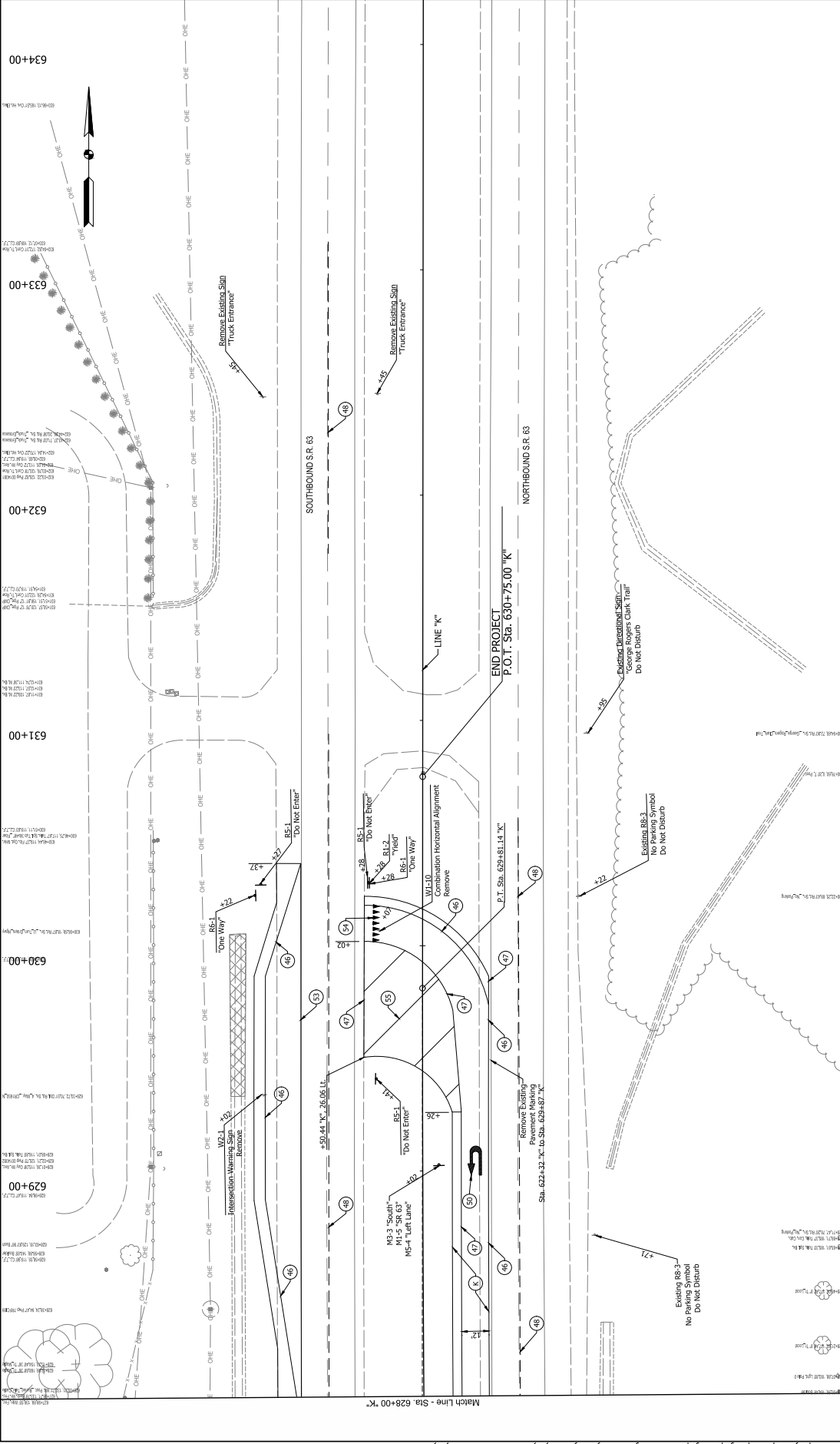
PAVEMENT MARKINGS & SIGNAGE	
EXISTING SHEET SIGN	PROPOSED SHEET SIGN
EXISTING MESSAGE MARKING	PROPOSED MESSAGE MARKING
EXISTING LANE INDICATION ARROW	PROPOSED LANE INDICATION ARROW

LEGEND

- (1) FULL DEPTH HMA PAVEMENT: SEE TYPICAL SECTIONS
- (2) LINE, THERMOPLASTIC, BROKEN WHITE, 6"
- (3) LINE, THERMOPLASTIC, SOLID WHITE, 6"
- (4) LINE, THERMOPLASTIC, SOLID YELLOW, 6"
- (5) LINE, THERMOPLASTIC, DOTTED WHITE, 6", 2" LINE W/ 6" GAP
- (6) LINE, THERMOPLASTIC, BROKEN WHITE, 6"
- (7) PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW
- (8) PROPOSED SHEET SIGN
- (9) PROPOSED PANEL SIGN

PRELIMINARY PLANS

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LEGEND (46) LINE, THERMOPLASTIC, SOLID WHITE, 6" (47) LINE, THERMOPLASTIC, SOLID YELLOW, 6" (48) LINE, THERMOPLASTIC, BROKEN WHITE, 6" (49) LINE, THERMOPLASTIC, STOP LINE, 24"	LEGEND (50) PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW (51) LINE, THERMOPLASTIC, DOTTED WHITE, 6", 2" LINE W/ 6" GAP (52) YIELD TRIANGLE, MULTI-COMPONENT, WHITE (2'-3" x 1'-6", 1" SPA.) (55) TRANSVERSE MARKING, MULTI-COMPONENT, YELLOW, SOLID, CROSS HATCH LINE, 24", 20" O.C.	EXISTING SIGN (Symbol) PROPOSED SIGN (Symbol)	RECOMMENDED FOR APPROVAL: _____ DATE: _____ DESIGN ENGINEER: _____ DESIGNED: _____ INR _____ JMT _____ CHECKED: _____ JCS _____ INNO _____	INDIANA DEPARTMENT OF TRANSPORTATION PAVEMENT MARKINGS & SIGNAGE	HORIZONTAL SCALE: 1"=20' VERTICAL SCALE: N/A SURVEY BOOK: _____ CONTRACT: R-40772
					BRIDGE FILE: _____ DESIGNATION: 1700098 SHEET: 17 PROJECT: 1700098

PRELIMINARY PLANS

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APPENDIX C: Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

March 17, 2022

Sample Early Coordination Letter

{See Attached List}

Re: Early Coordination letter
Designation Number (Des. No.) 1700098, Intersection Improvement
State Road (SR) 63 and County Road (CR) 1650 North
Highland Township, Vermillion County, Indiana

Dear Agency:

The Indiana Department of Transportation (INDOT), with federal funding, intends to proceed with a project involving the aforementioned intersection improvement project in Vermillion County. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

This project is located at the intersection of SR 63 and CR 1650 N, approximately 320 feet (ft.) north of the westbound I-74 ramp to 1,290 ft. north of the westbound I-74 ramp in Highland Township, Vermillion County, Indiana in Sections 4 and 5, Township 19 North, Range 9 West on the Perrysville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle.

SR 63 is classified as a principal arterial and the intersection of SR 63 and CR 1650 N is a two-way stop-controlled intersection. SR 63 consists of two 12 ft. wide through travel lanes with 5 ft. wide shoulders for both northbound and southbound traffic. The posted speed limit is 45 miles per hour. CR 1650 N eastbound consists of one 12 ft. wide right-turn lane and one 12 ft. wide receiving lane. The posted speed limit is 30 miles per hour. The existing intersection is not skewed more than 30 degrees. Land use within the project area is zoned as interchange development and agriculture with some suburban residential nearby.

The draft need for this project is due to the crash history on SR 63 through the project area. Crash data collected by INDOT from 2015 through 2018 indicated that this segment of SR 63 had a total of 11 crashes of which 5 crashes were "failure to yield", right-angle crashes due either to an obstructed view of opposing traffic or misjudging the gap in traffic. The data resulted in an Index of Crash Frequency of 1.53, indicating that crash frequency is higher than average to be expected for an intersection of this type. The draft purpose of this project is to reduce the crash rate on this segment of SR 63.

The preferred alternative is to construct a new U-turn median opening on SR 63 approximately 390 ft. north of the north commercial drive to the truck plaza; eliminating the existing median opening on SR 63 located across from the north commercial drive; and extending and/or raising splitter islands at CR 1650 N. New signs and streetlights will also be installed. Maintenance of traffic will be conducted by closing one lane shifting traffic to one lane travel in each direction.

No permanent and/or temporary right-of-way is anticipated to be necessary. Due to the proposed change in the facility, the public will likely have an opportunity to comment on the project per the INDOT Project Development Public Involvement Procedures Manual. Several median U-turns have been controversial due to businesses and residences concerns.

This project likely qualifies for informal consultation for the *USFWS Interim Policy for the Review of Highway Projects in the State of Indiana* (1993) and the *USFWS Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat*. The appropriate submittal will be made on the USFWS Information for Planning and Consultation website.

This project appears to fall under Category A-2, A-4, and B-2 of the *Programmatic Agreement Among the FHWA, INDOT, the Advisory Council on Historic Preservation, and the Indiana State Historic Preservation Office Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana* (2006, Rev. 2018). The appropriate submittal will be made to INDOT Cultural Resources Office to fulfill the requirements of FHWA and INDOT under Section 106 of the National Historic Preservation Act.

Please review the information herein and provide a written evaluation of any potential impacts upon resources under your jurisdiction. Please reply within 30 days of receipt of this letter or the environmental document will indicate that your agency has no comment. Should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions, please contact Susan Castle, Senior Scientist, Metric Environmental, at 317.608.2730, susanc@metricenv.com, or 6958 Hillsdale Court, Indianapolis, Indiana 46250, or Sara Heck, INDOT Project Manager, at 765.361.5231 or SHeck@INDOT.IN.gov. Your cooperation in expediting this process is appreciated.

Sincerely,

Metric Environmental, LLC



Susan Castle
Senior Scientist

cc: File No. 18-0056-3
Matt Oyer, P.E., Butler, Fairman & Seufert, Inc.
Sarah Heck, Project Manager, INDOT Crawfordsville District

Attachments: Early Coordination Recipients, Location Map, USGS Topographic Map, and Aerial Photograph

The attachments were intentionally removed. Please refer to Appendix B in the CE document.



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

Federal Highway Administration
k.carmanygeorge@dot.gov

IDEM Wellhead Proximity Determinator
Electronic Review of Location
<http://www.in.gov/idem/cleanwater/2456.htm>

Midwest Regional Office
National Park Service
Mwro_Compliance@nps.gov

Indiana Geological and Water Survey
<https://igws.indiana.edu/eAssessment>

Indiana Department of Natural Resources
Division of Fish and Wildlife
environmentalreview@dnr.in.gov

U.S. Department of Housing & Urban Develop.
Chicago Regional Office
erik.r.sandstedt@hud.gov

Indiana Department of Transportation
Crawfordsville District - Environmental
RKurtz@indot.in.gov

Indiana Department of Transportation
Crawfordsville District – INDOT PM
SHeck@indot.IN.gov

Terre Haute Area Metropolitan Organization
JWeir@Terrehauteedc.com
JMitchell@Terrehauteedc.com

Vermillion County Commissioners
Britton Luther, Tim Yocum, and RJ Dunavan
Brenda.Furry@Vermillioncounty.IN.gov

Vermillion County Surveyor
ronald.mack@vermillioncounty.in.gov

Vermillion County Emergency Management
mark.oheir@vermillioncounty.in.gov

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-24574

Request Received: March 17, 2022

Requestor: Metric Environmental
Susan Castle
6971 Hillside Court
Indianapolis, IN 46250

Project: SR 63 and CR 1650 North intersection improvement, from about 320' north to 1290' of the westbound I-74 ramp; Des #1700098

County/Site info: Vermillion

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

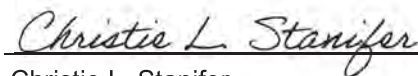
Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. The American Badger (*Taxidea taxus*), a state species of special concern, has been documented within 1/2 mile of the project area.

Fish & Wildlife Comments: Badgers are a wide ranging species that prefer an open, prairie-type habitat, with Indiana being at the eastern edge of their natural range. The range of the badger continues to expand as a result of land-use changes from forest to farmland and open pastureland. Impacts to the American badger or its preferred habitat are unlikely as a result of this project.

The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:
1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: April 13, 2022

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 08, 2022

Project Code: 2022-0017000

Project Name: Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/>

[s7process/index.html](#). This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see <https://www.fws.gov/birds/policies-and-regulations.php>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see <https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Migratory Birds
- Wetlands

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Project Code: 2022-0017000
Event Code: None
Project Name: Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana
Project Type: Road/Hwy - Maintenance/Modification
Project Description: Indiana Department of Transportation, with funding from Federal Highway Administration, intends to proceed with an intersection improvement project at State Route (SR) 63 at County Road (CR) 1650 North (N), Vermillion County, Indiana (Des. No. 1700098).

The need for this project is based on the crash history on SR 63 at the project location. Crash data collected by INDOT from 2015 through 2018 indicates that this segment of SR 63 had a total of 11 crashes during this timeframe, five of which were “failure to yield”, right-angle crashes due either to an obstructed view of opposing traffic or misjudgement of the gap in traffic. The data resulted in an Index of Crash Frequency of 1.53, indicating that crash frequency is higher than average to be expected for an intersection of this type. The purpose of this project is to reduce the crash rate on this segment of SR 63 and provide a designated location for westbound CR 1650 N to make a U-turn.

The preferred alternative is to construct a new U-turn median opening along SR 63 approximately 390 feet north of the north commercial drive to the truck plaza, eliminating the existing median opening on SR 63 located across from the north commercial drive, and extending and/or raising splitter islands at CR 1650 N. New signs and streetlights would also be installed as part of this project.

It is not anticipated that any temporary or new permanent right-of-way will be needed for project construction.

Maintenance of traffic will be conducted by closing one lane shifting traffic to one lane travel in each direction along SR 63. Traffic on SR 63 will first be merged into the median lanes and work will proceed on the outside edge of pavement. Once complete, traffic will be merged to outer lanes and work will commence on the median.

There is suitable summer habitat located within 1,000 feet of the project areas; however, no trees should need to be trimmed or removed as part of the project. No mitigation is necessary.

Based on consultation with INDOT Crawfordsville District, March 7, 2022, a review of the U.S. Fish and Wildlife Service (USFWS) database

was checked on February 9, 2022 and did not indicate the presence of endangered bat species in or within 0.5 mile of the project areas.

New signs and streetlights would be installed along the project corridor. Existing lighting to be removed or remain will be identified on the plans. Temporary lighting may be required during construction.

The project is planned to begin in spring 2024 and be completed by late summer 2024.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.12355305,-87.45181016812694,14z>



Counties: Vermillion County, Indiana

Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Insects

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

Migratory Birds

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

The birds listed below are birds of particular concern either because they occur on the [USFWS Birds of Conservation Concern](#) (BCC) list or warrant special attention in your project location. To learn more about the levels of concern for birds on your list and how this list is generated, see the FAQ [below](#). This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the [E-bird data mapping tool](#) (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found [below](#).

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.	Breeds Sep 1 to Jul 31
Bobolink <i>Dolichonyx oryzivorus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 20 to Jul 31

NAME	BREEDING SEASON
Lesser Yellowlegs <i>Tringa flavipes</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9679	Breeds elsewhere

Probability Of Presence Summary

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (l)

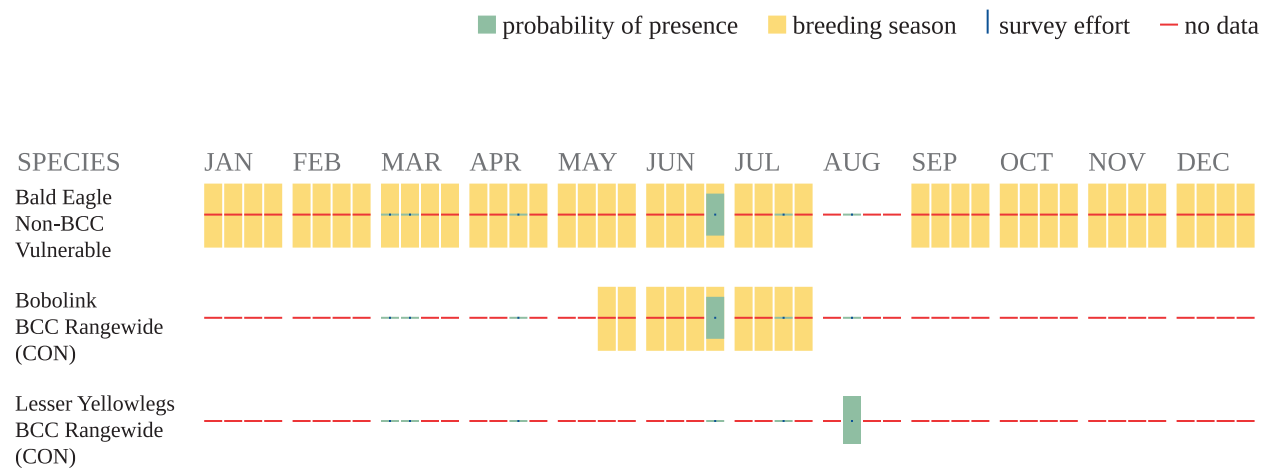
Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

No Data (—)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.



Additional information can be found using the following links:

- Birds of Conservation Concern <http://www.fws.gov/birds/management/managed-species/birds-of-conservation-concern.php>
- Measures for avoiding and minimizing impacts to birds <http://www.fws.gov/birds/management/project-assessment-tools-and-guidance/conservation-measures.php>
- Nationwide conservation measures for birds <http://www.fws.gov/migratorybirds/pdf/management/nationwidestandardconservationmeasures.pdf>

Migratory Birds FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#)

may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the migratory birds potentially occurring in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [AKN Phenology Tool](#).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go to the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering, migrating or present year-round in my project area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may refer to the following resources: [The Cornell Lab of Ornithology All About Birds Bird Guide](#), or (if you are unsuccessful in locating the bird of interest there), the [Cornell Lab of Ornithology Neotropical Birds guide](#). If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);

2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

What if I have eagles on my list?

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities,

should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

Wetlands

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

THERE ARE NO WETLANDS WITHIN YOUR PROJECT AREA.

IPaC User Contact Information

Agency: Indiana Department of Transportation
Name: Jason Damm
Address: 6958 Hillside Court
City: Indianapolis
State: IN
Zip: 46250
Email: jasond@metricenv.com
Phone: 3176052392

Lead Agency Contact Information

Lead Agency: Department of Transportation



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 08, 2022

Project code: 2022-0017000

Project Name: Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana

Subject: Concurrence verification letter for the 'Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated March 08, 2022 to verify that the **Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period

allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessments failed to detect Indiana bats, but you later detect bats prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. 1700098, Intersection Improvement Project, SR 63 at CR 1650 N, Vermillion County, Indiana

Description

Indiana Department of Transportation, with funding from Federal Highway Administration, intends to proceed with an intersection improvement project at State Route (SR) 63 at County Road (CR) 1650 North (N), Vermillion County, Indiana (Des. No. 1700098).

The need for this project is based on the crash history on SR 63 at the project location. Crash data collected by INDOT from 2015 through 2018 indicates that this segment of SR 63 had a total of 11 crashes during this timeframe, five of which were “failure to yield”, right-angle crashes due either to an obstructed view of opposing traffic or misjudgement of the gap in traffic. The data resulted in an Index of Crash Frequency of 1.53, indicating that crash frequency is higher than average to be expected for an intersection of this type. The purpose of this project is to reduce the crash rate on this segment of SR 63 and provide a designated location for westbound CR 1650 N to make a U-turn.

The preferred alternative is to construct a new U-turn median opening along SR 63 approximately 390 feet north of the north commercial drive to the truck plaza, eliminating the existing median opening on SR 63 located across from the north commercial drive, and extending and/or raising splitter islands at CR 1650 N. New signs and streetlights would also be installed as part of this project.

It is not anticipated that any temporary or new permanent right-of-way will be needed for project construction.

Maintenance of traffic will be conducted by closing one lane shifting traffic to one lane travel in each direction along SR 63. Traffic on SR 63 will first be merged into the median lanes and work will proceed on the outside edge of pavement. Once complete, traffic will be merged to outer lanes and work will commence on the median.

There is suitable summer habitat located within 1,000 feet of the project areas; however, no trees should need to be trimmed or removed as part of the project. No mitigation is necessary.

Based on consultation with INDOT Crawfordsville District, March 7, 2022, a review of the U.S. Fish and Wildlife Service (USFWS) database was checked on February 9, 2022 and did not indicate the presence of endangered bat species in or within 0.5 mile of the project areas.

New signs and streetlights would be installed along the project corridor. Existing lighting to be removed or remain will be identified on the plans. Temporary lighting may be required during construction.

The project is planned to begin in spring 2024 and be completed by late summer 2024.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

No

10. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

11. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

12. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

14. Does the project include slash pile burning?

No

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

No

16. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

17. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

18. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

19. Will the project install new or replace existing **permanent** lighting?

Yes

20. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?

Yes

21. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

22. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

23. Will the project raise the road profile **above the tree canopy**?

No

24. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

25. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

26. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

27. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

28. **Lighting AMM 2**

Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on February 24, 2022. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPaC User Contact Information

Agency: Indiana Department of Transportation
Name: Benjamin Neild
Address: 41 W. 300 N.
City: Crawfordsville
State: IN
Zip: 47933
Email: bneild@indot.in.gov
Phone: 7653615259



Organization and Project Information

Project ID: 18-0056-3
Des. ID: 1700098
Project Title: SR 63 and CR 1650 North
Name of Organization: Metric Environmental, LLC
Requested by: Susan Castle

Environmental Assessment Report

1. Geological Hazards:

- Moderate liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: High Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

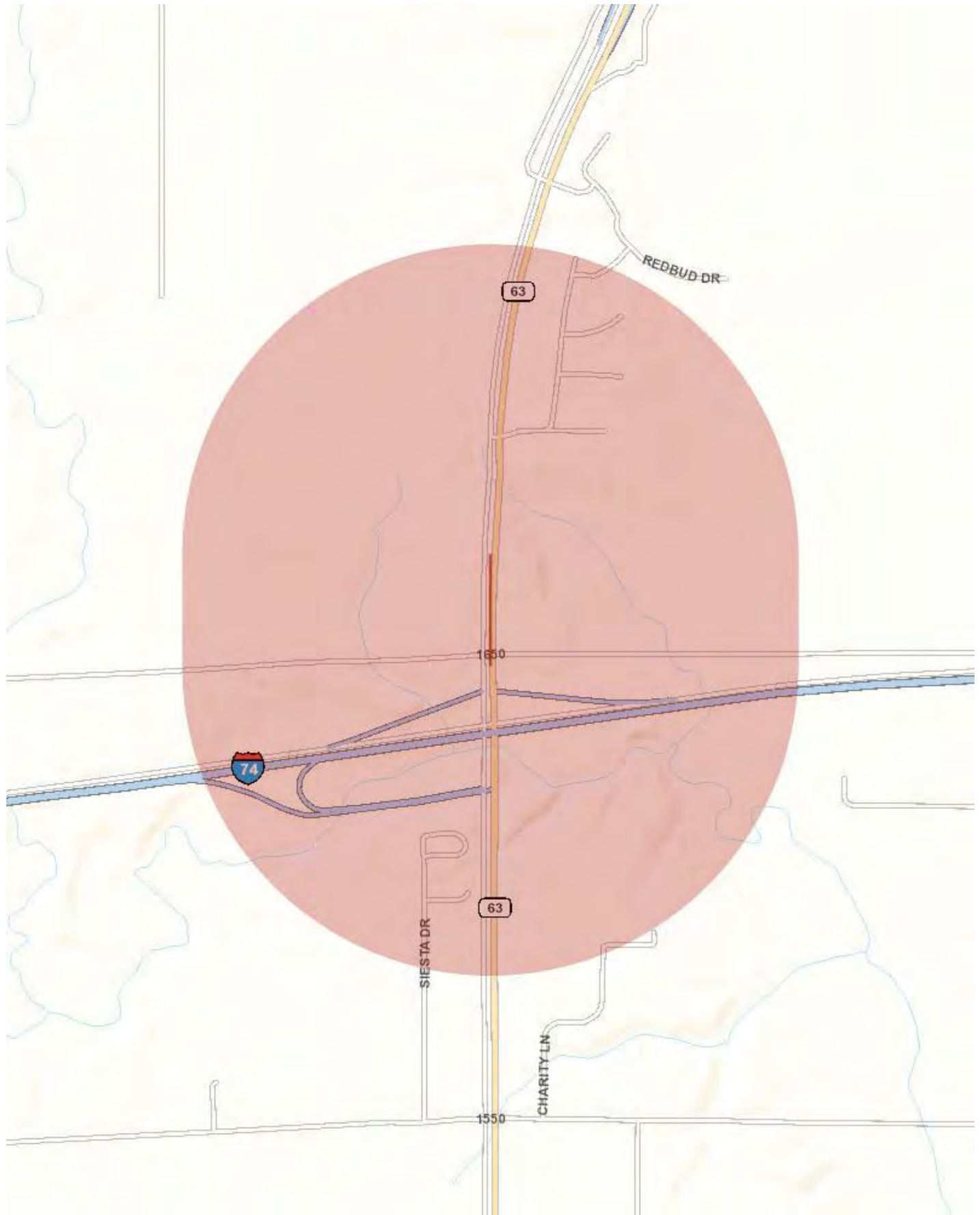
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: March 17, 2022



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

Susan Castle

Subject: FW: Agency Coordination, Des. No. 1700098, Intersection Improvement, SR 63, Vermillion County, Indiana

From: Ronald Mack <ronald.mack@vermillioncounty.in.gov>

Sent: Thursday, March 17, 2022 11:40 AM

To: Susan Castle <susanc@metricenv.com>

Cc: RJ Dunavan <rj.dunavan@vermillioncounty.in.gov>; Colby Nale <colby.nale@vermillioncounty.in.gov>

Subject: Re: Agency Coordination, Des. No. 1700098, Intersection Improvement, SR 63, Vermillion County, Indiana

Madam,

Thank you for informing us of your intended work. As Surveyooyer, I really have no qualms about the project, except that if any Cornerstone Monuments are found, the accurate recording of such be forwarded to my office. If new monuments are installed, please make sure that the concrete base for those monuments have some rebar or other detectable metal in them - all those along St. Rd. 63 that we've found have NONE, making our statue dictated mandate of verifying 5% per year extremely difficult.

I am forwarding this email on to our County Highway Supervisor for his review in case he knows of any issues you need to be concerned with.

Thank you,
Ronald A. Mack
Vermilion County Surveyor
Vermillion County Courthouse - Rm. 206
P.O. Box 280
225 Main Street
Newport, IN 47966
Ph: 765-492-5366
Email: ronald.mack@vermillioncounty.in.gov

Susan Castle

Subject: FW: SR 63 & CR 1650 Environmental Coordination, DES 1700098

From: Sliker, Andrew <ASliker@idem.IN.gov>
Sent: Tuesday, September 6, 2022 2:19 PM
To: Matt Oyer <MOyer@bfsengr.com>
Subject: RE: SR 63 & CR 1650 Environmental Coordination, DES 1700098

Good afternoon Matt,

Generally, the depth to groundwater at the site is approximately sixteen to twenty two feet below ground surface. There is no impacted in the area near the proposed construction. IDEM doesn't have any further recommendation concerning the potential to encounter/handling contamination beyond proper handling, removal, and disposal. If impact is found during construction that is beyond the SR 63 & CR 1650 intersection, please notify me.

Thanks,

Andy



Andrew Sliker
Environmental Project Manager
Petroleum Remediation Section
Petroleum Branch
Office of Land Quality
Indiana Department of Environmental Management

(317) 644-6215 | ASliker@idem.IN.gov



From: Matt Oyer <MOyer@bfsengr.com>
Sent: Tuesday, September 06, 2022 11:28 AM
To: Sliker, Andrew <ASliker@idem.IN.gov>
Cc: Heck, Sara R <SHeck@indot.IN.gov>
Subject: RE: SR 63 & CR 1650 Environmental Coordination, DES 1700098

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Good morning Andrew,

Following up on the environmental coordination below for DES 1700098. Could you let me know if you have any concerns at your earliest convenience? Your prompt input would be greatly appreciated to keep the environmental approval from delaying the project.

Sincerely,
Matt Oyer, P.E.
Transportation Engineer

Butler, Fairman & Seufert, Inc.
11 South Third Street, Suite 200 | Lafayette, IN 47901
p 765-423-5602 | f 765-742-5321
MOyer@bfsengr.com | www.BFSEngr.com



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From: Matt Oyer
Sent: Monday, August 29, 2022 2:35 PM
To: asliker@idem.in.gov
Cc: Heck, Sara R <SHeck@indot.IN.gov>
Subject: SR 63 & CR 1650 Environmental Coordination, DES 1700098

Good afternoon Andrew,

Concerning SR 63 & CR 1650 intersection improvements project, DES 1700098: We are to coordinate with you per the approved LRFI regarding the nearby LUST site to confirm that construction activities will not impact any groundwater. Please see the information below from our environmental sub and let me know if you have any questions or concerns. We would like to get your approval and add it as an attachment to the CE.

LUST Site Agency ID 55323, Pilot Travel Center #339, Located in the northeast quadrant of SR 63 and East CR 1650N. Atlas Technical Consultants submitted an updated draft of the Environmental Restrictive Covenant to IDEM (Andrew Sliker) on May 19, 2022. On June 14, 2022 Andrew responded to Atlas "IDEM has completed its review of a revised draft ERC for the Pilot Travel Center #339 (FID #17266). You can forward with recordation of the ERC. The revised draft looks ready to go. Thank you for making the changes. The record ERC must be submitted by June 28, 2022." There is no additional information on file regarding the approved ERC.

Thank you!

Sincerely,
Matt Oyer, P.E.
Transportation Engineer

Butler, Fairman & Seufert, Inc.
11 South Third Street, Suite 200 | Lafayette, IN 47901
p 765-423-5602 | f 765-742-5321
MOyer@bfsengr.com | www.BFSEngr.com



APPENDIX D:
**Section 106 of the National Historic
Preservation Act**

Minor Projects PA Project Assessment Form

Date: 4/8/2022

Project Designation Number: 1700098

Route Number: State Road (SR) 63 and County Road (CR) 1650 North

Project Description: Auxiliary Lanes, Acceleration/Deceleration or Turn Lanes from 1400 feet south of EB I-74 ramp to 1650 feet north of WB I-74 ramp, partial J-Turn

The project is located 0.1 mile north of I-74 on SR 63, with a total project length of 0.184 mile, 320 feet north of westbound I-74 ramp to 1,290 feet north of westbound I-74 ramp. It involves constructing a new U-turn median opening on SR 63 approximately 390 feet north of the north commercial drive to the truck plaza; eliminating the existing median opening on SR 63 located across from the north commercial drive; and extending and/or raising splitter islands at CR 1650 N. New signs and streetlights would also be installed. Additionally, there will be drainage work on the west side of SR 63. They will be removing an existing inlet and pipe and replacing it with a ditch. Earthwork cut and fill to be expanded in future submittal to include adequate cover over proposed storm pipe.

The purpose of the project is to reduce crashes along the SR 63 corridor near I-74, and to provide a designated location for westbound CR 1650N to make a U-turn. The project's need has been demonstrated by the significant number of crashes, including a recent fatality in 2016, at the intersection of SR 63 and CR 1650N. The existing SR 63 left-turn sight distance restrictions and low compliance with the posted speed limit has resulted in frequent accidents. The project will accomplish crash reduction by an alternative traffic operation for SR 63, especially a traffic operation that is efficient for trucks, and that would result in high driver compliance.

Currently SR 63 through the project area includes two through lanes for both northbound and southbound traffic. County Road 1650N eastbound consists of one shared left-turn, one through lane, and right turn lane, and one receiving lane; CR 1650N westbound consists of one right-turn lane and one receiving lane. The intersection of SR 63 and CR 1650N is a two-way stop-controlled intersection.

The proposed undertaking would offset the north and south left-turn lanes to be aligned directly across one another on SR 63 at its intersection with CR 1650N, and to add an improved, raised splitter island on CR 1650N east of SR 63 to prevent westbound through and left-turn movements onto SR 63. The offset left-turn lanes on SR 63 would increase the sight distance when a queue of opposing left-turn traffic is present. The east leg splitter island on CR 1650N would increase compliance of westbound traffic's right-out-only movement.

Lane closures along SR 63 would occur during construction, with one lane of travel in each direction to remain open. The project would require no new permanent or temporary right-of-way.

This project will take place within existing right-of-way (R/W) and no R/W acquisition is anticipated.

Feature crossed (if applicable): N/A

City/Township: Highland Township

County: Vermillion County

Information reviewed (please check all that apply):

- General project location map USGS map Aerial photograph Interim Report
 Written description of project area General project area photos Soil survey data
 Previously completed historic property reports Previously completed archaeology reports

Bridge Inspection Information SHAARD SHAARD GIS Streetview Imagery

Other (please specify): Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM); County GIS data (accessed via <http://thinkopengis.vermillion.in.wthtechnology.com/>); MPPA application (including maps, photographs, and project plans) sent by Metric Environmental, dated January 25, 2022 and March 29, 2022, and on file at INDOT-CRO;

Banks, Benjamin and Steven Katz

2020 Voluntary Phase Ia Archaeological Field Reconnaissance of the Proposed Transmission Line for the Jordan Creek Wind Energy Center Warren and Vermillion Counties, Indiana. Project No. 17000959, Atwell, Southfield, Michigan.

Cantin, Mark

1991 Archaeological Records Review, Reconnaissance and Recommendation, Project FTF-306 (11), Sale of Excess Land Parcel 35 along SR 63, Vermillion County, Indiana. Cultural Resource Management Report #91-75, Indiana State University Anthropology Laboratory, Terre Haute.

Please specify all applicable categories and condition(s) (applicable conditions are highlighted):

A-2. All work within interchanges and within medians of divided highways in previously disturbed soils.

A-3. Replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone or brick structures or parts therein and are in previously disturbed soils.

B-2. Installation of new lighting, signals, signage, and other traffic control devices under the following conditions *[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]*:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. **Work occurs in previously disturbed soils;** OR
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. yes no

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. yes no

Additional comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Vermillion County. No listed resources are present within 0.25 mile of the project area, a distance that would serve as an adequate area of potential effects (APE) given the scope of the project and the surrounding terrain.

The *Vermillion County Interim Report* (2000; Highland Township) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The National Register & IHSSI information is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The SHAARD information was checked against the Interim Report hard copy maps. The following IHSSI resource is recorded within 0.25 mile of the project: IHSSI #165-505-00002 (House; 1009 East 1650 North; c. 1880; "contributing")—demolished.

According to the IHSSI rating system, generally properties rated "contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "notable" might possess the necessary level of significance after further research. Properties rated "outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

Land surrounding the project area is rural with agricultural fields and scattered residential and commercial buildings present within 0.25 mile of the project. I-74 is located south of the project area; the bridges that carry SR 63 over I-74 shield all properties south of the interstate. There are 18 above-ground properties north of the interstate and within 0.25 mile of the project. These properties include a mix of commercial buildings and residential houses dating between the mid-twentieth century and the twenty-first century. However, there is no evidence that any of these properties possess the cultural significance or the material integrity to be considered eligible to the National Register for the purposes of this determination.

Based on the available information, as summarized above, no above-ground concerns exist as long as the project scope does not change.

Archaeological Resources

An INDOT Cultural Resources Office (CRO) archaeologist, who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, reviewed the proposed project area and prepared an archaeological assessment. According to SHAARD, two sites have been recorded within or adjacent to the project R/W.

Site 12Ve46 was an unidentified prehistoric camp site recorded by Jack Householder in 1959. A portion of this site was documented within the SR 63 R/W. A resurvey of the site for a proposed transmission line (Banks and Katz 2020) found that the portion of the site located within the R/W had been completely destroyed by previous highway construction, and the site was recommended to be ineligible for the National Register.

Site 12Ve393 was an unidentified prehistoric lithic scatter recorded within the SR 63 R/W during a survey for the sale of INDOT excess land (Cantin 1991). The site area was severely disturbed, and the cultural material was believed to have eroded down from a since-disturbed blufftop. The site was determined to be ineligible for the National Register, and no additional investigation was recommended.

(Cantin 1991). A resurvey of the site for a proposed transmission line (Banks and Katz 2020) found that the portion of the site located within the R/W had been completely destroyed by previous highway construction, and the site was recommended to be ineligible for the National Register.

The project area and all project activities are limited to the existing R/W of the SR 63 and CR E 1650N intersection. SR 63 consists of a four-lane divided highway flanked by ditches and utilities. The R/W has been completely disturbed. The portion of the county road within the project area has been widened and reinforced to carry the weight of heavy vehicles turning to enter an adjacent truck stop. This area has been completely disturbed.

Since the project is limited to within the previously disturbed existing R/W, and construction limits are within the existing paved roadway, there is no potential to impact intact archaeological resources. Therefore, there are no archaeological concerns provided that the scope of the project does not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, construction within 100 feet of the discovery will be stopped and the INDOT Cultural Resources Office and the Division of Historic Preservation and Archaeology will be notified immediately.

INDOT Cultural Resources staff reviewer(s): Kelyn Alexander and Matt Coon

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

APPENDIX E:
Red Flag and Hazardous Materials

Susan Castle

Subject: FW: Des 1700098 - Roadway Reconstruction - SR 63 at CR 1650 N - Vermillion County

From: Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>

Sent: Friday, March 4, 2022 10:57 AM

To: Bales, Ronald <rbales@indot.IN.gov>; Kurtz, Randy <RKurtz@indot.IN.gov>; Colin Keith <colink@metricenv.com>; Passmore, Andrew D <APassmore@indot.IN.gov>

Cc: Susan Castle <susanc@metricenv.com>; Heck, Sara R <SHeck@indot.IN.gov>

Subject: RE: Des 1700098 - Roadway Reconstruction - SR 63 at CR 1650 N - Vermillion County

Greetings -

SAM concurs with Ron that a Limited RFI would be appropriate for the scope of work described below.

Please let me know if there are any questions or concerns.

Sincerely,

Nicole

Nicole Fohey-Breting

Site Assessment & Management (SAM) Team Lead

100 North Senate Avenue N758-ES

Indianapolis, Indiana 46204

Office: (317) 416-7084

Email: NFoheyBreting@indot.in.gov

Office Hours: 8 to 4 PM



The Site Assessment and Management (SAM) Manual can be found at

<https://www.in.gov/indot/engineering/environmental-services/environmental-policy/site-assessment-and-management/>

Be sure to refer to the updated information in the SAM Manual for document preparation and submission.

From: Bales, Ronald <rbales@indot.IN.gov>

Sent: Friday, March 4, 2022 9:59 AM

To: Kurtz, Randy <RKurtz@indot.IN.gov>; Colin Keith <colink@metricenv.com>; Passmore, Andrew D <APassmore@indot.IN.gov>

Cc: Susan Castle <Susanc@metricenv.com>; Heck, Sara R <SHeck@indot.IN.gov>; Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>

Subject: RE: Des 1700098 - Roadway Reconstruction - SR 63 at CR 1650 N - Vermillion County

For median turns we have processed these as CE Level 1's due to the change in facility that would necessitate an opportunity to comment on the project per the INDOT Project Development Public Involvement Procedures Manual. Several median turns have been quite controversial due to businesses and residences concerns. Especially when you have businesses that have large vehicles that frequent their establishment.

Most of the impacts will be very minor as confined mainly to previously disturbed locations so the CE-1 would be relatively straight forward. Likely this could still fall under a Limited RFI due to minor project impacts.

If you have any questions, please let me know.

Ron Bales

INDOT-Environmental Services Division

Office: (317) 515-7908

Email: rbales@indot.in.gov



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: June 30, 2022

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Colin Keith
Metric Environmental, LLC
6958 Hillside Court
Indianapolis, Indiana 46250
colink@metricenv.com

Re: LIMITED RED FLAG INVESTIGATION
DES 1700098, State Project
Intersection Improvement Project
SR 63 at CR 1650 North
Vermillion County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The project is located at the intersection of State Road (SR) 63 and County Road (CR) 1650 N, north of Interstate (I)-74, in Vermillion County, Indiana. This project is elevated to a CE-1 due to public involvement needs, but project activities are otherwise consistent with PCE-level documentation and include limited excavation; therefore, a request to complete a Limited RFI was submitted to the Central Office on March 3, 2022 and approval was received on March 4, 2022, with concurrence by SAM on the same date. This intersection has experienced a significant number of crashes, including a recent fatality. The existing SR 63 left turn sight distance restrictions and low compliance with the posted speed limit result in frequent right-angle failure-to-yield crashes. The proposed project would construct a U-turn median opening on SR 63 approximately 390 feet north of the north commercial drive to the truck plaza, eliminate the existing median opening on SR 63 located across from the north commercial drive, and extend and/or raise existing splitter islands at CR 1650 N.

Bridge Work Included in Project: Yes No Structure #(s) _____

Is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes No Structure #(s) _____

Proposed right of way: Temporary # Acres _____, Permanent # Acres 0.2, Not Applicable (N/A)

Type and proposed depth of excavation: Excavation to a maximum depth of three feet below grade, for grading and addition of pavement.

Maintenance of traffic (MOT): Traffic will be reduced to one lane in each direction along SR 63. Traffic on SR 63 will first be merged into the median lanes and work will proceed on the outside edge of pavement. Once complete, traffic will be merged to outer lanes and work will commence on the median.

Work in waterway: Yes No Below ordinary high-water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	1*
Solid Waste Landfill	N/A	NPDES Facilities	3
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	3
Leaking Underground Storage (LUST) Sites	3	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

Leaking Underground Storage (LUST) Sites: Three (3) LUST sites, one (1) with an institutional control, are located within the 0.5 mile search radius. All three are located adjacent to the project area and are discussed below.

- Pilot Travel Center #339, 16502 N. SR 63, Agency Interest (AI) ID 55323, located at the northeast corner of SR 63 and East CR 1650 N, adjacent east of the project area. Two of the three LUST sites are noted to occur here. The property owner submitted a Closure Strategy Report in May 2018, indicating their intent to achieve No Further Action (NFA) status through the use of an environmental restrictive covenant (ERC). Data provided in support of closure included soil sampling results from monitoring well installation and 16 quarters of groundwater sampling. The data showed all contamination remaining on the site is localized to an area southeast of the building, with samples closer to the project area reporting as non-detect. Groundwater flow is to the east-southeast, away from the project area. The facility submitted a draft ERC in August 2021 that IDEM approved in an e-mail dated December 3, 2021. The ERC will allow groundwater extraction only from a specified potable water well, with other uses prohibited except for environmental investigation purposes. The ERC does not appear to have been recorded yet, so no NFA has been issued. Coordination will be conducted with the IDEM Project Manager, Andrew Sliker, asliker@idem.in.gov, before RFC.

- Covington BP #289 (16512 N. SR 63, AI ID 56543), located along northbound SR 63 near the northern end of project area. The site is an inactive gas station that still has tanks in the ground (one 15,000-gallon UST and one 8,000-gallon UST, both fiberglass tanks containing gasoline). Although listed in the GIS layer as a LUST site, no LUST-related documents were present in the VFC file for the facility. IDEM conducted an inspection on April 15, 2021, and several violations related to financial responsibility and recordkeeping/reporting requirements were noted; however, documentation reviewed does not indicate that a release occurred. No impact is expected.

NPDES Facilities: Three (3) NPDES facilities are located within the 0.5 mile search radius. The nearest NPDES facilities are both associated with Spring Creek Travel Plaza (aka Pilot Travel Center #339). The site is adjacent east of the project area and has an effective permit (permit #INRA00634) and a terminated mixed ownership permit (permit #IN0057151). Coordination with Spring Creek Travel Plaza will occur.

NPDES Pipe Locations: Three (3) NPDES pipe locations are located within the 0.5 mile search radius. The nearest NPDES pipe is approximately 0.17 mile west of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Vermillion County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high-quality natural communities is provided at https://www.in.gov/dnr/naturepreserve/files/np_vermillion.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5-mile search radius. Due to the nature of project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

HAZARDOUS MATERIAL CONCERNS:

LUST/Institutional Control Sites: Pilot Travel Center #339, 16502 N. SR 63, AI ID 55323, located at the northeast corner of SR 63 and East CR 1650 N, adjacent east of the project area. Two of the three LUST sites are noted to occur here. The property owner submitted a Closure Strategy Report in May 2018, indicating their intent to achieve NFA status through the use of an ERC. Data provided in support of closure included soil sampling results from monitoring well installation and 16 quarters of groundwater sampling. The data showed all contamination remaining on the site is localized to an area southeast of the building, with samples closer to the project area reporting as non-detect. Groundwater flow is to the east-southeast, away from the project area. The facility submitted a draft ERC in August 2021 that IDEM approved in an e-mail on December 3, 2021. The ERC will allow groundwater extraction only from a specified potable water well, with other uses prohibited except for environmental investigation purposes. The ERC does not appear to have been recorded yet, so no NFA has been issued. Coordination will be conducted with the IDEM Project Manager, Andrew Sliker, asliker@idem.in.gov, before RFC.

NPDES Facilities: Spring Creek Travel Plaza, which has an effective permit, is adjacent east of the project area. Coordination with Spring Creek Travel Plaza will occur.

ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

Nicole Fohey
Breting

Digitally signed by
Nicole Fohey-Breting
Date: 2022.07.01
11:16:18 -04'00'

INDOT ESD concurrence: _____ (Signature)

Prepared by:
Colin Keith
Project Scientist
Metric Environmental, LLC

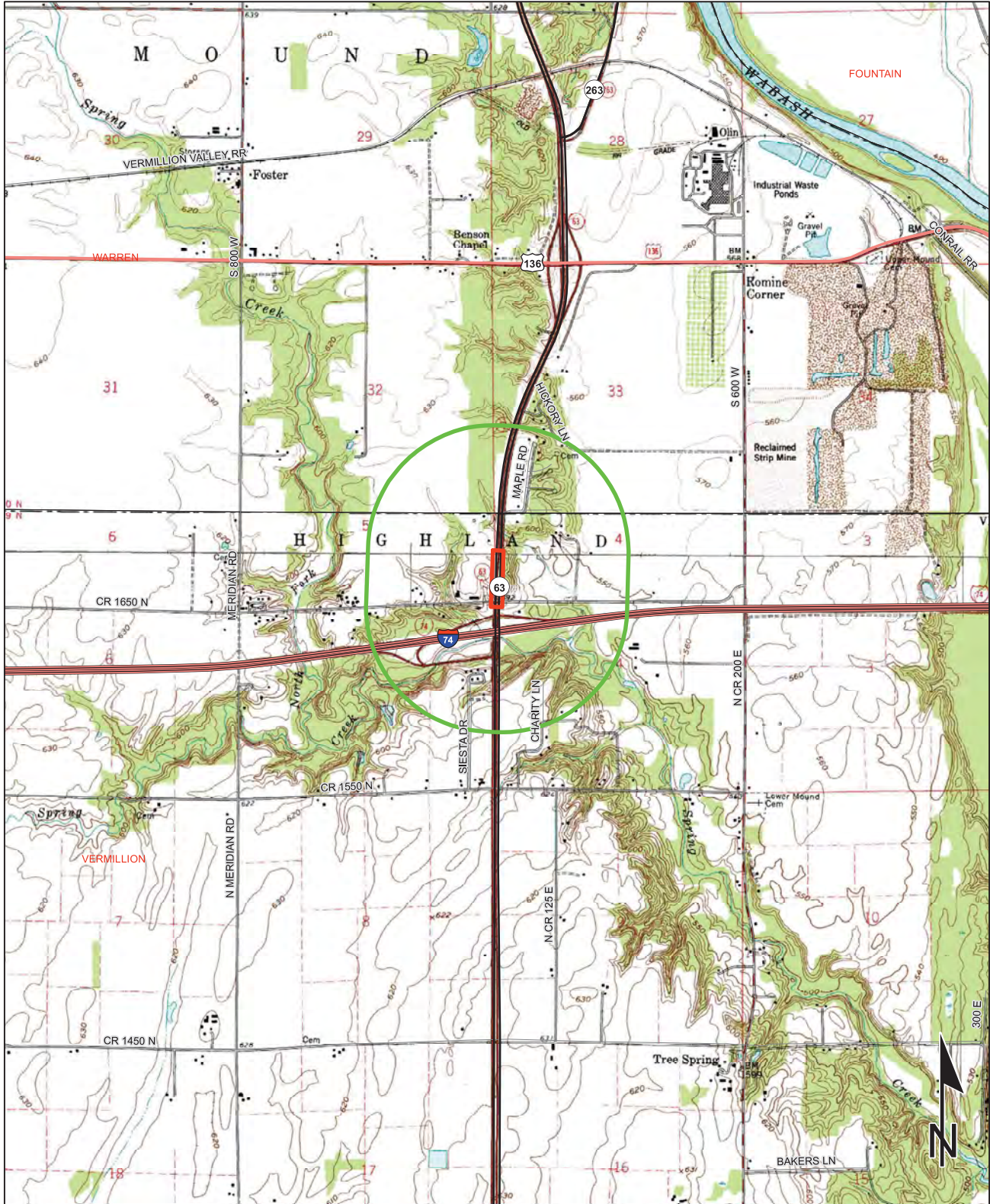
Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

Limited Red Flag Investigation - Site Location
 SR 63 at CR 1650 North
 Des. No. 1700098, Intersection Improvement Project
 Vermillion County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

COVINGTON AND PERRYSVILLE
 QUADRANGLE
 INDIANA
 7.5 MINUTE SERIES

Limited Red Flag Investigation - Hazardous Material Concerns
 SR 63 at CR 1650 North
 Des. No. 1700098, Intersection Improvement Project
 Vermillion County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation Notice_of_Contamination		Septage Waste Site		Project Area
	Construction/Demolition Site		Solid Waste Landfill		Half Mile Radius
	Infectious/Medical Waste Site		State Cleanup Site		Toll
	Leaking Underground Storage Tank		Superfund		Interstate
	Manufactured Gas Plant		Tire Waste Site		State Route
	NPDES Facilities		Underground Storage Tank		US Route
	NPDES Pipe Locations		Voluntary Remediation Program		Local Road
	Open Dump Waste Site		Waste Transfer Station		



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83

APPENDIX F:

Air Quality

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2022 - 2026

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2022	2023	2024	2025	2026	
Vermillion County																			
Indiana Department of Transportation	200502	Init.	I 74	Rest Area Modernization	Spring Creek Welcome Center Renovation	Crawfordsville	.57	NHPP	\$34,390,303.00	Statewide Consulting	PE	\$2,581,636.50	\$265,848.50				\$2,865,485.00		
										Statewide Construction	CN	\$28,369,636.20	\$3,152,181.80					\$31,521,818.00	
Performance Measure Impacted: Reliability and Freight Reliability																			
Vermillion County	38265 / 150253	Init.	VA VARI	Bridge Inspections	Countywide Bridge Inspection and Inventory Program for Cycle Years 2019-2022	Crawfordsville	0	STBG	\$174,263.00	Local Funds	PE	\$0.00	\$2,473.20		\$2,473.20				
										Local Bridge Program	PE	\$9,892.80	\$0.00		\$9,892.80				
Performance Measure Impacted: Bridge Condition																			
Comments:Include DES 160253																			
Indiana Department of Transportation	40162 / 1298389	Init.	SR 63	Bridge Deck Replacement	SB Bridge over Vermillion River 0.62 miles N of SR 234	Crawfordsville	0	NHPP	\$5,185,134.00	Bridge Construction	CN	\$4,081,867.20	\$1,020,466.80	\$5,102,334.00					
										Safety Construction	CN	\$475,440.80	\$118,860.20	\$594,301.00					
Performance Measure Impacted: Bridge Condition																			
Comments:Include DES 1601886, 1298389																			
Indiana Department of Transportation	40727 / 1700098	Init.	SR 63	Auxiliary Lanes, Accel & Decel or Turn Lanes	From 1400 ft S of EB I-74 ramp to 1650 ft N of WB I-74 ramp Partial J-Turn	Crawfordsville	.728	NHPP	\$813,286.00	Safety Construction	CN	\$475,440.80	\$118,860.20	\$594,301.00					
										Bridge ROW	RW	\$16,000.00	\$4,000.00	\$20,000.00					
Performance Measure Impacted: Reliability and Freight Reliability																			
Comments:Include DES 1700098																			
Indiana Department of Transportation	42181 / 1800145	Init.	SR 71	Small Structure Replacement	2.31 mi S of US 36	Crawfordsville	0	STBG	\$545,162.00	Bridge ROW	RW	\$16,000.00	\$4,000.00	\$20,000.00					
										Bridge Construction	CN	\$268,273.60	\$67,068.40	\$335,342.00					
										Bridge Consulting	PE	\$0.00	\$0.00	\$0.00					
Performance Measure Impacted: Bridge Condition																			
Comments:Include DES 1800145																			
Indiana Department of Transportation	42252 / 1800187	Init.	SR 63	Pavement Replacement	From 0.15 mi S to 0.23 mi N of I-74	Crawfordsville	.38	NHPP	\$7,918,893.39	Bridge ROW	RW	\$16,000.00	\$4,000.00	\$20,000.00					
										Road Construction	CN	\$5,387,416.80	\$1,346,854.20	\$30,000.00	\$6,704,271.00				
										Road ROW	RW	\$24,000.00	\$6,000.00	\$30,000.00					
										Bridge Construction	CN	\$181,377.60	\$47,844.40	\$239,222.00					
Performance Measure Impacted: Pavement Condition																			
Comments:Include DES 1900302, 1900420, 1800187																			

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

APPENDIX G: Additional Studies

Crash Analysis 2015 to 2018

ID	Master Record Number	Collision Date	Day of Week	Collision Time	Surface Condition	Light Condition	Primary Factor	Notes	Manner of Collision	Travel Dir 1	Travel Dir 2	Travel Dir 3	Roadway ID	Location	Incapacitating	Non-Incapacitating	Property Damage Only
1	902760862	8/18/16	Thurs	12:17 PM	DRY	DAYLIGHT	FAILURE TO YIELD RIGHT OF WAY	D1's view was obstructed by a semi in the opposing LT lane	LEFT TURN	NB-WB	SB		SR 63	CR 1650 N			1
2	902618108	12/17/15	Thurs	5:43 PM	DRY	DAWN/DUSK	FOLLOWING TOO CLOSELY	D1 was watching SB traffic and struck V2	REAR END	EB	EB		SR 63	I-74 EB Ramps			1
3	902900687	4/15/17	Wed	5:10 PM	WET	DAYLIGHT	FAILURE TO YIELD RIGHT OF WAY	V1 changed lanes approaching CR 1650 N and struck V2	SAME DIRECTION SIDESWIPE	NB	NB		SR 63	CR 1650 N			1
4	902998829	9/17/17	Sun	5:33 PM	WET	DAYLIGHT	DRIVER ASLEEP OR FATIGUED	D1 was fatigued and didn't stop in time after cresting a hill south of the I-74 EB Ramps	REAR END	NB	NB		SR 63	I-74 EB Ramps			1
5	902548503	9/22/15	Tues	8:58 PM	DRY	DARK (LIGHTED)	FAILURE TO YIELD RIGHT OF WAY	V1 made an illegal left turn and misjudged the gap, striking V2; V3 was then struck as well	RIGHT ANGLE	WB-SB	SB	SB	SR 63	CR 1650 N		1	
6	903172725	6/19/18	Tues	3:56 PM	DRY	DAYLIGHT	FOLLOWING TOO CLOSELY	D1 thought V2 had already turned	REAR END	WB	WB		SR 63	I-74 WB Ramps			1
7	903175587	6/24/18	Sun	8:29 PM	DRY	DAYLIGHT	CELL PHONE USAGE	No Reason Given	REAR END	EB	EB		SR 63	I-74 EB Ramps			1
8	902961849	7/11/17	Tues	2:50 PM	WET	DAYLIGHT	RAN OFF ROAD RIGHT	While attempting the turn, D1 lost control of V1 on wet pavement	RAN OFF ROAD	SB-WB			SR 63	I-74 WB Ramps			1
9	902478469	7/10/15	Fri	6:50 PM	DRY	DAYLIGHT	FAILURE TO YIELD RIGHT OF WAY	D1 said V2 (a semi) didn't use its turn signal; V1 pulled up to the right of V2 and was struck as V2 made a wide right turn	SAME DIRECTION SIDESWIPE	WB-NB	WB-NB		SR 63	CR 1650 N			1
10	902687120	4/11/16	Mon	6:04 PM	DRY	DAYLIGHT	FAILURE TO YIELD RIGHT OF WAY	D1's view was obstructed by a semi in the opposing LT lane; V2 was excessively speeding	LEFT TURN	NB-WB	SB		SR 63	CR 1650 N	1 (Fatal)		
11	902716377	5/31/16	Tues	4:12 PM	DRY	DAYLIGHT	IMPROPER TURNING	D1 thought V2 (a semi) was turning left; V1 pulled up to the right of V2 and was struck as V2 made a wide right turn	SAME DIRECTION SIDESWIPE	WB-NB	WB-NB		SR 63	CR 1650 N			1
12	902967950	7/24/17	Mon	3:46 PM	DRY	DAYLIGHT	UNSAFE BACKING	D1 had missed his turn onto EB I-74, was backing up to turn around, and didn't see V2	BACKING CRASH	NB	NB		SR 63	CR 1650 N			1
13	902521945	9/12/15	Sat	11:58 AM	DRY	DAYLIGHT	FOLLOWING TOO CLOSELY	D1 said V2 (a semi) didn't use its turn signal; V1 pulled up to the right of V2 and was struck as V2 made a wide right turn	SAME DIRECTION SIDESWIPE	WB-NB	WB-NB		SR 63	CR 1650 N			1
14	902570225	11/23/15	Mon	8:50 AM	DRY	DAYLIGHT	OTHER (DRIVER) - EXPLAIN IN NARRATIVE	D2 ran off the road to avoid a collision with V1 (a semi) that had turned right from CR 1650 N and was attempting a U-turn from the right lane at the crossover just north of CR 1650 N	OTHER	NB-SB	NB		SR 63	Driveway N of CR 1650 N			1
15	902918077	4/23/17	Sun	5:15 PM	DRY	DAYLIGHT	FAILURE TO YIELD RIGHT OF WAY	V1 made an illegal thru movement; D1 said she didn't see V2	RIGHT ANGLE	WB	NB		SR 63	CR 1650 N		1	
16	902929800	5/24/17	Wed	12:23 AM	DRY	DARK (LIGHTED)	FAILURE TO YIELD RIGHT OF WAY	There were conflicting statements in the narrative, but V2 and W1 agreed that V1 stopped at the stop sign and then failed to yield to V2	RIGHT ANGLE	EB	NB		SR 63	CR 1650 N			1
17	902671804	3/17/16	Thurs	4:45 PM	DRY	DAYLIGHT	OVERCORRECTING/OVERSTEERING	D1 turned too sharply and struck V2, which was waiting to turn right	OPPOSITE DIRECTION SIDESWIPE	SB-EB	WB-NB		SR 63	CR 1650 N			1
18	902910126	4/7/17	Fri	5:33 PM	DRY	DAYLIGHT	FOLLOWING TOO CLOSELY	V2 started to turn and then stopped abruptly; V1 couldn't stop in time and struck V2	REAR END	EB-SB	EB-SB		SR 63	I-74 EB Ramps			1

Crash Analysis

- SR 63 at the Drive just N of CR 1650 N
- SR 63 at CR 1650 N
- SR 63 at the I-74 WB Ramps
- SR 63 at the I-74 EB Ramps

Data: 7/1/2015 to 6/30/2018

All Intersections				
Manner of Collision	Total	Incapacitating	Non-Incapacitating	Prop. Damage Only
LEFT TURN	2	1	0	1
REAR END	5	0	0	5
RIGHT ANGLE	3	0	2	1
SAME DIRECTION SIDESWIPE	4	0	0	4
RAN OFF ROAD	1	0	0	1
OTHER	1	0	0	1
BACKING CRASH	1	0	0	1
OPPOSITE DIRECTION SIDESWIPE	1	0	0	1
Grand Total	18	1	2	15

SR 63 at Drive N of CR 1650 N				
Manner of Collision	Total	Incapacitating	Non-Incapacitating	Prop. Damage Only
OTHER	1	0	0	1
Grand Total	1	0	0	1

SR 63 at I-74 WB Ramps				
Manner of Collision	Total	Incapacitating	Non-Incapacitating	Prop. Damage Only
REAR END	1	0	0	1
RAN OFF ROAD	1	0	0	1
Grand Total	2	0	0	2

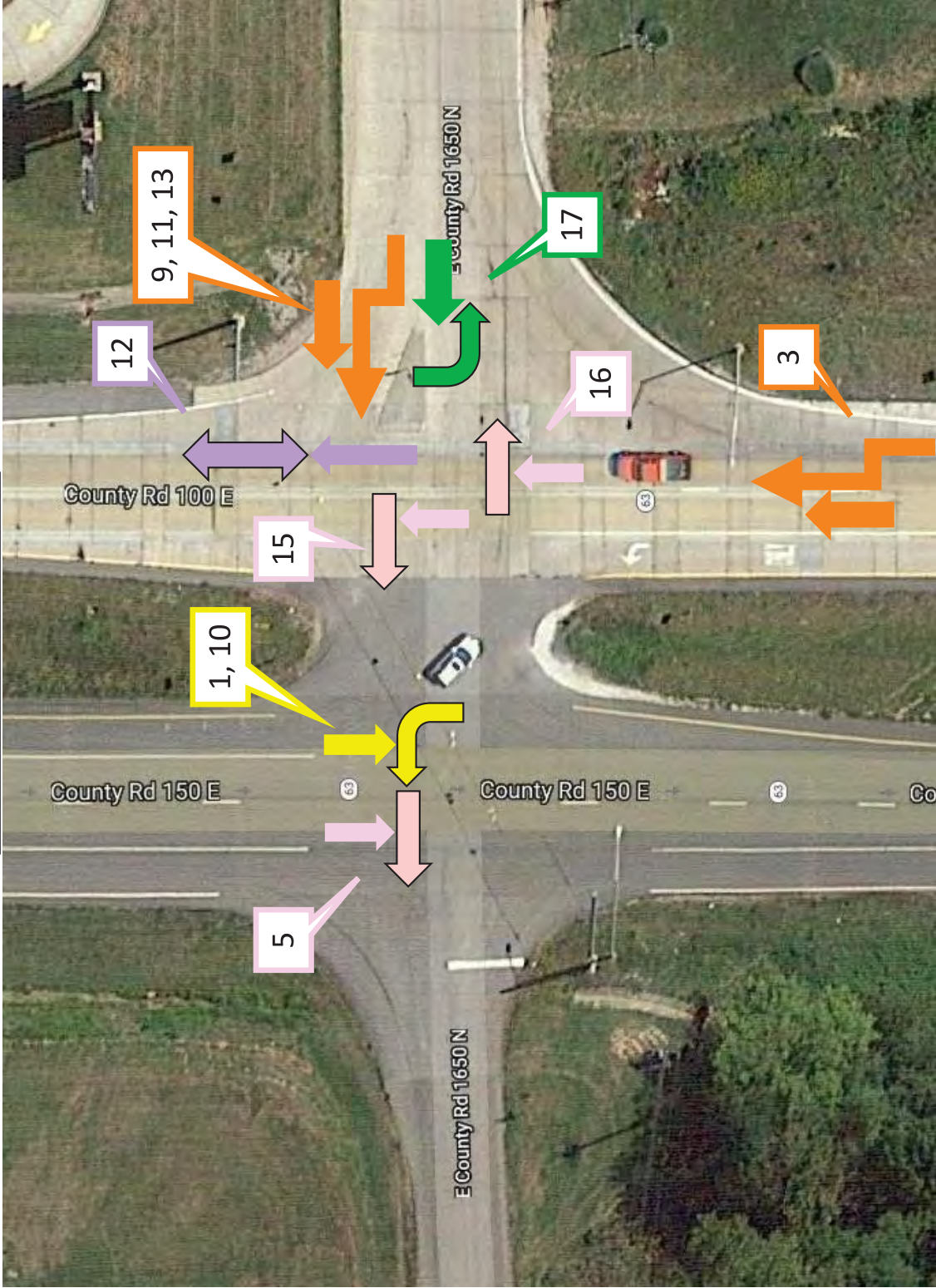
SR 63 at CR 1650 N				
Manner of Collision	Total	Incapacitating	Non-Incapacitating	Prop. Damage Only
LEFT TURN	2	1	0	1
RIGHT ANGLE	3	0	2	1
SAME DIRECTION SIDESWIPE	4	0	0	4
BACKING CRASH	1	0	0	1
OPPOSITE DIRECTION SIDESWIPE	1	0	0	1
Grand Total	11	1	2	8

SR 63 at I-74 EB Ramps				
Manner of Collision	Total	Incapacitating	Non-Incapacitating	Prop. Damage Only
REAR END	4	0	0	4
Grand Total	4	0	0	4

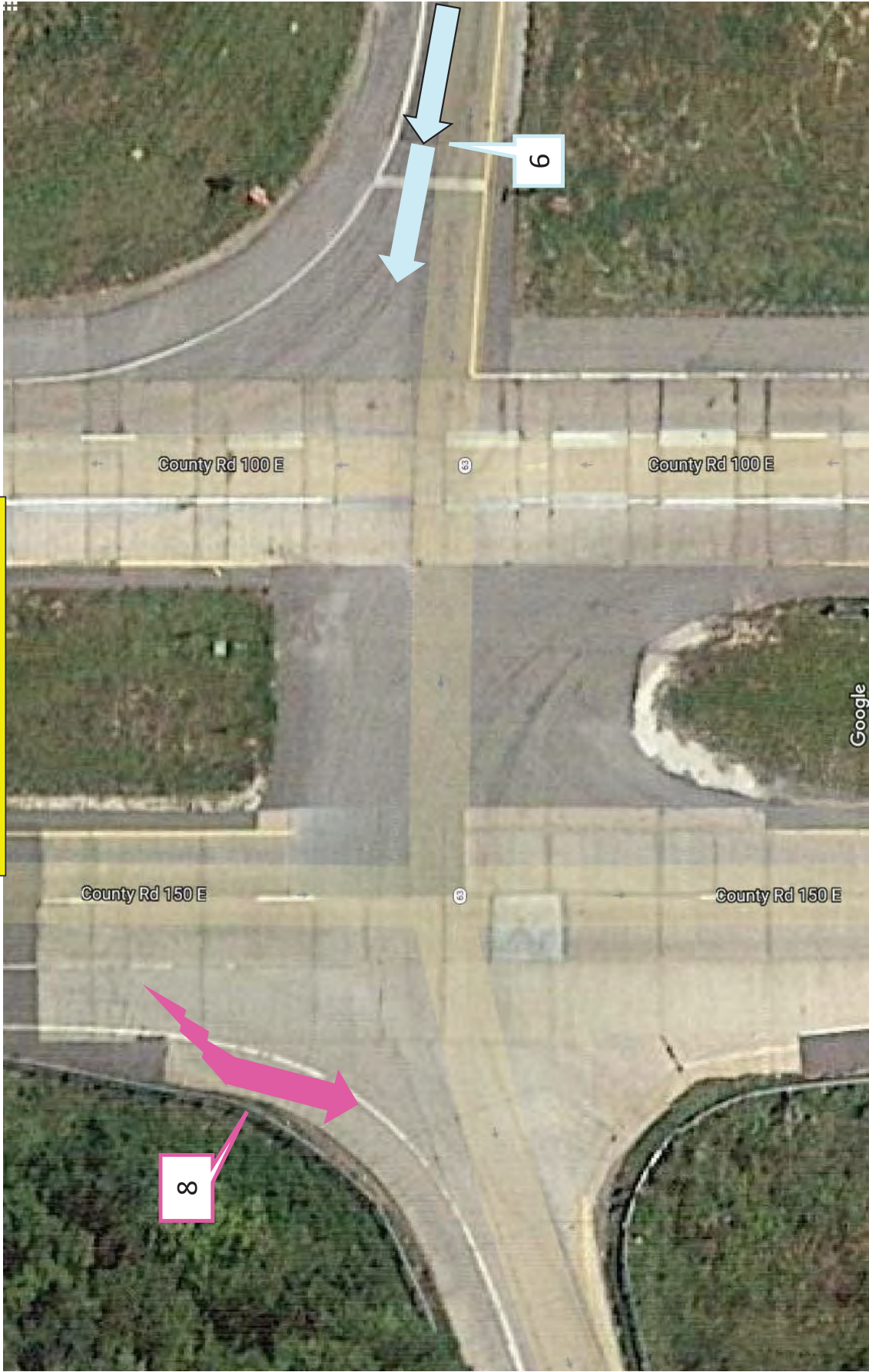
SR 63 at Drive just N of CR 1650 N



SR 63 at CR 1650 N



SR 63 at the I-74 WB Ramps



SR 63 at the I-74 EB Ramps



Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800103	1800103	Vermillion	Blanford Community Park
1800144	1800144	Vermillion	Fairview Park Ballfield
1800208	1800208	Vermillion	Millers Park, Miller Community Park
1800286	1800286	Vermillion	Perrysville Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.