

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 10 / Starke County
Designation Number(s):	2100231
Project Description/Termini:	SR 10 hot mix asphalt (HMA) overlay maintenance project from 0.58 mile east of the west junction of SR 39 to the east junction of SR 39.

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

SFM 09/25/2025
INDOT DE Initials and Date

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Jennifer Graf, Parsons

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

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County StarkeRoute SR 10Des. No. 2100231

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on August 21, 2023, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, pages G-1 to G-2.

The project meets the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: LaPorteLocal Name of the Facility: SR 10Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

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PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The need for the project is due to the deteriorating condition of the pavement on SR 10. According to the Pavement Scoping Application Report last edited on January 15, 2021, the existing pavement on SR 10 between the west junction of SR 39 and the east junction of SR 39 is rutted, cracked, and potholed (Appendix I, pages I-2 to I-5). According to INDOT's Pathway data the International Roughness Index (IRI) for the pavement within the project area averages 114 (acceptable condition) (Appendix I, page I-1). The IRI is the standard method used to measure the smoothness or ride-quality of pavement by measuring the vertical movement of a vehicle's suspension as it travels over a section of road. A lower IRI value indicates a smoother road, while a higher value indicates a rougher road. The Federal Highway Administration (FHWA) considers pavement with an IRI rating of less than 95 to be in "good" condition, an IRI rating from 96 to 170 to be in "acceptable" condition, and an IRI rating exceeding 170 to be in "poor" condition.

Purpose: The purpose of the project is to address the deterioration and restore the pavement, provide a ride quality with an IRI value of less than 70, and extend the service life of this section of SR 10 by at least nine years.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: StarkeMunicipality: Wayne TownshipLimits of Proposed Work: On SR 10 approximately 0.58 mile east of the west junction of SR 39 to the east junction of SR 39.Total Work Length: 1.62 Mile(s)Total Work Area: 4.5 Acre(s)Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes¹

No

Date:

X

¹If an IAD is required, a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

INDOT, with funding from FHWA, plans to proceed with an HMA overlay and preventative maintenance project along SR 10, in Starke County, Indiana (Appendix B, page B-1).

Location: The project is located in Sections 14, 15, 16, 21, 22, and 23, Township 32 North, Range 3 West in Wayne Township (Appendix B, page B-2). The project begins on SR 10 approximately 0.58 mile east of the west junction of SR 39 and extends eastward to the east junction of SR 39, a distance of approximately 1.62 miles.

Existing Conditions: The project setting is rural and adjacent land uses include maintained grass areas, roadside ditches, agricultural fields, residential properties, a commercial property, a railroad corridor, a recreational trail, and a trailhead parking area. The Hoosier Valley Railroad Museum (HVRM) owns the railroad corridor and parcel containing the trailhead parking area. The North Judson Erie (NJE) Trail follows parallel to the railroad. The Prairie Trails Club manages and maintains the NJE Trail. An aerial map and photographs of the project corridor are provided in Appendix B, pages B-3 to B-6.

SR 10

Within the project area, SR 10 is classified as a *Rural Major Collector*. It has two 11.0-foot-wide travel lanes (one eastbound and one westbound) and outside shoulders that vary in width from 3.0 feet to 10.0 feet wide. The pavement is in poor condition, exhibiting rutting, cracking, and potholes. There are no sidewalks or streetlights along SR 10 within the project area.

Utilities

Overhead and underground utilities are present along SR 10 (Appendix B, page B-21). Along the north side of SR 10, Mediacom cable TV and Northern Indiana Public Service Company (NIPSCO) electric lines run overhead on utility poles and Brightspeed fiber optic cable lines run underground. Along the south side of SR 10, Brightspeed fiber optic cable lines run overhead. Two Buckeye

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pipelines run parallel underground on the north side of the railroad corridor.

Culverts and Bridges

There are four culverts within the project corridor. A table of the existing culverts and proposed work is provided in Appendix I, page I-10

- INDOT Culvert No. CLV-010-075-45.65 (CLV-29637) is a 15.0-inch diameter corrugated metal pipe (CMP) located approximately 0.62 mile west of the SR 10/SR 39 E intersection. Based on visual observation during the site visit conducted by Parsons on May 3, 2023, this culvert is compressed and restricted by dirt and debris (Appendix B, B-5).
- INDOT Culvert No. CLV-010-075-45.91 is a 15.0-inch diameter CMP located approximately 0.38 mile west of the SR 10/SR 39 E intersection.
- INDOT Culvert No. CV 010-075-45.50 (Pine Creek) is a 5.0-foot diameter CMP located approximately 0.78 mile west of the SR 10/SR 39 E intersection.
- INDOT Culvert No. CLV-010-075-44.94 (CLV-29641) is a 30.0-inch diameter CMP located approximately 1.35 miles west of the SR 10 and SR 39 E intersection.

A three-span, concrete slab bridge (INDOT Structure No. 010-75-06886 A; National Bridge Inventory (NBI) No. 002960) is located at the project's eastern limits and carries SR 10 over Bogus Run. The structure is 80.0 feet long and 47.5 feet wide (out-to-out coping) and consists of two 12.0-foot-wide travel lanes (one eastbound and one westbound), and 6.0-foot-wide outside shoulders. Guardrails are located at the bridge approaches and concrete barriers are present on both sides of the bridge deck. According to the INDOT *Routine Bridge Inspection Report* dated January 24, 2024, the bridge is in good condition and has a sufficiency rating of 98.1 (Appendix I, pages I-6 to I-9).

Preferred Alternative:

SR 10

The preferred alternative will remove 2.0 inches of existing pavement and replace it with 2.0 inches of HMA. Center line and shoulder rumble strips will be included in the new pavement. At the NJE Trail crossing of SR 10, an Americans with Disabilities Act (ADA) compliant 10.0-foot-wide pedestrian crossing will be constructed. The ADA treatments will include detectable warning surfaces on the north and south sides of the crosswalk and 24.0-inch-wide white painted lines. Plan sheets for the project are provided in Appendix B, pages B-8 to B-23.

Utilities

The Brightspeed fiber optic cable lines located underground along the north side of SR 10, in the vicinity of INDOT Culvert No. CLV-010-075-45.65, will be relocated. In order to accommodate the culvert replacement (detailed below), the fiber optic cable lines will be lowered an additional 11.0 feet in the same location.

Culvert and Bridges

Of the four culverts in the project corridor, three (CLV-010-075-45.91, CV 010-075-45.50, and CLV-010-075-44.94) will remain place. INDOT Culvert No. CLV-010-075-45.65 (CLV-29637) is compressed and restricted by dirt and debris. This culvert will be replaced with a larger CMP culvert (63-foot-long, 24-inch diameter) to improve drainage in the project area (Appendix B, page B-21 and Appendix I, page I-10). Riprap will be placed at the north culvert outfall for scour protection.

Sections of the approach guardrail for INDOT Structure No. 010-75-06886 A over Bogus Run will be removed and replaced (Appendix B, page B-22).

Right-of-Way

Approximately 1.7 acres of right-of-way (ROW) acquisition will be required near the NJE Trail crossing and trailhead parking area (Appendix B, page B-7). Of this total, 1.08 acres consist of new permanent ROW; and approximately 0.62 acre of ROW will be re-acquisitioned.

Maintenance of Traffic

During construction, SR 10 will be closed, a 28-mile detour along SR 39, US 421, and SR 8 will be utilized (Appendix B, pages B-16 and B-17). Access to residential and commercial driveways and the NJE Trail crossing along SR 10 will be maintained during construction. Please see the **Maintenance of Traffic (MOT) During Construction** section of this Categorical Exclusion (CE) document for more information.

Environmental Impacts

The project will impact approximately 0.005 acre of terrestrial habitat.

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Logical Termini/Independent Utility: The project is a single and complete project that is not reliant on any other projects to be constructed. The project will not restrict consideration of alternatives for other reasonably foreseeable transportation improvements. This project begins approximately 0.58 mile east of the west junction of SR 39 and terminates at the east junction of SR 39, which are the logical termini for the project. Therefore, this project meets FHWA criteria for independent utility and logical termini.

The preferred alternative will address the deterioration and restore the pavement, improve roadway rideability to an IRI value of less than 70, and extend the service life of SR 10 by at least nine years. Therefore, the preferred alternative meets the purpose and need.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

No-Build Alternative: The no-build alternative would leave the pavement and one compressed culvert as they currently exist. The no-build alternative would not address the identified roadway pavement deficiencies, and the roadway pavement and culvert conditions would continue to deteriorate. Although the no-build alternative would not incur construction costs or environmental impacts, it would not meet the project's purpose and need. Therefore, the no-build alternative was dismissed from further consideration.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway	<u>SR 10</u>			
Functional Classification:	<u>Rural Major Collector</u>			
Current ADT:	<u>4,703</u>	VPD (2025)	Design Year ADT:	<u>5,460</u> VPD (2040)
Design Hour Volume (DHV):	<u>470</u>	Truck Percentage (%)	<u>16</u>	
Designed Speed (mph):	<u>55</u>	Legal Speed (mph):	<u>55</u>	

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Travel		Travel	
Pavement Width:	11.0	ft.	11.0	ft.
Shoulder Width:	3.0 to 10.0	ft.	3.0 to 10.0	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:	<input type="checkbox"/>	Urban	<input type="checkbox"/>	Suburban	<input checked="" type="checkbox"/>	Rural
Topography:	<input checked="" type="checkbox"/>	Level	<input type="checkbox"/>	Rolling	<input type="checkbox"/>	Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): 010-75-06886 A/ NBI No. 002960 Sufficiency Rating: 98.1 - INDOT Routine Bridge Inspection Report dated January 24, 2024
(Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Three-span continuous reinforced concrete slab	N/A
Number of Spans:	3	N/A
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft.	N/A ft.
Curb to Curb Width:	44.0 ft.	N/A ft.
Outside to Outside Width:	47.5 ft.	N/A ft.
Shoulder Width:	6.0 ft. Outside	N/A ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

A desktop review of INDOT's Bridge and Drainage Asset Viewer identified one bridge and four culverts located within the project area:

Bridge: The bridge over Bogus Run (INDOT Structure No. 010-75-06886 A; NBI No. 002960) is a three-span concrete slab bridge that was built in 1987 (Appendix I, pages I-6 to I-9). The structure is 80 feet long and 47.5 feet wide (out-to-out coping) and consists of two 12-foot-wide travel lanes (one eastbound and one westbound), and 6.0-foot-wide outside shoulders. Guardrails are located at the bridge approaches and concrete barriers are present on both sides of the bridge deck. This bridge is not listed or eligible for listing on the National Register of Historic Places (NRHP). Sections of the approach guardrail will be removed and replaced.

Culverts: A table of the existing culverts and proposed work is provided in Appendix I, page I-10.

- INDOT Culvert No. CLV-010-075-45.65 (CLV-29637) is a 15-inch diameter CMP located approximately 0.62 mile west of the SR 10/SR 39 E intersection. This culvert is compressed and restricted by dirt and debris (Appendix B, B-5). These deficiencies are causing drainage problems in the project area. As part of this project, this structure will be replaced with a larger CMP culvert (63-foot-long, 24-inch diameter) to improve drainage in the project area (Appendix B, B-21 and Appendix I, I-10).
- INDOT Culvert No. CLV-010-075-45.91 is a 15-inch diameter CMP located approximately 0.38 mile west of the SR 10/SR 39 E intersection. This culvert will not be impacted as part of the project.
- INDOT Culvert No. CV 010-075-45.50 (Pine Creek) is a 5-foot diameter CMP located approximately 0.78 mile west of the SR 10/SR 39 E intersection. This culvert will not be impacted as part of the project.
- INDOT Culvert No. CLV-010-075-44.94 (CLV-29641) is a 30-inch diameter CMP located approximately 1.35 mile west of the SR 10/SR 39 E intersection. This culvert will not be impacted as part of the project.

The culverts are not listed or eligible for listing on the NRHP. No other bridges or small structures are located within the project area.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		<input checked="" type="checkbox"/>
Is a temporary roadway proposed?		<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	
Provisions will be made to accommodate any local special events or festivals.		<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?		<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?		<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)		<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	<input checked="" type="checkbox"/>	

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require closure of SR 10 for approximately four to six months. A 28-mile detour along SR 39, US 421, and SR 8 will be provided, which will add approximately 30 minutes of travel time (Appendix B, pages B-16 and B-17). Access to residential driveways, one commercial business, and the NJE Trail crossing along SR 10 will be maintained during construction. During construction, pavement will be removed and replaced one lane at a time. This will provide a lane in which local traffic will be able to access residential and commercial driveways within the construction zone.

Pedestrian and bicycle access to the NJE Trail will be maintained during construction, with only limited, short-term, temporary shifts (to the east or west) of the trail crossing to accommodate the roadway paving and installation of the ADA-compliant crossing of SR 10.

The MOT for the project will not impact any local special events or festivals. The Indiana Festivals website (<https://indianafestivals.org/>) was checked on August 8, 2025, by Parsons to determine community events and festivals planned near the project area. The main festival in North Judson is the Mint Festival, which celebrates the town's connection to mint farming. The festival occurs in June at Lane Street and North Judson Park. Festival participants will be able to access Lane Street and North Judson via other local roads.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE

Engineering: \$ 60,000 (2026)	Right-of-Way: \$ 30,000* (2026)	Construction: \$ 766,000 (2026)
	*State funds will be used for ROW acquisition.	\$ 250,000 (2028)

Anticipated Start Date of Construction: Fall 2026

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.0	0.0
Commercial	0.0	0.0
Agricultural	0.0	0.0
Forest	0.0	0.0
Wetlands	0.0	0.0
Other: Recreational Trail, Railroad Corridor, and Trailhead Parking Area	1.08	0.0
Other:		
TOTAL	1.08	0.0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing state-owned ROW along SR 10 ranges from approximately 35.0 to 60.0 feet wide north and south from the centerline of SR 10 (Appendix B, page B-23). The existing ROW consists of the roadway, bridge, grassy sideslopes, and roadside ditches.

Approximately 1.7 acres of ROW will be required for the project near the NJE Trail crossing and trailhead parking area (Appendix B, page B-7). Of this total, 1.08 acres consist of new permanent ROW and approximately 0.62-acre consists of re-acquired ROW. The land to be acquired and re-acquired consists primarily of grassy shoulders and a paved pathway connecting the NJE Trail to SR 10.

The property lines of the railroad corridor and the parcel containing the trailhead parking area extend to the SR 10 centerline. INDOT does not own ROW near the NJE Trail crossing and trailhead parking area. Therefore, INDOT will acquire land along this section of SR 10 to provide ROW for SR 10, utilities and culvert replacement.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on August 21, 2003 (Appendix C, pages C-1 to C-3).

Agency	Sent	Response	Appendix
FHWA	8/21/2023	N/A	N/A
US Department of Housing and Urban Development	8/21/2023	N/A	N/A
US Fish and Wildlife Service (USFWS)	8/21/2023	8/22/2023	C-12
National Park Service	8/21/2023	N/A	N/A
US Army Corps of Engineers (USACE)	8/21/2023	N/A	N/A
Natural Resources Conservation Services (NRCS)	8/21/2023	9/28/2023	C-11
Indiana Geological and Water Survey (IGWS)	8/21/2023	8/21/2023	C-7 to C-8
Indiana Department of Environmental Management (IDEM)	8/21/2023	8/30/2023	C-9 to C-10
Indiana Department of Natural Resources – Division of Fish and Wildlife (IDNR-DFW)	8/21/2023	9/20/2023	C-4 to C-6

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Agency	Sent	Response	Appendix
INDOT– LaPorte District, Project Manager and Environmental Manager	8/21/2023	No Response Received	N/A
Starke County Commission	8/21/2023	No Response Received	N/A
Starke County Council	8/21/2023	No Response Received	N/A
Starke County Emergency Management	8/21/2023	No Response Received	N/A
Starke County Health Department	8/21/2023	No Response Received	N/A
Starke County Highway Department	8/21/2023	No Response Received	N/A
Starke County Planning Commission	8/21/2023	No Response Received	N/A
Starke County Sheriff's Office	8/21/2023	No Response Received	N/A
Starke County Surveyor	8/21/2023	No Response Received	N/A
Knox Community School Corporation	8/21/2023	No Response Received	N/A
North Judson-San Pierre School Corporation	8/21/2023	No Response Received	N/A
Town of North Judson Town Council	8/21/2023	No Response Received	N/A
Town of North Judson Police Department	8/21/2023	No Response Received	N/A
Town of North Judson Fire Department	8/21/2023	No Response Received	N/A
Town of North Judson Water/Sewer/Street Department	8/21/2023	No Response Received	N/A
HVRM	8/21/2023	No Response Received	N/A
Prairie Trails Club	8/21/2023	8/21/2023	C-13 to C-16

All applicable recommendations are included in the **Environmental Commitments** section of this CE document.

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SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

X

Impacts

Yes	No
	X

Total stream(s) in project area: 336.0 Linear feet Total impacted stream(s): 0.0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Bogus Run	Perennial	127.0	0.0	Located approximately 200 feet west of the SR 10 and SR 39 intersection at the eastern end of the project corridor, flows north eventually draining into the Kankakee River, likely a water of the US (Appendix F, pages F-14 and F-16).
Maciolek Ditch	Intermittent	209.0	0.0	Located west of the SR 10 and SR 39 intersection at the eastern end of the project corridor, flows west draining into Bogus Run, likely a water of the US (Appendix F, pages F-14 and F-16).

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review and an aerial map of the project area (Appendix B, page B-3), there are two streams, rivers, watercourse, or other jurisdictional features within or adjacent to the project area. That number was confirmed during site visits conducted by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023.

A *Wetland Delineation and Waters Report* was approved by INDOT Ecology, Waterway Permitting, and Stormwater Office (EWPSO) on February 27, 2024. Please refer to Appendix F, pages F-3 to F-19 for the *Wetland Delineation and Waters Report*. It was determined that two water features, Bogus Run and Maciolek Ditch, are within or adjacent to the project area and that both are likely waters of the US. The USACE makes all final determinations regarding jurisdiction.

- Bogus Run crosses SR 10 near SR 39 (Appendix F, pages F-14 and F-16). It is a perennial, poor-quality stream that flows north under INDOT Structure No. 010-75-06886 A for approximately 127.0 linear feet through the project area. Bogus Run is not listed as a Federal Wild and Scenic River, a State Natural, Scenic, and Recreational River, nor is it located within 2.0 miles of any such resource. Bogus Run is not a National Rivers Inventory waterway or navigable waterway, but eventually outfalls into the Kankakee River, which is a traditionally navigable waterway. The SR 10 resurfacing will terminate at the western approach of INDOT Structure No. 010-75-06886 A over Bogus Run. Sections of the approach guardrail for this bridge will be removed and replaced (Appendix B, page B-22). No work will be conducted in Bogus Run; therefore, no impacts are expected.
- Maciolek Ditch runs along the north side of SR 10 near SR 39 (Appendix F, pages F-14 and F-16). It is an intermittent, poor-quality stream that flows west for approximately 209.0 linear feet before draining into Bogus Run. Maciolek Ditch is not listed as a Federal Wild and Scenic River, a State Natural, Scenic, and Recreational River, nor is it located within 2.0 miles of such resources. Maciolek Ditch is not a navigable waterway or a National Rivers Inventory waterway; however, it drains into Bogus Run, which eventually outfalls into the Kankakee River, a traditionally navigable waterway. Maciolek Ditch is located on the east side of Bogus Run, which is outside of the construction area. Therefore, no impacts to Maciolek Ditch are expected.

Bogus Run and Maciolek Ditch will be labeled "Do Not Disturb" on the project plans. This is included as a firm commitment in the **Environmental Commitments** section of this CE document.

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Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review and the aerial map of the project area (Appendix B, page B-3), there are no lakes or other open water features within or adjacent to the project area. This was confirmed during site visits conducted by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023. Therefore, no impacts are expected.

A *Wetland Delineation and Waters Report* was approved by INDOT EWPSO on February 27, 2024. Please refer to Appendix F, pages F-3 to F-19 for the *Wetland Delineation and Waters Report*. It was determined that no lakes or other open water features are within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

Wetlands	Presence	Impacts	
		Yes	No
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Total wetland area: 0.108 Acre(s) Total wetland area impacted: 0.0 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland A	PEM	0.022	0.0	Located along the north side of Maciolek Ditch between Bogus Run and the SR 10 and SR 39 intersection at the eastern end of the project corridor. Classified as a poor-quality wetland with a hydrological connection to Maciolek Ditch; therefore, likely a water of the US (Appendix F, page F-16).
Wetland B	PEM	0.029	0.0	Located along the south side of SR 10 between Bogus Run and the SR 10 and SR 39 intersection at the eastern end of the project corridor. Classified as a poor-quality wetland with a hydrological connection to Bogus Run; therefore, likely a water of the US (Appendix F, page F-16).
Wetland C	PEM	0.057	0.0	Located along the south side of SR 10 and the west side of Bogus Run at the eastern end of the project corridor. Classified as a poor-quality wetland with a hydrological connection to Bogus Run; therefore, likely a water of the US (Appendix F, page F-16).

Wetlands (Mark all that apply)	Documentation	ESD Approval Dates
Wetland Determination	<input checked="" type="checkbox"/>	February 27, 2024
Wetland Delineation	<input checked="" type="checkbox"/>	February 27, 2024
USACE Isolated Waters Determination	<input type="checkbox"/>	

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Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review and the aerial map of the project area (Appendix B, B-3), three wetlands were identified within or adjacent to the project area. That number was confirmed during site visits conducted by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023.

A *Wetland Delineation and Waters Report* was approved by INDOT EWPSO on February 27, 2024. Please refer to Appendix F, pages F-3 to F-19 for the *Wetland Delineation and Waters Report*. It was determined that three likely jurisdictional wetlands, totaling 0.108 acre are within the project area. The USACE makes all final determinations regarding jurisdiction.

- Wetland A is an emergent wetland located within a roadside ditch in the northeast quadrant of SR 10 and Bogus Run (Appendix F, page F-16). Approximately 0.022 acre of the wetland is within the project area. It is dominated by invasive species and is classified as a poor-quality wetland. Wetland A derives water from the adjacent roadway and Maciolek Ditch. The wetland immediately abuts Maciolek Ditch, an intermittent stream that drains to Bogus Run, eventually draining into the Kankakee River, a traditionally navigable waterway (TNW). Due to the direct connectivity to a TNW, it is likely that Wetland A is a water of the US and under the jurisdictional authority of the USACE. The SR 10 pavement resurfacing will terminate at the Bogus Run bridge's western approach and no bridge work will occur. Portions of the approach guardrail for the bridge over Bogus Run will be temporarily removed and reset in place. Wetland A is located on the east side of Bogus Run, which is outside of the construction area. Therefore, no impacts to Wetland A are anticipated.
- Wetland B is an emergent wetland located within a roadside ditch in the southeast quadrant of SR 10 and Bogus Run (Appendix F, page F-16). Approximately 0.029 acre of the wetland is within the project area. It is dominated by invasive species and is classified as a poor-quality wetland. Wetland B abuts Bogus Run, a perennial stream, which continues north and eventually drains into the Kankakee River, a TNW. Due to direct connectivity to a TNW, it is likely that Wetland B is a water of the US and under the jurisdictional authority of the USACE. The SR 10 pavement resurfacing will terminate at the Bogus Run bridge's western approach and no bridge work will occur. Portions of the approach guardrail for the bridge over Bogus Run will be temporarily removed and reset in place. Wetland B is located on the east side of Bogus Run, which is outside of the construction area. Therefore, no impacts to Wetland B are anticipated.
- Wetland C is an emergent wetland located within a roadside ditch in the southwest quadrant of SR 10 and Bogus Run (Appendix F, page F-16). Approximately 0.057 acre of the wetland is within the project area. Wetland C is dominated by invasive species and is located within INDOT's maintained ROW. It was classified as a poor-quality wetland. Wetland C abuts Bogus Run, a perennial stream that drains into the Kankakee River, a THW navigable waterway. Due to direct connectivity to a TNW, it is likely that Wetland C is a water of the US and under the jurisdictional authority of the USACE. The SR 10 resurfacing will terminate at the Bogus Run bridge's western approach and no bridge work will occur. The approach guardrail at the structure's western approach will be removed and reset in place. Wetland C is located outside of the construction area. Therefore, no impacts to Wetland C are anticipated.

Wetlands A, B, and C will be labeled "Do Not Disturb" on the project plans. This is included as a firm commitment in the **Environmental Commitments** section of this CE document.

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Terrestrial Habitat

Presence

☒

Impacts

Yes

NO

☒

☐

Total terrestrial habitat in project area: 9.0 Acres Total tree clearing: 0.0 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a project area map (Appendix B, page B-3), and site visits conducted by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023, there are three types of terrestrial habitat within the project area; maintained grass areas, roadside ditches, and row-crop fields (Appendix B, pages B-4 and B-5). The roadside ditches are prominently dominated by elder (*Sambucus nigra*), amur honeysuckle (*Lonicera maackii*), canary honeysuckle (*Phalaris arundinacea*), and field bindweed (*Convolvulus arvensis*). No tree species are present within the project area.

The project will impact 0.005 acre of terrestrial habitat due to the installation of a culvert and riprap (Appendix B, page B-21). Construction is anticipated to be completed within four to six months, after which the terrestrial habitat will be restored in accordance with INDOT Standard Specifications for revegetation of disturbed areas. Impacts to terrestrial habitat are unavoidable because the project cannot meet the purpose and need without the disturbance. Tree clearing or trimming are not required for this project.

IDNR-DFW responded to early coordination on September 20, 2023, with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts (Appendix C, pages C-4 to C-6). All applicable recommendations are included in the **Environmental Commitments** section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed

Section 7 informal consultation completed (IPaC cannot be completed)

Section 7 formal consultation Biological Assessment (BA) required

Yes

☒

No

☐

☐

☒

☐

☒

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)

State species (not bird) found in project area (based upon consultation with IDNR)

Yes

☒

No

☐

☐

☒

Migratory Birds

Known usage or presence of birds (i.e. nests)

State bird species based upon coordination with IDNR

Yes

☐

No

☒

☐

☒

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the Red Flag Investigation (RFI) report (Appendix E, pages E-1 to E-5) completed by Parsons on October 24, 2023, the IDNR Starke County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated September 20, 2023, the Natural Heritage Program database was checked and no state or federally threatened, endangered, or rare plant or animal species have been documented to occur within the vicinity of the project area to date (Appendix C, pages C-4 to C-6). An INDOT 0.5-mile bat review occurred on August 2, 2023. The review did not indicate the presence of endangered bats within 0.5 mile of the project area.

Project information was submitted through the USFWS Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-13 to C-26). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to the paragraph below.

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and NLEB, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 9, 2023, and based on the responses provided, the project was found to "may effect, not likely to adversely affect" the Indiana bat and/or the NLEB (Appendix C, pages C-27 to C-37). INDOT reviewed and

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verified the effect finding on August 17, 2023, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. The Avoidance and Minimization Measures (AMMs) for this project are General AMM 1, requiring all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs and Lighting AMM 1, requiring temporary lighting be directed away from suitable habitat during the active season. These AMMs are included as firm commitments in the **Environmental Commitments** section of this document.

A bridge inspection occurred on January 24, 2024, on INDOT Structure No. 010-75-06886 A over Bogus Run and there were no signs of bats or birds found using the structure (Appendix I, page I-9). USFWS Bridge/Structure Assessments are only valid for two years. If construction begins after January 24, 2026, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the **Environmental Commitments** section of this document.

The IPaC official species list also indicated three additional species present within the project area: the proposed federally endangered tricolored bat (*Perimyotis subflavus*), the non-essential experimental population whooping crane (*Grus americana*), and the candidate monarch butterfly (*Danaus plexippus*). No critical habitat for the tricolored bat is located within the project area and the project will not jeopardize its continued existence. Since these species are not federally listed as threatened or endangered, and are not afforded protection under the Endangered Species Act, further coordination with USFWS related to the tricolored bat, whooping crane, or monarch butterfly is not required.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Indiana Karst Region

Karst features identified within or adjacent to the project area

Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): _____

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review, the project is located outside the designated Indiana Karst region as outlined in the most current Protection of Karst Features during Project Development and Construction. According to the project area topo map (Appendix B, page B-2) and the IndianaMap portal (<https://www.indianamap.org/>), there are no karst features within or adjacent to the project area. In the early coordination response received on August 21, 2023, IGWS did not indicate that karst features exist in the project area (Appendix C, pages C-7 and C-8). The response noted that the project area has a high liquefaction potential, a high potential for bedrock resources, and low potential for sand and gravel mineral resource sites in the area. Evidence of active or abandoned petroleum exploration wells were noted. The IGWS response was communicated with the designer on August 22, 2023. No impacts are expected.

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SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

X

Impacts

Yes	No
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer: The project is located in Starke County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water: In an early coordination letter dated August 30, 2023, IDEM stated that the project is not located within a wellhead or source water area (Appendix C, pages C-9 and C-10). No impacts are expected.

Water Wells: The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 15, 2024, by Parsons. One well is located near County Road (CR) 300 West and SR 10 approximately 200 feet south of the INDOT ROW. This well will not be affected because the work in this area consists of removing 2.0 inches of existing pavement and replacing it with 2.0 inches of HMA. Center line and shoulder rumble strips will be included in the new pavement. All project work will occur within INDOT ROW. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

Urban Area Boundary : Based on a desktop review of IDEM Urban Area Boundary and Municipal Separate Storm Sewer Systems (MS4) website (www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/) conducted by Parsons on March 15, 2024, this project is not located in an Urban Area Boundary. No impacts are expected.

Public Water System: Based on a desktop review of utility engineering records, the aerial map of the project area (Appendix B, page B-3), and site visits conducted by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023, no public water systems were identified. Therefore, no impacts are expected.

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Floodplains

Project located within a regulated floodplain
 Longitudinal encroachment
 Transverse encroachment
 Homes located in floodplain within 1000' up/downstream from project

Presence

X

Impacts

Yes	No
	X

If applicable, indicate the Floodplain Level?

Level 1 ☒ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of the IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) conducted on April 24, 2024, this project is located within a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, pages F-1 and F-2). An early coordination letter was sent to the local Floodplain Administrator on August 21, 2023. The Floodplain Administrator did not respond within the 30-day time frame. This project qualifies as a Category 1 per the current INDOT CE Manual, which states:

- Although this project involves work within the horizontal limits of the 100-year floodplain, no work is being performed below the 100-year flood elevation and as a result this project does not encroach upon the base floodplain.

Farmland

Agricultural Lands
 Prime Farmland (per NRCS)

Presence

X

Impacts

Yes	No
	X

Total Points (from Section VII of CPA-106/AD-1006*)

N/A

**If 160 or greater, see CE Manual for guidance.*

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and site visits by Parsons on May 3, 2023, and American Structurepoint on August 8, 2023, there is farmland, as defined by the Farmland Protection Policy Act adjacent to the project. The project will not convert any farmland because the work includes replacing 2.0 inches of existing pavement with 2.0 inches of HMA, constructing an ADA compliant pedestrian crossing on SR 10, replacing one culvert under SR 10, and replacing guardrails in their current location. No ROW will be acquired from adjacent farmland.

An early coordination letter was sent to NRCS on August 21, 2023, and a response letter dated September 28, 2023, stated that the project will not cause a conversion of prime farmland (Appendix C, page C-11). No alternatives, other than those previously discussed in this document, will be investigated without reevaluating impacts to prime farmland.

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SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s) <div style="border: 1px solid black; padding: 2px;">Category B, Type 9; and Category A, Types 4 and 9</div>	INDOT Approval Date(s) <div style="border: 1px solid black; padding: 2px;">February 26, 2025</div>	N/A <div style="border: 1px solid black; width: 40px; height: 20px; margin: 2px;"></div>																												
Full 106 Effect Finding No Historic Properties Affected <input type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect <input type="checkbox"/>																															
Eligible and/or Listed Resources Present NRHP Building/Site/District(s) <input type="checkbox"/> Archaeology <input type="checkbox"/> NRHP Bridge(s) <input type="checkbox"/>																															
Documentation Prepared (mark all that apply) <table border="0" style="width: 100%; margin-top: 5px;"> <tr> <td style="width: 45%;">APE, Eligibility and Effect Determination</td> <td style="width: 5%; text-align: center;"><input type="checkbox"/></td> <td style="width: 20%;">ESD Approval Date(s)</td> <td style="width: 30%;">SHPO Approval Date(s)</td> </tr> <tr> <td>800.11 Documentation</td> <td style="text-align: center;"><input type="checkbox"/></td> <td></td> <td></td> </tr> <tr> <td>Historic Properties Report or Short Report</td> <td style="text-align: center;"><input type="checkbox"/></td> <td></td> <td></td> </tr> <tr> <td>Archaeological Records Check and Assessment</td> <td style="text-align: center;"><input type="checkbox"/></td> <td></td> <td></td> </tr> <tr> <td>Archaeological Phase Ia Survey Report</td> <td style="text-align: center;"><input checked="" type="checkbox"/></td> <td>February 26, 2025</td> <td>N/A</td> </tr> <tr> <td>Archaeological Phase Ic Survey Report</td> <td style="text-align: center;"><input type="checkbox"/></td> <td></td> <td></td> </tr> <tr> <td>Other:</td> <td style="text-align: center;"><input type="checkbox"/></td> <td></td> <td></td> </tr> </table>				APE, Eligibility and Effect Determination	<input type="checkbox"/>	ESD Approval Date(s)	SHPO Approval Date(s)	800.11 Documentation	<input type="checkbox"/>			Historic Properties Report or Short Report	<input type="checkbox"/>			Archaeological Records Check and Assessment	<input type="checkbox"/>			Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	February 26, 2025	N/A	Archaeological Phase Ic Survey Report	<input type="checkbox"/>			Other:	<input type="checkbox"/>		
APE, Eligibility and Effect Determination	<input type="checkbox"/>	ESD Approval Date(s)	SHPO Approval Date(s)																												
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Archaeological Records Check and Assessment	<input type="checkbox"/>																														
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	February 26, 2025	N/A																												
Archaeological Phase Ic Survey Report	<input type="checkbox"/>																														
Other:	<input type="checkbox"/>																														
Memorandum of Agreement (MOA) <input type="checkbox"/>		MOA Signature Dates (List all signatories) <div style="border: 1px solid black; height: 20px; width: 100%;"></div>																													

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

On February 26, 2025, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category B, Type 9, under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, pages D-1 to D-5). The project also falls within the guidelines of Category A, Types 4 and 9 (Appendix D, page D-10).

- Category A-4: Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
- Category A-9: Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.
- Category B-9: Installation, replacement, repair, lining, or extension of culverts and other drainage structures under the specified conditions.

An archaeological Phase Ia records check and reconnaissance survey of the project area was conducted by Weintraut & Associates, Inc., which was approved by INDOT CRO on February 26, 2025 (Appendix D, pages D-6 to D9). There were 20 shovel test probes conducted during fieldwork. No cultural materials were recovered from the shovel test probes and no new archaeological sites were recorded.

No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

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SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input type="text"/>	<input type="text"/>	<input type="text"/>
Publicly owned recreation area	<input type="text"/>	<input type="text"/>	<input type="text"/>
Other (school, state/national forest, bikeway, etc.)	<input type="text"/>	<input type="text"/>	<input type="text"/>
Wildlife and Waterfowl Refuges			
National Wildlife Refuge	<input type="text"/>	<input type="text"/>	<input type="text"/>
National Natural Landmark	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Wildlife Area	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Nature Preserve	<input type="text"/>	<input type="text"/>	<input type="text"/>
Historic Properties			
Site eligible and/or listed on the NRHP	<input type="text"/>	<input type="text"/>	<input type="text"/>
<u>Evaluations</u>			
	<u>Prepared</u>		
Programmatic Section 4(f)	<input type="text"/>		
"De minimis" Impact	<input type="text"/>		
Individual Section 4(f)	<input type="text"/>		
Any exception included in 23 CFR 774.13	<input type="text"/>		

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP eligible or listed historic resources regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, an aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, pages E-1 to E-5), there are three potential Section 4(f) resources located within the 0.5 mile search radius. According to additional research of Starke County property records (<https://starkein.wthgis.com/>), and by the site visits on May 3, 2023, by Parsons and American Structurepoint on August 8, 2023, there are no Section 4(f) resources located within or adjacent to the project area.

The NJE Trail and trailhead parking area are located within and adjacent to the project corridor (Appendix B, pages B-5, B-6, B-12, and B-20). The NJE Trail is a 10.0-foot-wide paved asphalt multi-use trail located within the former CKIN corridor and crosses SR 10 west of CR South 250 West. The trailhead parking area is located in the southwest quadrant of the SR 10 and CR South 250 West intersection. The NJE Trail and trailhead parking area are open to the public but are privately owned and maintained. The HVRM owns the railroad corridor and parcel containing the trailhead parking area. Prairie Trails Club manages and maintains the trail.

Because the NJE Trail and trailhead parking area are privately owned, they are not Section 4(f) resources. Therefore, no Section 4(f) use is expected.

An early coordination letter was sent to the Prairie Trail Club on August 21, 2023. The Prairie Trail Club provided several early coordination responses requesting that the former striped pavement markings for the NJE Trail crossing be restored on SR 10 (Appendix C, pages C-13 to C-16). At the NJE Trail crossing of SR 10, an ADA-compliant 10.0-foot-wide pedestrian crossing will be constructed, which will include striped pavement markings similar to those previously denoting the trail across SR 10. This is included as a firm commitment in the **Environmental Commitments** section of this document. Details of the NJE Trail crossing of SR 10 are provided in Appendix B, page B-20.

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Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐
☐
☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of three LWCF properties in Starke County (Appendix I, page I-11). None of the properties are located within or adjacent to the project area; therefore, there will be no impacts to Section 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes

No

X

X
X

Location in STIP:

STIP FY 2026-2030, page 164

Name of MPO (if applicable):

Location in TIP (if applicable):

Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

STIP/TIP: This project is included in the Fiscal Year (FY) 2026-2030 Statewide Transportation Improvement Programs (STIP) (Appendix H, page H-1). State funds will be used for ROW acquisition.

Attainment Status: This project is located in Starke County, which is currently in attainment for all criteria pollutants according to <https://www.in.gov/idem/sips/nonattainment-status-of-counties/>; therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT: This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

Indiana Department of Transportation

County StarkeRoute SR 10Des. No. 2100231

SECTION G - NOISE

Noise**Yes****No**Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? ☐ ☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes**No**☒☐

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒☐

If No, are steps being made to advance the community's transition plan?

☐☐

Does the project comply with the transition plan? (explain in the discussion below)

☒☐

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project complies with local development plans including the *North Judson Comprehensive Plan, 2019, North Judson 5-Year Parks and Recreation Master Plan, 2022-2026, and North Judson Transition Plan*. The Comprehensive Plan is focused on economic development, public facilities, and place-making, with goals including attracting businesses, enhancing connectivity, and revitalizing the downtown area. The Comprehensive Plan states that enhancing connectivity between the NJE Trail and local destinations is one of the place-making goals. One goal of the 5-Year Parks and Recreation Master Plan is to create a more unified and connected park system, with improved pedestrian accessibility and ADA compliance for parks. The project will support the goals of the local plans by providing an improved NJE Trail crossing of SR 10. An ADA compliant 10.0-foot-wide pedestrian crossing will be constructed. The ADA treatments will include detectable warning surfaces on the north and south sides of the crosswalk and 24.0-inch-wide white painted lines (Appendix B, page B-20).

The project will not result in impacts to community cohesion or the local tax base because all work will occur on existing infrastructure, which will be improved or replaced in their current locations. Access to residential driveways and the trail crossing along SR 10 will be maintained during construction. As discussed in the **Maintenance of Traffic (MOT) During Construction** section of this CE document, during construction detours will be implemented for through traffic (Appendix B, pages B-16 and B-17).

The Indiana Festivals website (<https://indianafestivals.org/>) was checked on August 8, 2025, by Parsons to determine community events and festivals planned near the project area. The main festival in North Judson is the Mint Festival, which celebrates the town's connection to mint farming. The festival occurs in June at Lane Street and North Judson Park. Festival participants will be able to access Lane Street and North Judson via other local roads. The project will not impact community events and festivals.

Indiana Department of Transportation

County Starke

Route SR 10

Des. No. 2100231

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, aerial map of the project area (Appendix B, page B-3), and RFI (Appendix E, pages E-1 to E-5) there are two recreational facilities, three pipelines, three railroads, and one trail located within 0.5 mile of the project area. There are six public facilities within and adjacent to the project area. That number was confirmed by the site visit on May 3, 2023, by Parsons.

The two recreational facilities are the NJE Trail and a trailhead parking area (Appendix B, pages B-5, B-6, B-12, and B-20). The NJE Trail is a 10.0-foot-wide paved asphalt multi-use trail located within the former CKIN rail corridor and crosses SR 10 west of CR South 250 West. The trailhead parking area is located in the southwest quadrant of the SR 10 and CR South 250 West intersection. At the NJE Trail crossing of SR 10, an ADA-compliant 10.0-foot-wide pedestrian crossing will be constructed. The ADA treatments will include detectable warning surfaces on the north and south sides of the crosswalk and 24.0-inch-wide white painted lines. Pedestrian and bicycle access to the NJE Trail will be maintained during construction, with only limited, short-term, temporary shifts (to the east or west) of the trail crossing to accommodate the roadway paving and installation of the ADA-compliant crossing of SR 10. During construction, SR 10 will be closed to through traffic and there will be no access to the trailhead parking area for approximately four to six months. During construction, temporary trailhead parking for trail users will be provided behind HVRM, which is located on Main Street at the intersection of North Street and Oakwood Street. This trail head parking area is adjacent to the NJE Trail.

An early coordination letter was sent to the Prairie Trail Club on August 21, 2023. The Prairie Trail Club provided several early coordination responses requesting that the former striped pavement markings for the NJE Trail crossing be restored on SR 10 (Appendix C, pages C-13 to C-16). At the NJE Trail crossing of SR 10, an ADA-compliant 10.0-foot-wide pedestrian crossing will be constructed, which will include striped pavement markings similar to those previously denoting the trail across SR 10. This is included as a firm commitment in the **Environmental Commitments** section of this document. Details of the NJE Trail crossing of SR 10 are provided in Appendix B, page B-20.

Overhead and underground utilities are present along SR 10 (Appendix B, page B-21). Along the north side of SR 10, Mediacom cable TV and NIPSCO electric lines run overhead on utility poles and Brightspeed fiber optic cable lines run underground. Along the south side of SR 10, Brightspeed fiber optic cable lines run overhead. Two Buckeye pipelines run parallel underground on the north side of the railroad corridor. The Brightspeed fiber optic cable lines located underground along the north side of SR 10 will be relocated in the vicinity of INDOT Culvert No. CLV-010-075-45.65. In order to accommodate the culvert replacement, the fiber optic cable lines will be lowered 11.0 feet in the same location. The project team in conjunction with the INDOT Utilities and Railroad have ongoing communication with the utility companies. It is anticipated that there will not be any interruption to services provided by utility companies.

HVRM owns the railroad corridor that crosses through the project area. HVRM operates tourist train rides on this railroad. The trains depart from the HVRM depot in North Judson and travel through the farm country of northern Indiana. The project team in conjunction with the INDOT Utilities and Railroad have ongoing communication with HVRM. The project will not affect the operations of the tourist train rides. Therefore, no impacts the railroad are expected.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Due to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173, EO 12898 has been rescinded and this section is no longer applicable.

Indiana Department of Transportation

County Starke Route SR 10 Des. No. 2100231

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
Is a BIS or CSRS required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)
Phase I Environmental Site Assessment (Phase I ESA)
Phase II Environmental Site Assessment (Phase II ESA)
Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): October 25, 2023

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of geographic information systems (GIS) and available public records, there are eight sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances identified in or within 0.5 mile of the project area. One underground storage tank is adjacent to the north side of the project area at 2860 West SR 10, North Judson. The property is an auto repair shop and service station (Appendix B, B-4). There is no contamination on this site; therefore, no impacts are expected. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Indiana Department of Transportation

County Starke

Route SR 10

Des. No. 2100231

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Other

**IN Department of Environmental Management
(401/Rule 5)**

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Isolated Wetlands
Rule 5
Other

IN Department of Natural Resources

Construction in a Floodway
Navigable Waterway Permit
Other

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

No permits are required for this project. Applicable recommendations provided by resource agencies are included in the **Environmental Commitments** section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations. It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County Starke

Route SR 10

Des. No. 2100231

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District ES)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
4. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
5. Bogus Run and Maciolek Ditch will be labeled "Do Not Disturb" on the project plans. (INDOT ESD)
6. Wetlands A, B, and C will be labeled "Do Not Disturb" on the project plans. (INDOT ESD)
7. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the US Army Corps of Engineers permit. (INDOT EWPSO)
8. USFWS Bridge/Structure Assessments are only valid for two years. If construction begins after January 24, 2026, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
9. An ADA-compliant 10.0-foot-wide pedestrian crossing will be constructed at the NJE Trail crossing of SR 10, which will include striped pavement markings similar to those previously denoting the trail across SR 10. (Prairie Trails Club, Inc.)

For Consideration:

1. Pavement rehabilitation projects typically do not have a significant impact on fish, wildlife, and botanical resources if best management practices (BMPs) are in place to limit the migration of polycyclic Aromatic Hydrocarbons (PAHs) into local waterways. Where possible, road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species. (IDNR-DFW)
2. Small culverts can provide aquatic and terrestrial wildlife passage opportunities to reduce wildlife vehicle interactions and improve roadway safety. The replacement drainage structure, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for replacement structures is recommended whenever possible to improve wildlife/vehicle safety. Bank lines should be maintained or restored within structures to allow for wildlife passage above the ordinary high-water mark where appropriate. There are several techniques and materials for incorporating wildlife passage into the design of a crossing structure if maintaining or restoring banklines is not possible. All wildlife passage designs must include a smooth level pathway a minimum of 1-3 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The width and location of the wildlife pathway is dependent on the wildlife species using the area. (IDNR-DFW)
3. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles; seed and apply mulch on all other disturbed areas. (IDNR-DFW)

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Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level <ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

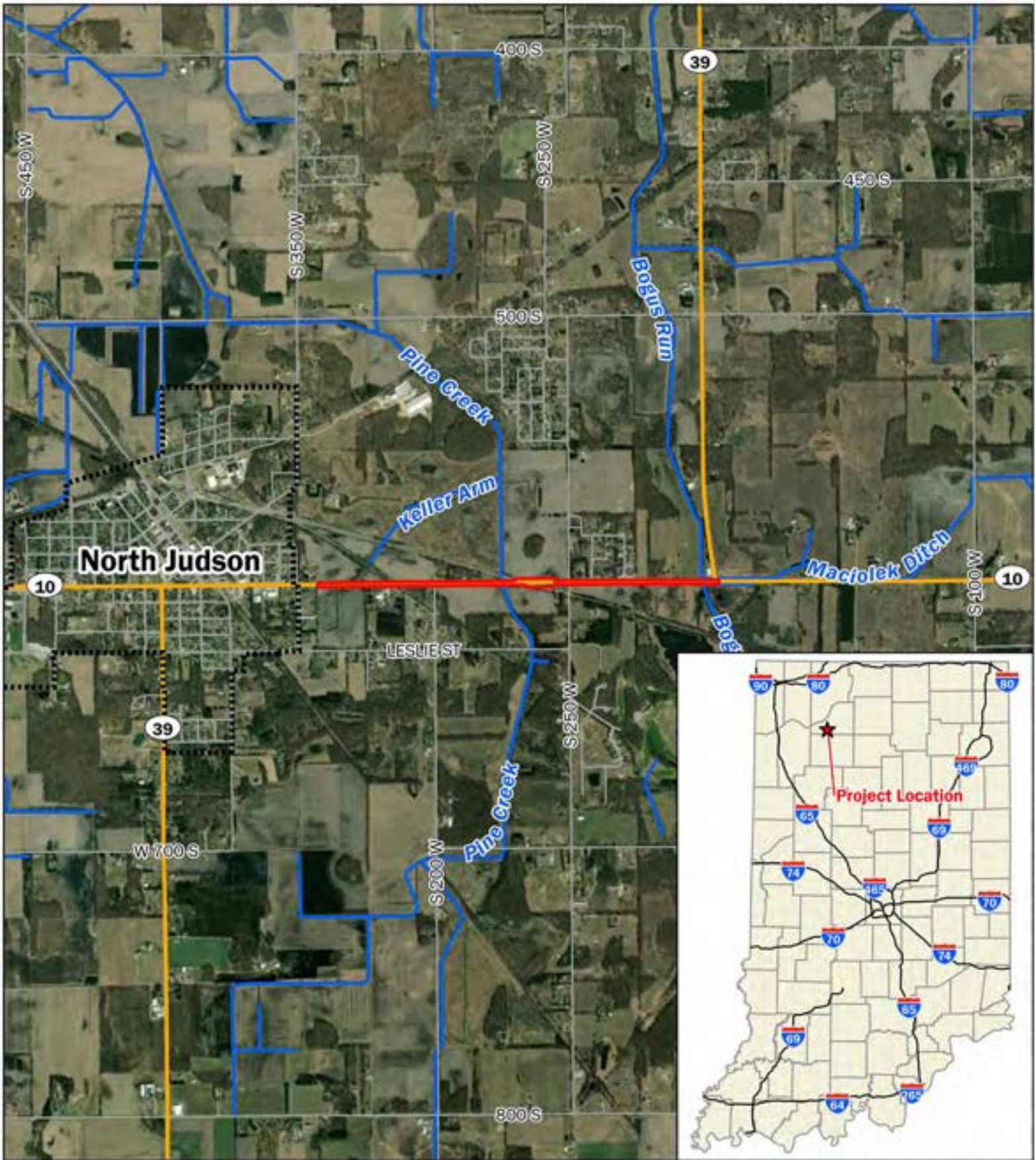
¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

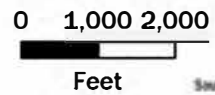
Note: Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics



- Study Area
- ~ Streams and Rivers
- Incorporated Areas
- Interstate
- State Road
- Local Road



Sources:
 Non Orthorectified Data -
 Obtained from the State of Indiana
 Geographical Information Office Library
 Orthorectified Data -
 Obtained from Indiana Map
 Framework Data (www.indianamap.org)

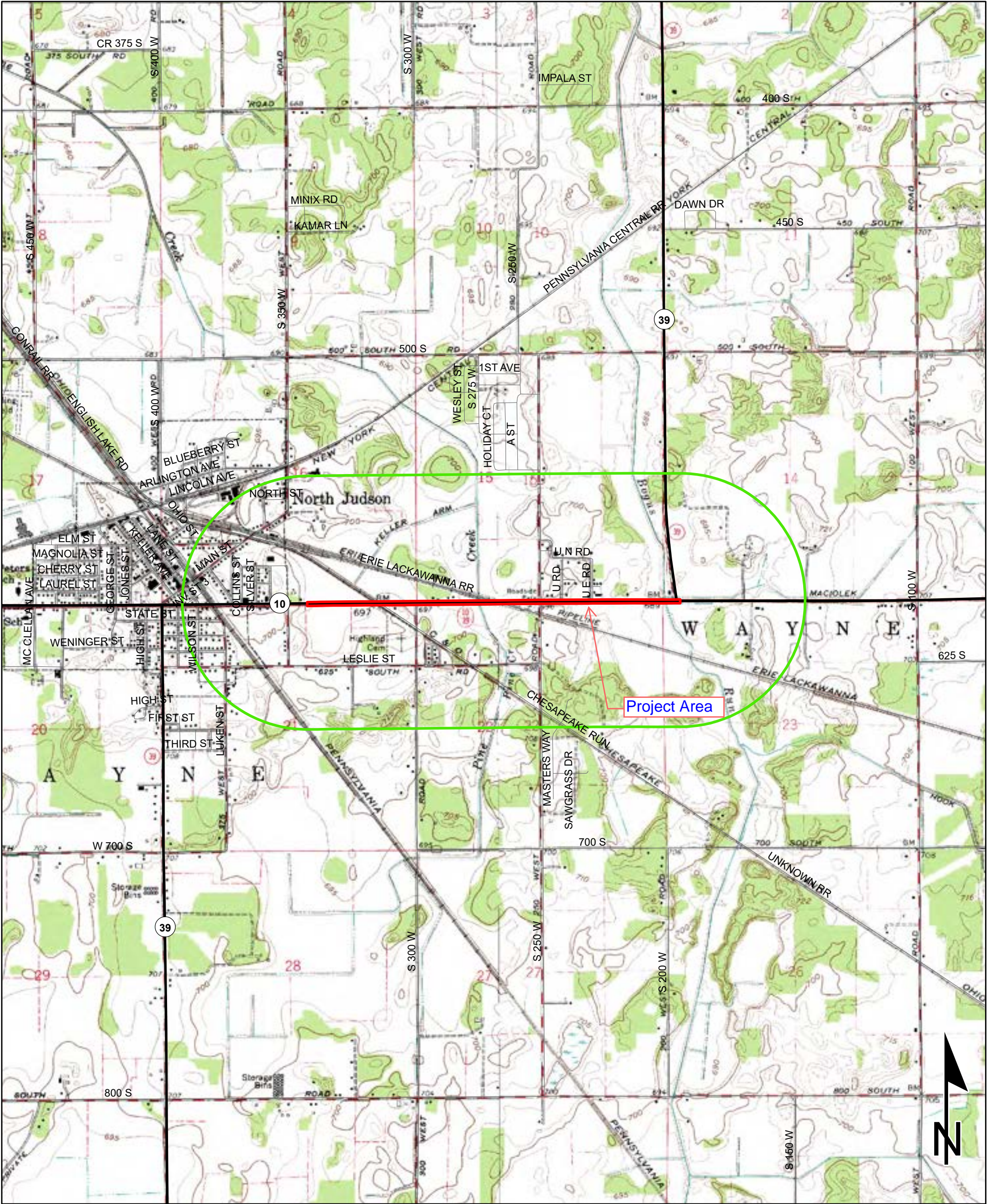
**SR 10 HMA Overlay,
 Preventive Maintenance
 Starke County, Indiana
 Project Location Map**

Des. 2100231
 Date: 8/2/2023



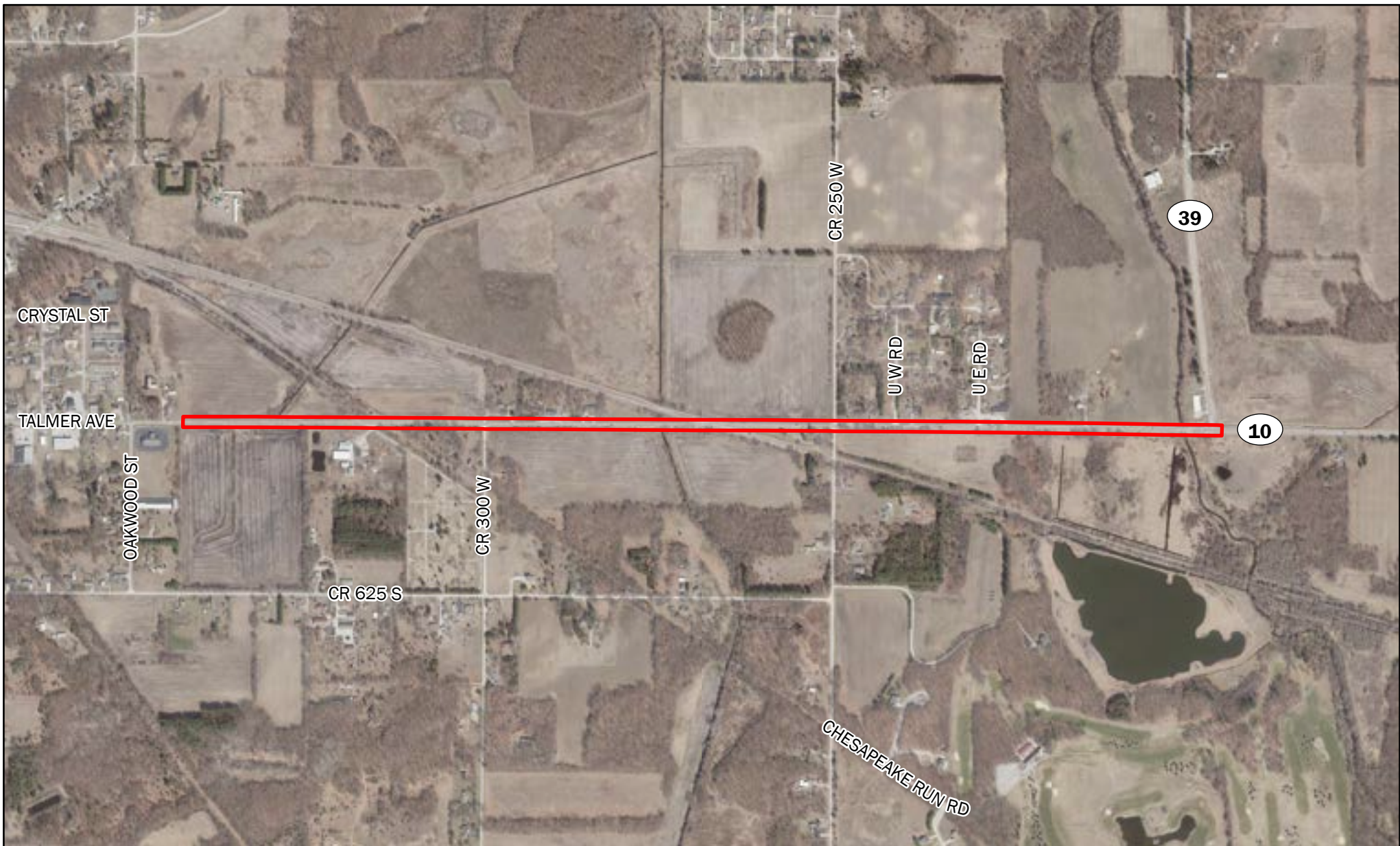
PARSONS


USGS Topographic Map

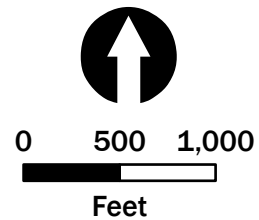


Sources: 0.45 0.23 0 0.45 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

DENHAM & NORTH JUDSON
QUADRANGLE INDIANA
7.5 MINUTE SERIES
USGS TOPOGRAPHIC MAP



 Study Area



Sources:
Non Orthophotography Data -
Obtained from the State of Indiana Geographical
Information Office Library
Orthophotography -
Obtained from Indiana Map
Framework Data (www.indianamap.org)

SR 10 HMA Overlay, Preventive
Maintenance Project
Starke County, Indiana
2024 Aerial Map

Des. 2100231

Date: 6/24/2025



PARSONS

Created by: p009334D



Photo 1— View along SR 10 facing west (05/24/2023).



Photo 2—View of SR 10 facing east (05/24/2023).



Photo 3 —View of SR 10 with auto repair shop and service station on the north side of SR 10 facing east (05/24/2023).



Photo 4 —View of the SR 10 facing west (05/24/2023).



Photo 1— View along the North Judson Erie Trail towards the SR 10 crossing, facing northwest (05/24/2023).



Photo 2—View along SR 10 of the North Judson Erie Trail and railroad crossing facing east (05/24/2024).



Photo 3 —View of the south end of the clogged culvert (CLV-29637) facing north (05/24/2023).



Photo 4 —View of the north end of the clogged culvert (CLV 29637) facing south (05/24/2023).

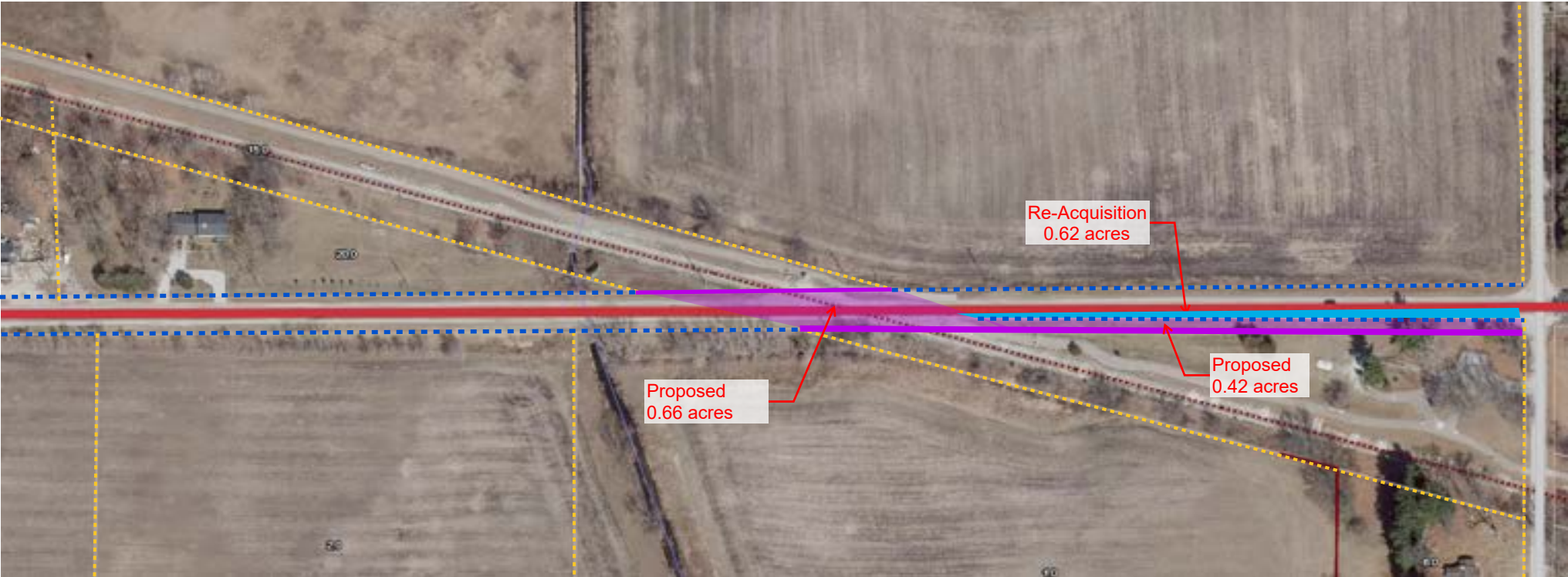


Aerial View of the North Judson Erie Trail within the Project Area



View of Trailhead Parking Area for the North Judson Erie Trail Facing South from SR 10

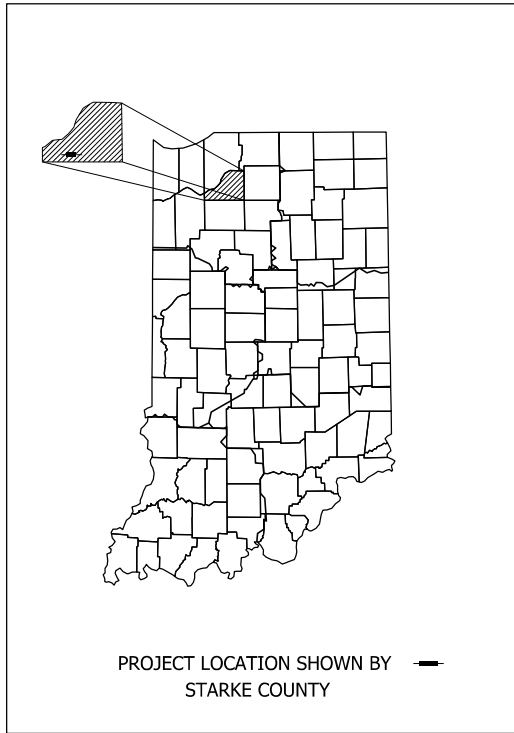
ROW Acquisition



LEGEND

- Property Lines Assumed
- INDOT ROW
- Proposed ROW
- Re-Acquisition

CONTRACT NO. R-43879



PROJECT LOCATION SHOWN BY — STARKE COUNTY

DES. NO. 2100231

BEGIN PROJECT
Sta. 346+50.00 "B"

PAVING EXCEPTIONS

Sta. 392+76.00 "A" Lt. to Sta. 393+12.00 "A" Lt.
Sta. 393+14.00 "A" Rt. to Sta. 393+50.00 "A" Rt.
Sta. 428+40.50 "A" to Sta. 429+77.00 "A"

Indiana Department of Transportation
Standard Specifications dated 2024
to be used with these plans.

INDIANA DEPARTMENT OF TRANSPORTATION

ROAD PLANS

PROJECT DESCRIPTION
HMA Overlay, Preventive Maintenance

PROJECT LOCATION

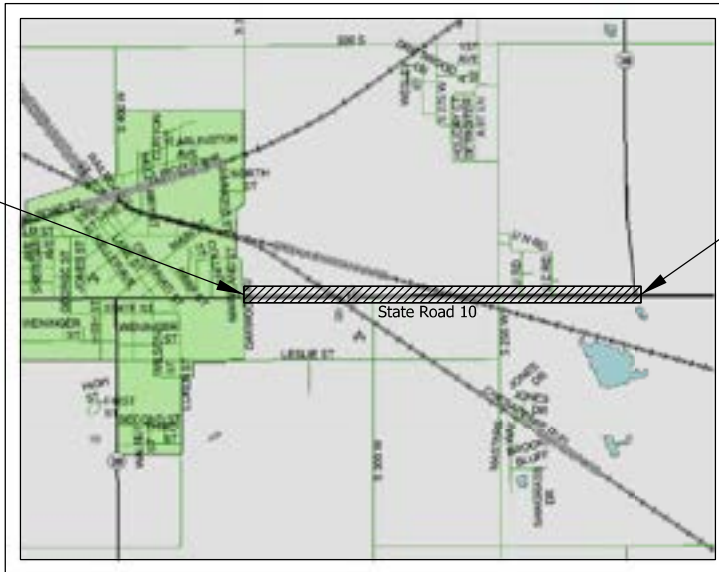
Pavement Preservation Project on State Road 10 From 0.58 Miles East of West Junction of State Road 39 to East Junction of State Road 39. Located in Sections 14, 15, 16, 21, 22, & 23, T-32-N, R-3-W, Wayne Township, Starke County, Indiana. (RP 44+79 to RP 46+28)

Gross Length: 1.62 miles

Net Length: 1.51 miles

Note to Reviewer

Preliminary Field Check Plans - Not for Construction



PROJECT LOCATION MAP
Scale: 1" = 32000'

TRAFFIC DATA

A.A.D.T. (2025)	4,703 V.P.D.
A.A.D.T. (2040) PROJ.	5,460 V.P.D.
D.H.V. (2025)	470 V.P.H.
DIRECTIONAL DISTRIBUTION	50%
TRUCKS	16% A.A.D.T.

DESIGN DATA

DESIGN SPEED	55 MPH
PROJECT DESIGN CRITERIA	PARTIAL 3R (NON-FREEWAY)
FUNCTIONAL CLASS	MAJOR COLLECTOR
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE

LATITUDE: 41° 12' 54" N
LONGITUDE: 86° 45' 56" W

HUC 14: 07120001070050

PLANS PREPARED BY:

PARSONS

PHONE NUMBER:

317-616-1016

END PROJECT

Sta. 432+00.00 "A"

DATE: 7/19/2024

CERTIFIED BY:

APPROVED FOR LETTING:

INDIANA DEPARTMENT OF TRANSPORTATION

DATE

INDEX

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2	INDEX SHEET
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8	TYPICAL CROSS SECTIONS
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UTILITIES

Brightspeed | OCM
1728 Churchman Avenue
Indianapolis, IN 46203
Attn: Melissa Teague
Phone: 765-656-4663
Melissa.Teague@Brightspeed.com

NIPSCO Electric
801 East 86th Avenue
Merrillville, IN 46410
Attn: Dean Garrett
Phone: 219-647-6260
Mobile: 219-713-6929
utilitycoordination@nisource.com

Buckeye
5 Tek Park
Breinigsville, PA 18031
Attn: Brian Barr
Phone: 610-904-4409
Mobile: 610-904-4539
encroachmentreviews@buckeye.com

NIPSCO Gas
801 East 86th Avenue
Merrillville, IN 46410
Attn: Tyler Noveroske
Phone: 219-647-6260
tnoverske@nisource.com

Mediacom, LLC
631 North Main Street
North Webster, IN 46555
Attn: Jason Pogar
Phone: 574-244-2056
Mobile: 309-797-4659
jpogar@mediacomcc.com

Hoosier Valley Railroad
Emergency Notification: 574-896-3950
Railroad Contact: 574-896-3950
State Contact: 855-463-6848

Note:

All utility information shall be considered QL-D unless otherwise indicated and is for informational purposes only. The Contractor is responsible for verifying and coordinating with utility companies in accordance with the plans and specifications.

DATE: 7/19/2024

**INDIANA DEPARTMENT
OF TRANSPORTATION**

INDEX SHEET

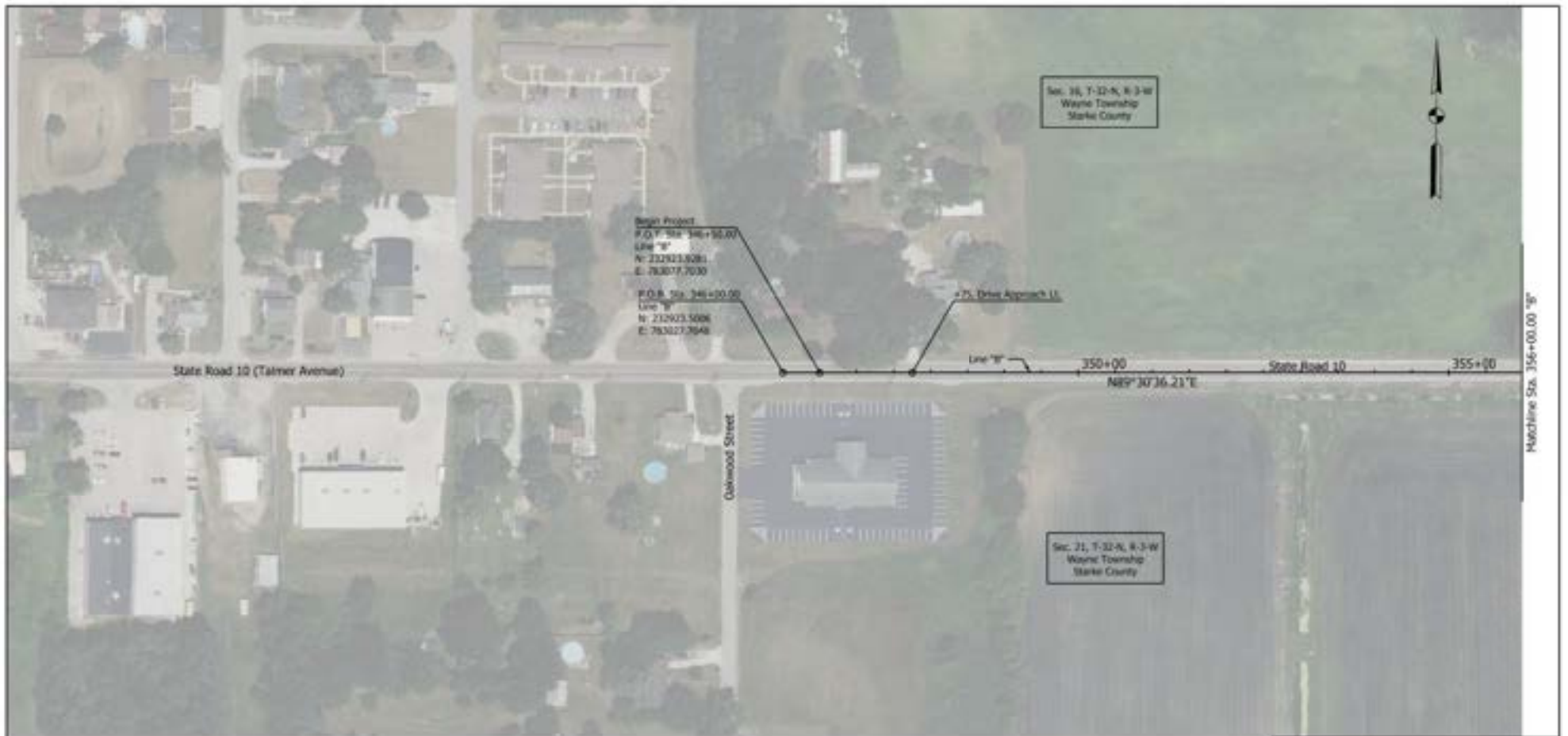
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Des. No: 2100321

Contract No: R-43879

Project: 2100321

Sheet: 2



DATE: 7/19/2024

INDIANA DEPARTMENT
OF TRANSPORTATION

STRIP MAP

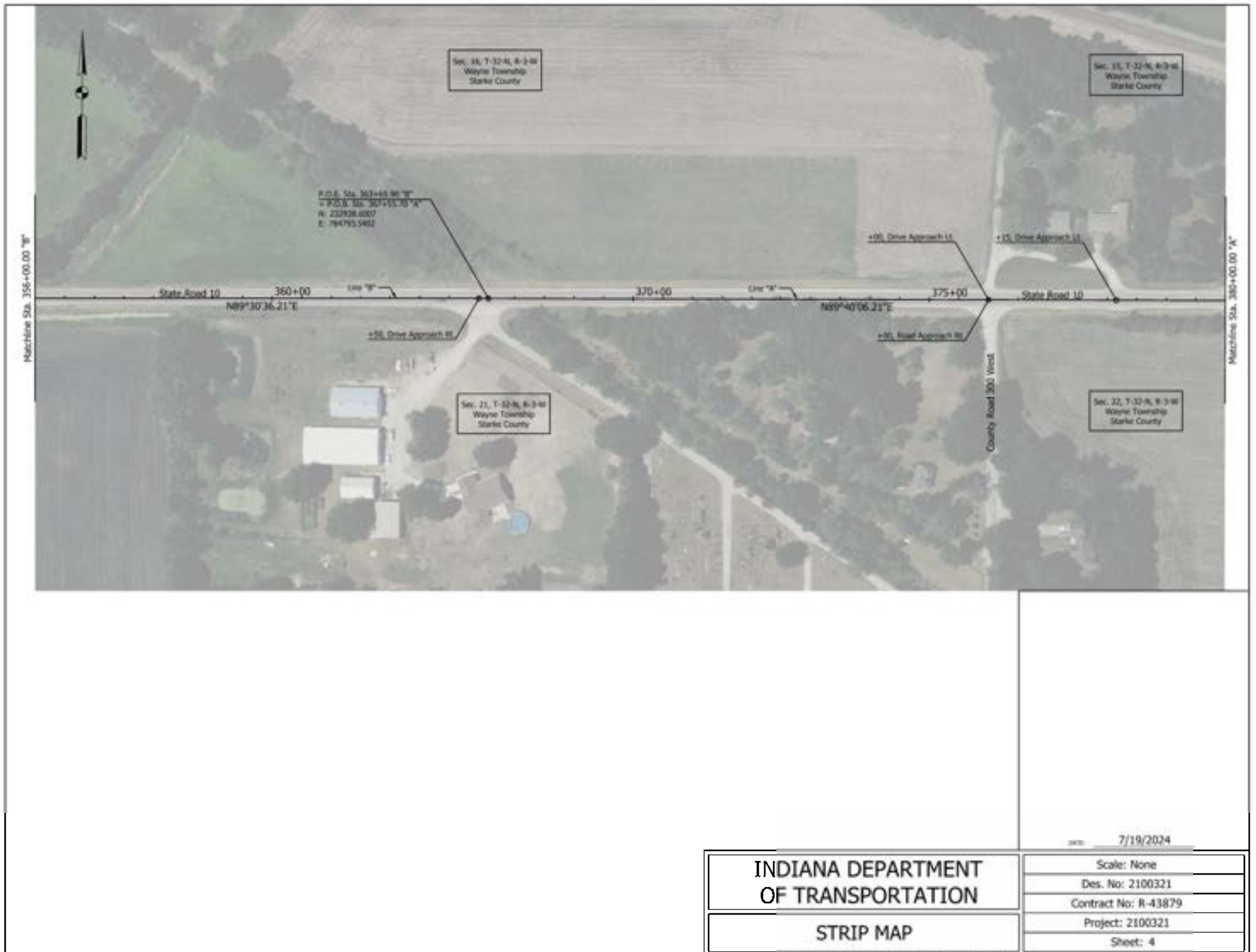
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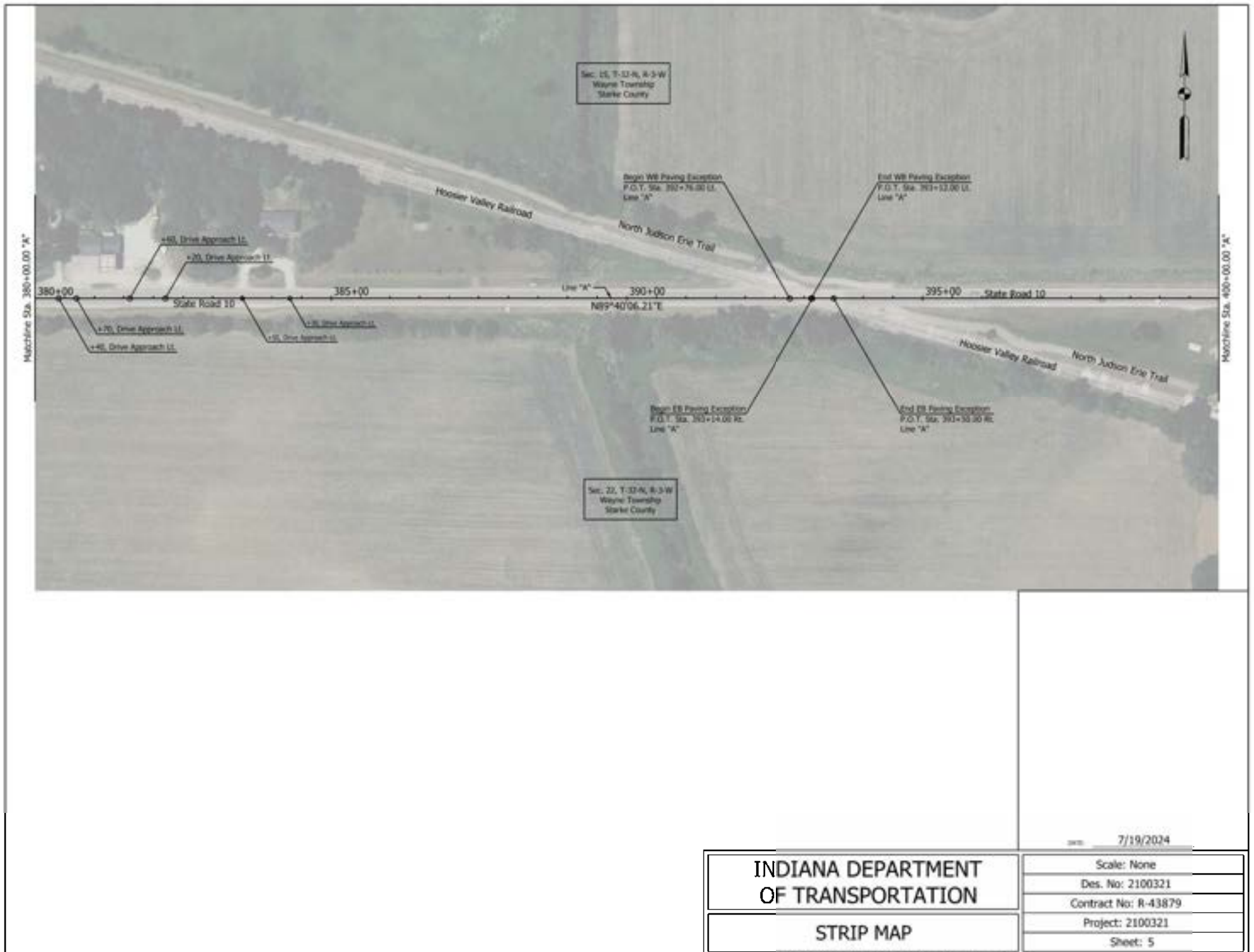
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Project: 2100321

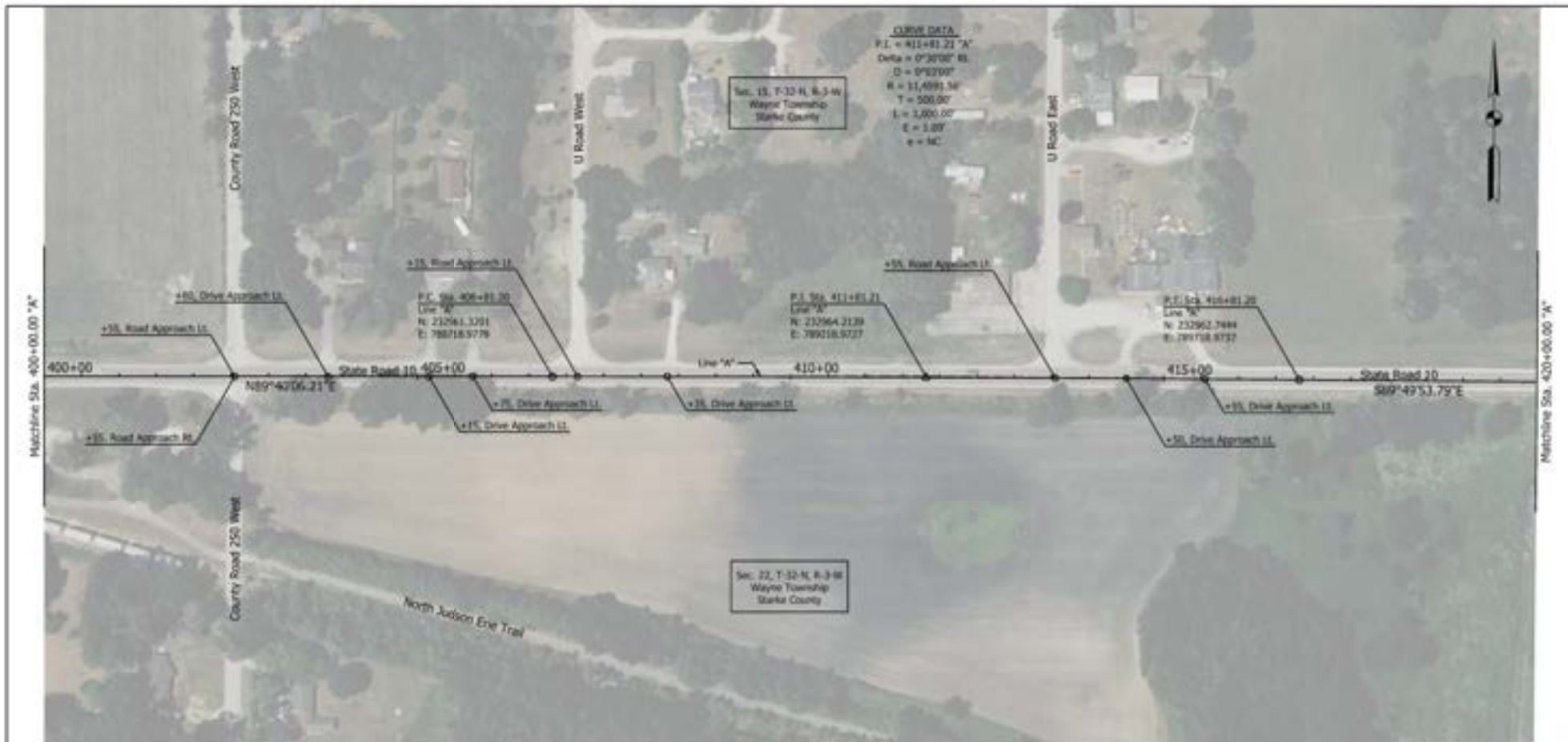
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DATE: 7/19/2024

INDIANA DEPARTMENT
OF TRANSPORTATION

STRIP MAP

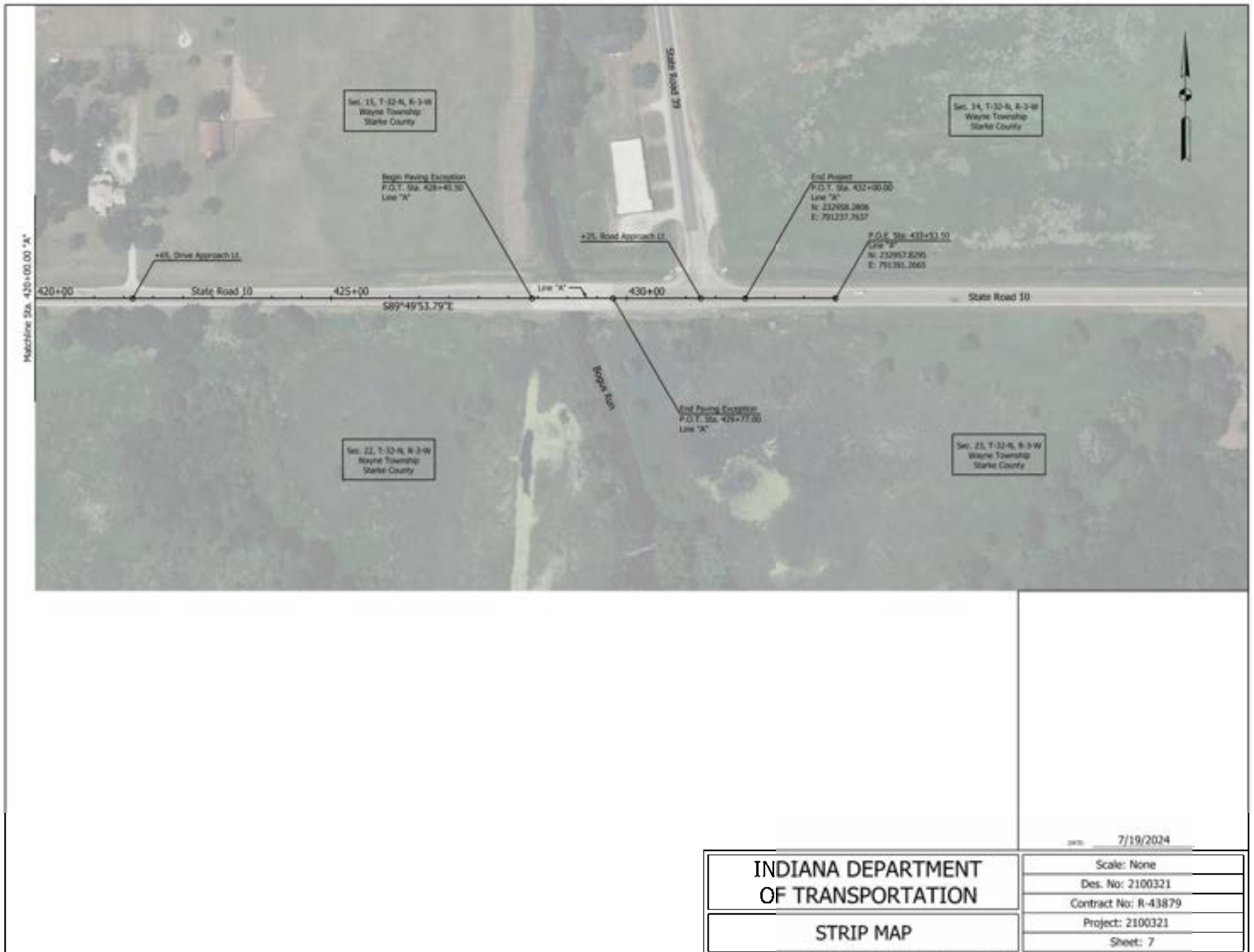
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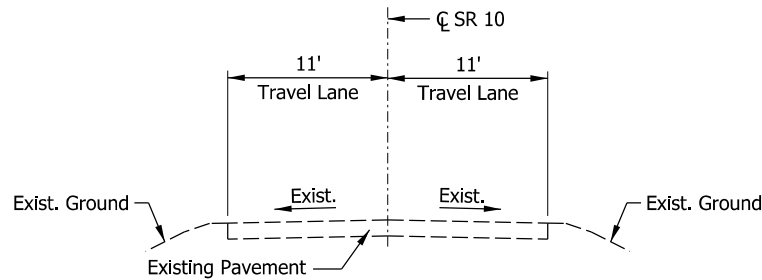
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Project: 2100321

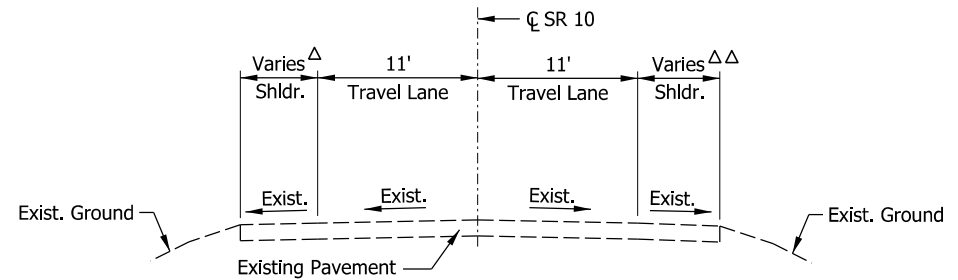
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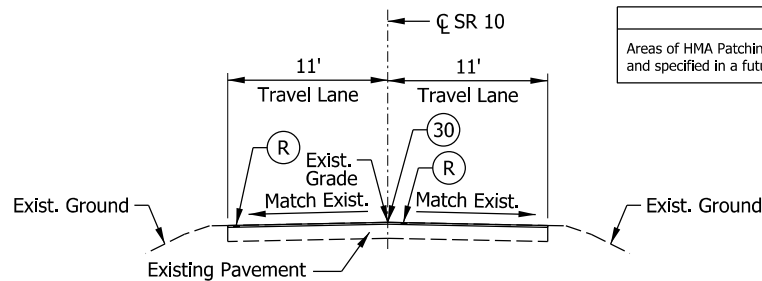
EXISTING TYPICAL SECTION SR 10
Sta. 346+50.00 "B" to Sta. 426+30.00 "A"



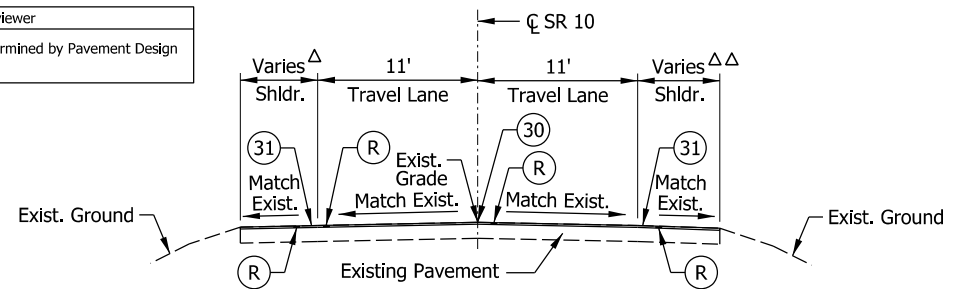
EXISTING TYPICAL SECTION SR 10
Sta. 426+30.00 "A" to Sta. 432+00.00 "A"

Note to Reviewer
Proposed pavement structure is an approximation, Pavement design for SR 10 will be determined for a future submittal.

Note to Reviewer
Areas of HMA Patching will be determined by Pavement Design and specified in a future submittal.



TYPICAL SECTION SR 10
Sta. 346+50.00 "B" to Sta. 426+30.00 "A"



TYPICAL SECTION SR 10
Sta. 426+30.00 "A" to Sta. 432+00.00 "A"

PAVING EXCEPTIONS

Sta. 392+76.00 "A" Lt. to Sta. 393+12.00 "A" Lt.
Sta. 393+14.00 "A" Rt. to Sta. 393+50.00 "A" Rt.
Sta. 428+40.50 "A" to Sta. 429+77.00 "A"

Δ Varies From 3'-0" at Sta. 426+30.00 Lt. "A"
To 10'-0" at Sta. 427+50.00 Lt. "A"
10'-0" From Sta. 427+50.00 Lt. "A"
To Sta. 428+40.50 Lt. "A"
10'-0" From Sta. 429+77.00 Lt. "A"
To Sta. 430+70.00 Lt. "A"
0'-0" From Sta. 430+70.00 Lt. "A"
To Sta. 432+00.00 Lt. "A"

ΔΔ Varies From 3'-0" at Sta. 426+30.00 Rt. "A"
To 10'-0" at Sta. 427+50.00 Rt. "A"
10'-0" From Sta. 427+50.00 Rt. "A"
To Sta. 428+40.50 Rt. "A"
10'-0" From Sta. 429+77.00 Rt. "A"
To Sta. 430+70.00 Rt. "A"
Varies From 10'-0" at Sta. 430+70.00 Rt. "A"
To 0'-0" at Sta. 432+00.00 Rt. "A"

Notes:

- Existing Pavement Composition, Including Location, Material, Thickness and Width, Are Based on Best Available Data and May Vary.
- Milled HMA Corrugations Shall Be Installed Per INDOT Standard Drawings No. E606-SHCG-01, E606-SHCG-02, E606-SHCG-05, E606-SHCG-06, E606-SHCG-08, and E606-SHCG-09.

LEGEND

- (30) Milled HMA Corrugations, Sinusoidal (R) Milling, Asphalt, 2 in., then 220#/SYD. QC/QA-HMA, 3, 58H, Surface 9.5mm
- (31) Milled HMA Corrugations, Conventional

**INDIANA DEPARTMENT
OF TRANSPORTATION**

TYPICAL SECTIONS

DATE: 7/19/2024

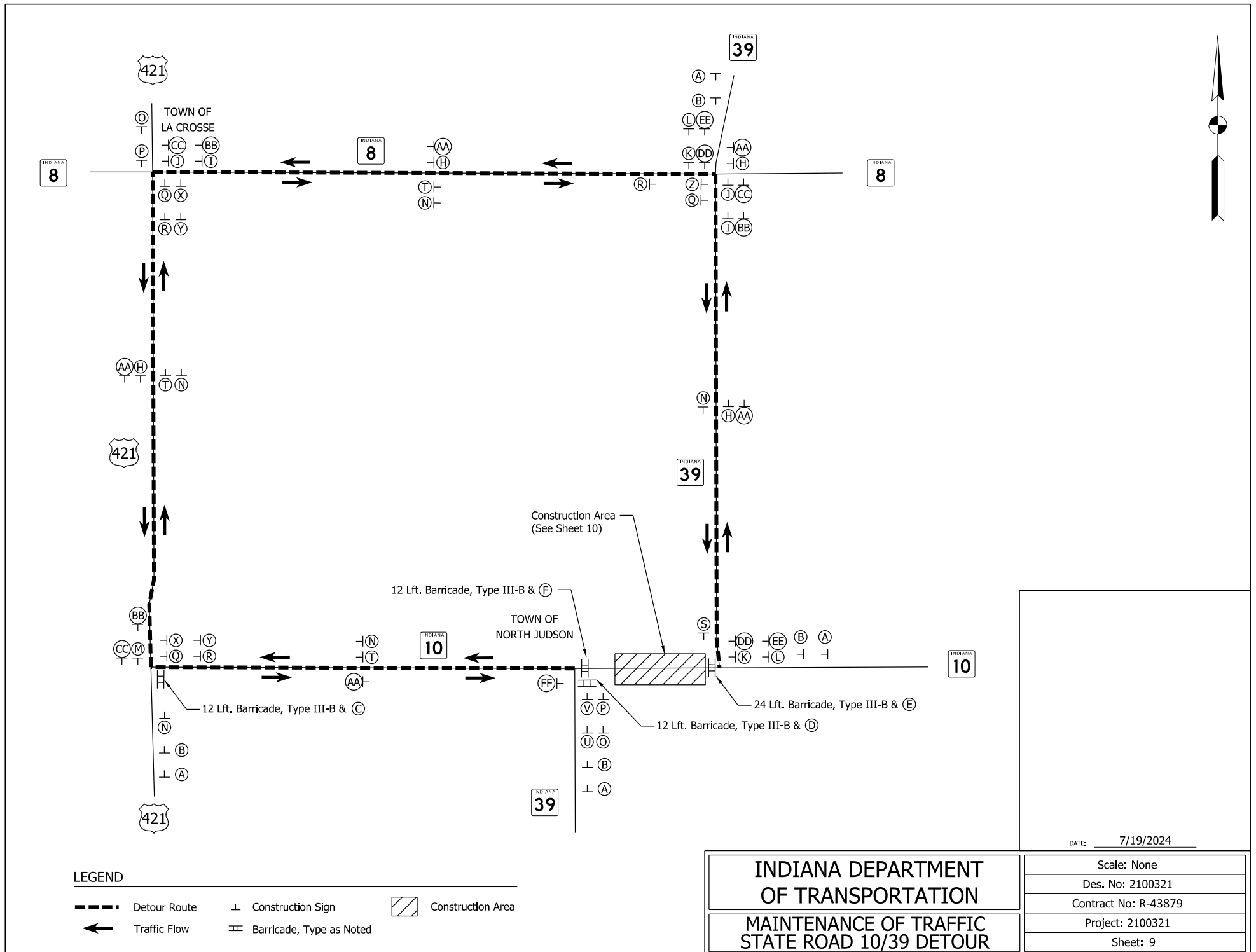
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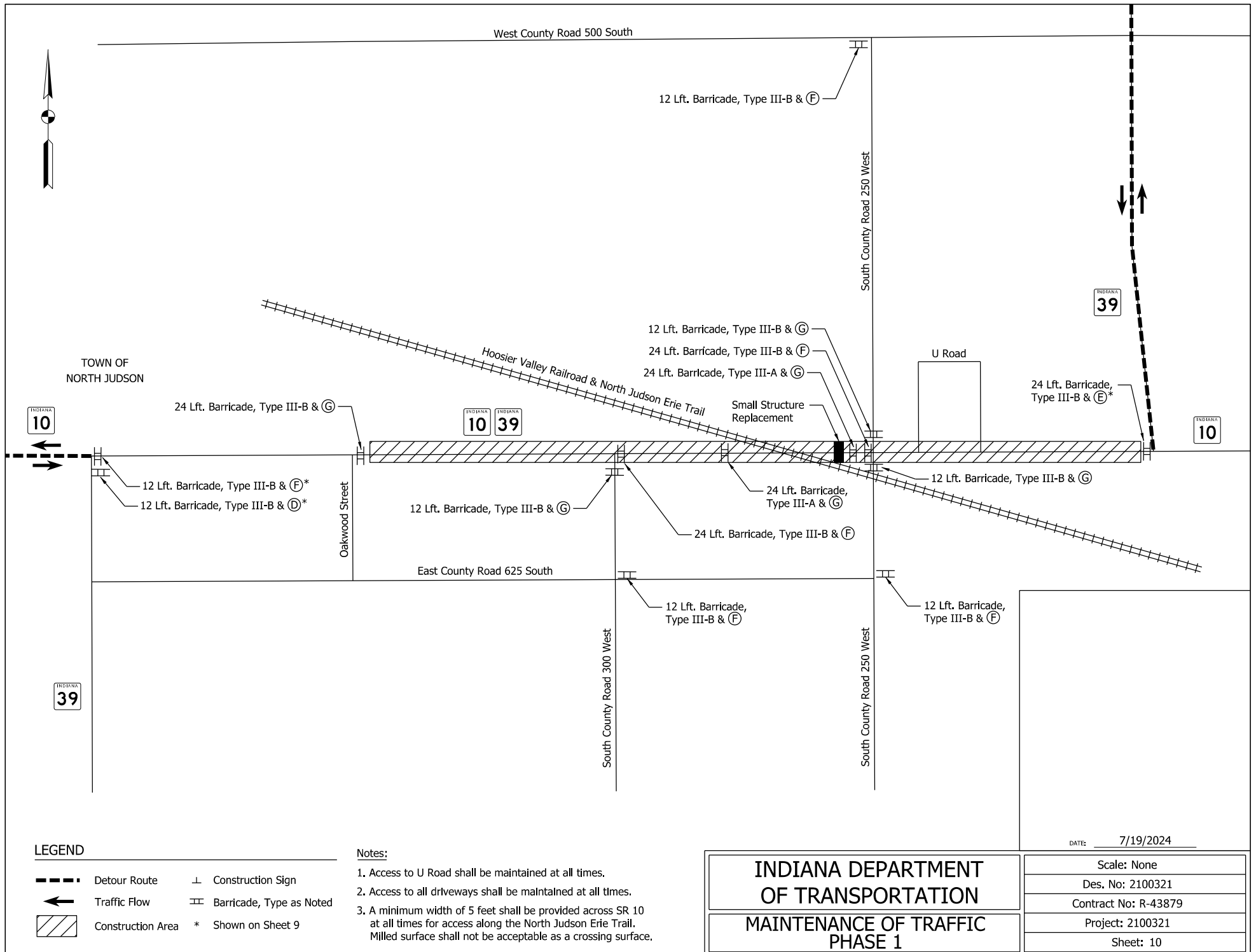
Contract No: R-43879

Project: 2100321

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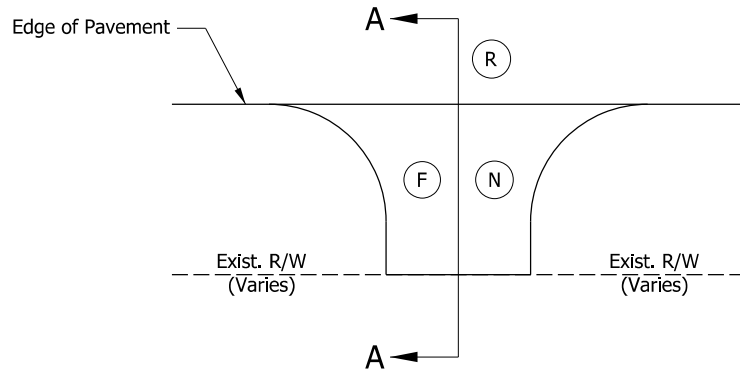
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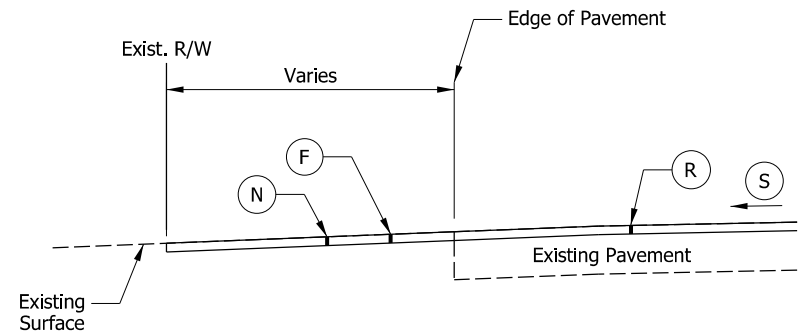
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Note to Reviewer

Proposed pavement structure is an approximation. Pavement design for SR 10 will be determined for a future submittal.



PLAN



SECTION A-A

LEGEND:

- (R) Milling, Asphalt, 2 in., then 220#/SYD. QC/QA-HMA, 3, 70, Surface 9.5mm
- (F) HMA for Approaches, Type C
- (N) Milling, Approach
- (S) Slope: 2.00% or match existing cross slope

DATE: 7/19/2024

INDIANA DEPARTMENT
OF TRANSPORTATION

TYPICAL ROAD APPROACH DETAIL
(IMPROVED)

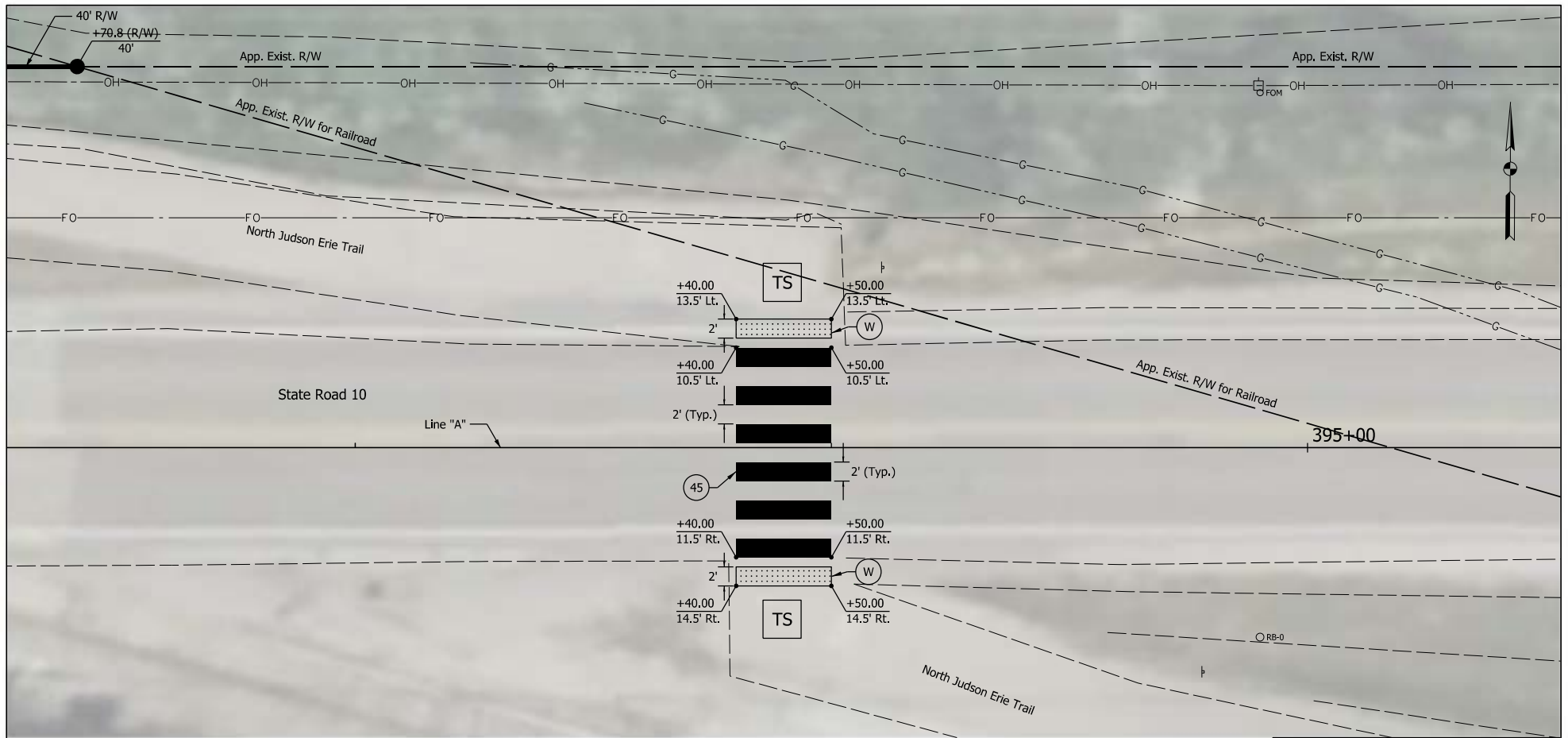
Scale: None

Des. No: 2100321

Contract No: R-43879

Project: 2100321

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LEGEND:

- (W) Detectable Warning Surface
- (TS) Turning Space (4 ft. x 4 ft.)
- (45) Transverse Marking, Thermoplastic, Crosswalk Line, White, 24 in.

INDIANA DEPARTMENT
OF TRANSPORTATION

TRAIL CROSSING DETAIL

DATE: 7/19/2024

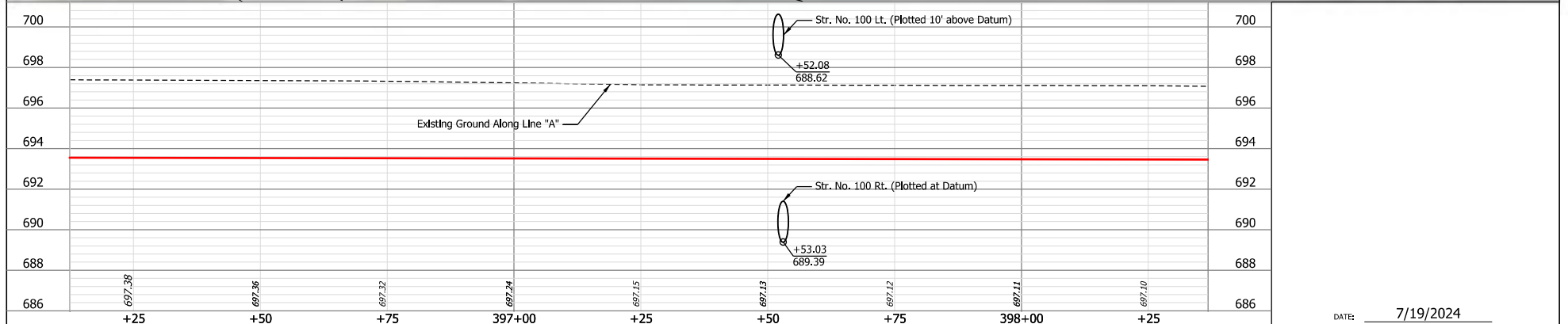
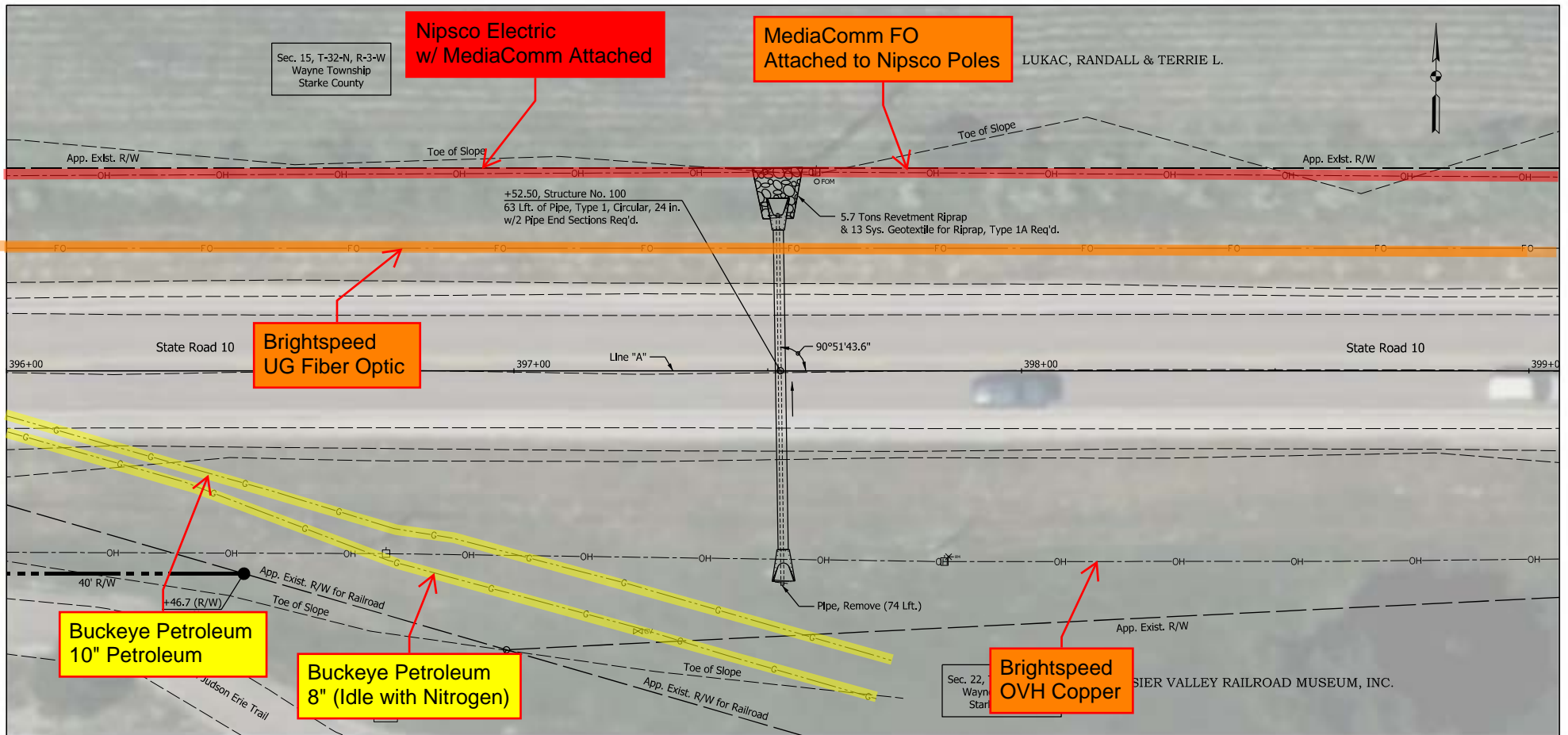
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Des. No: 2100321

Contract No: R-43879

Project: 2100321

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Note:
All utility information shall be considered Q/L-D unless otherwise indicated and is for informational purposes only. The Contractor is responsible for verifying and coordinating with utility companies in accordance with the plans and specifications.

INDIANA DEPARTMENT OF TRANSPORTATION

CULVERT DETAIL

DATE: 7/19/2024

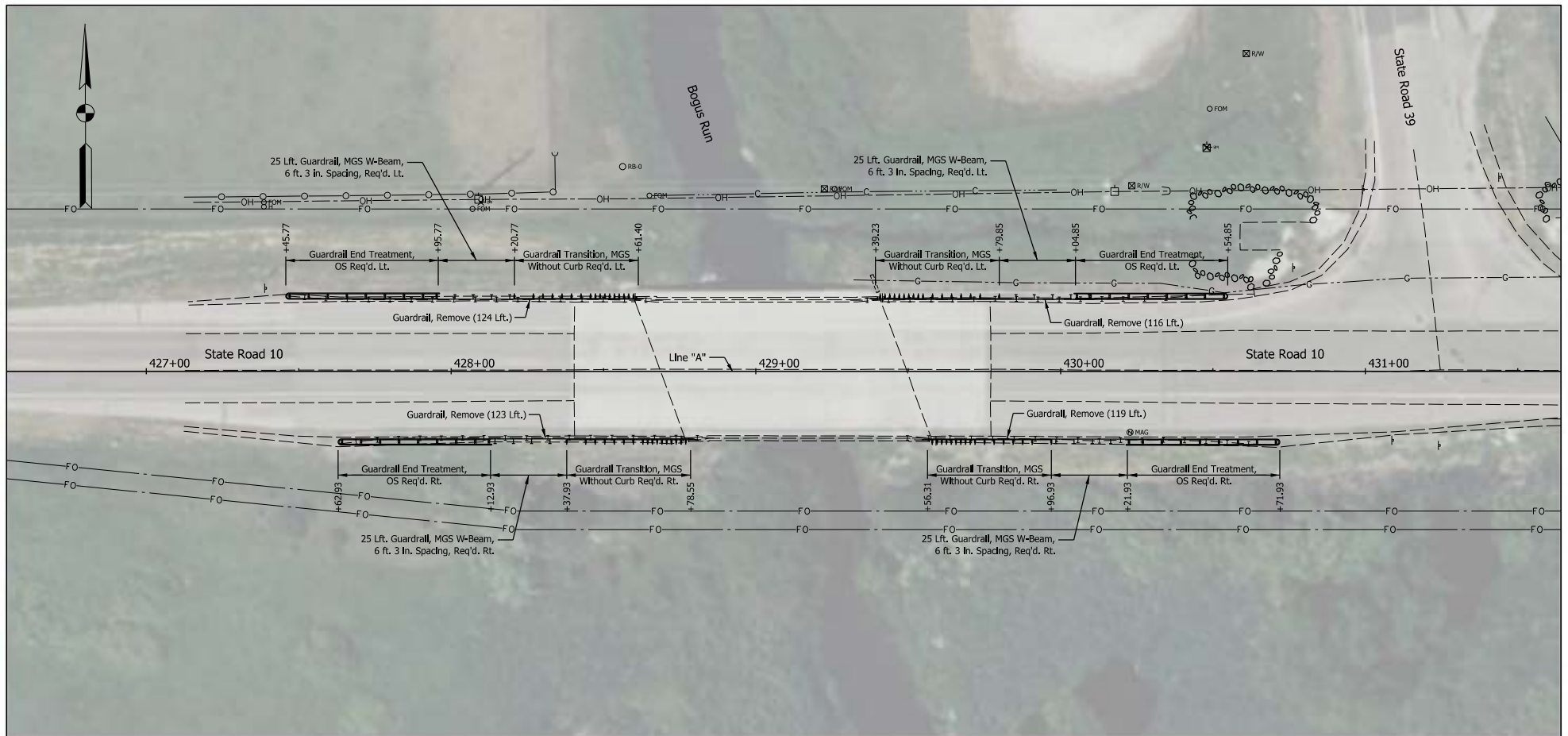
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Des. No: 2100321

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GUARDRAIL SUMMARY TABLE

LOCATION				MGS W-BEAM GUARDRAIL LENGTH											GUARDRAIL FLARE RATE	GUARDRAIL END TREATMENT TYPE OS	GUARDRAIL END TREATMENT TYPE MS	GUARDRAIL TRANSITION, MGS WITHOUT CURB	W-BEAM STANDARD POST AT _____ SPA.	GUARDRAIL REMOVE	GUARDRAIL RESET	IMPACT ATTENUATOR TYPE _____	REMARKS
FROM STATION	TO STATION	LEFT	MEDIAN LEFT MEDIAN RIGHT RIGHT	STANDARD POST AT 6 FT 3 IN. SPA.	STANDARD POST AT 3 FT 1.5 IN. SPA.	DOUBLE FACED AT 6 FT 3 IN. SPA.	DOUBLE FACED AT 3 FT 1.5 IN. SPA.	HEIGHT TRANSITION	GUARDRAIL TRANSITION WITH CURB	GUARDRAIL TRANSITION WITHOUT CURB	STRUCTURE TOP-MOUNTED POST	CABLE TERMINAL ANCHOR	STOP CURVED AT _____ FT. SPA.	LONG-SPAN GUARDRAIL									
				LEFT	LEFT	LEFT	LEFT	EACH	EACH	EACH	EACH	EACH	LEFT AT _____ FT. SPA.	EACH									
STATE ROAD 10																							
427+45.77	428+20.77	X		25												1		1		124			
429+79.85	430+04.85	X		25												1		1		116			
428+12.93	428+37.93		X	25												1		1		123			
429+45.93	430+21.93		X	25												1		1		119			
TOTAL					100											4		4		462			

DATE: 7/19/2024

**INDIANA DEPARTMENT
OF TRANSPORTATION**

GUARDRAIL DETAIL

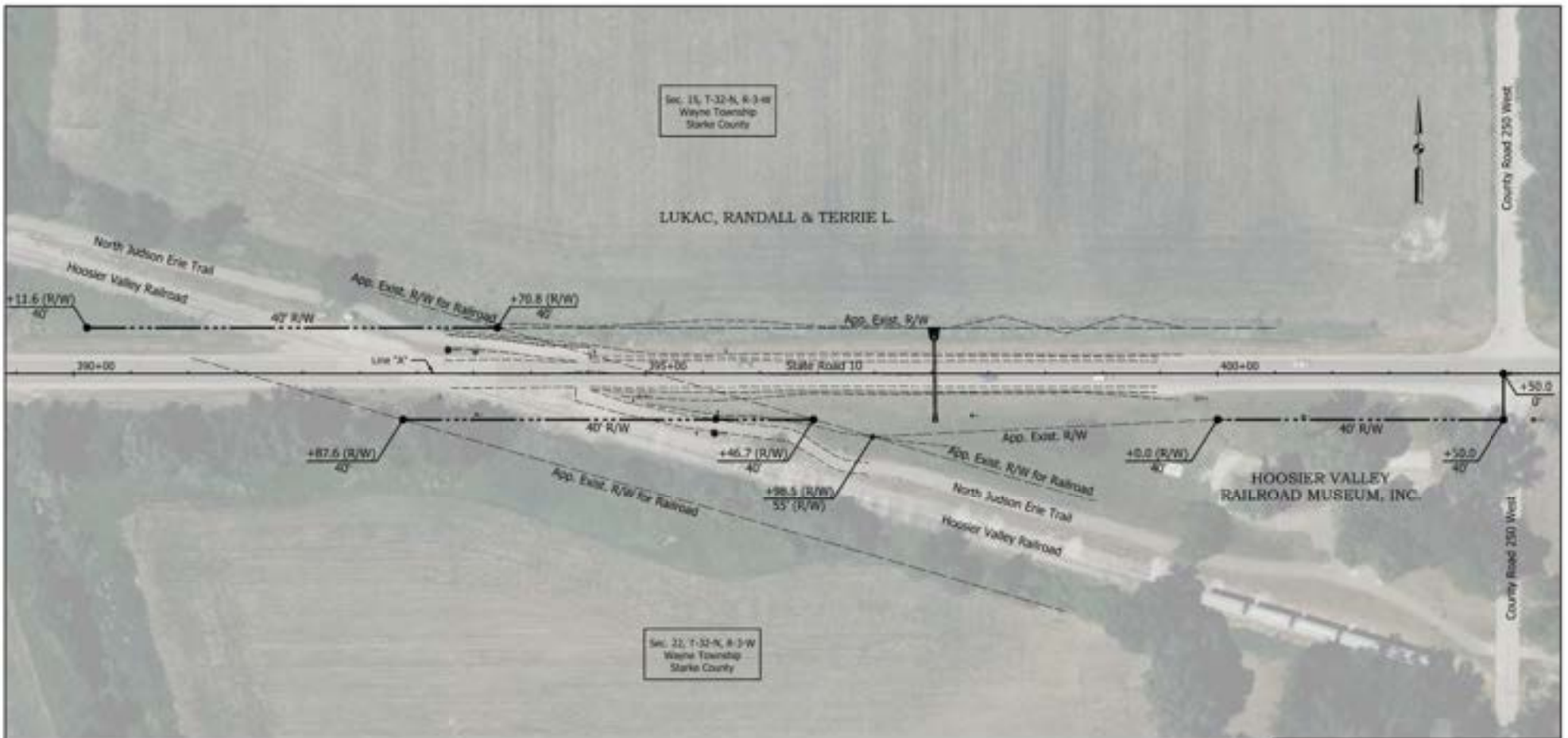
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Des. No: 2100321

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DATE: 7/19/2024

INDIANA DEPARTMENT
OF TRANSPORTATION

RIGHT OF WAY DETAIL

Scale: None

Des. No: 2100321

Contract No: R-43879

Project: 2100321

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Appendix C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 21, 2023

Sample Early Coordination Letter

Re: Early Coordination Letter, Des. 2100231
State Road (SR) 10 Preventative Maintenance Hot Mix Asphalt (HMA) Overlay Project
from 0.58 mile east of the west junction of SR 39 to the east junction of SR 39
Starke County, Indiana

Dear Stakeholder:

The Indiana Department of Transportation (INDOT), with federal funding, proposes a preventative maintenance HMA overlay project along SR 10 in Starke County, Indiana. Specifically, the project begins 0.58 mile east of the west junction of SR 39 and ends at the east junction of SR 39. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

Within the project area, SR 10 is classified as a Major Collector and consists of two 11-foot travel lanes (one in each direction) and 5-foot paved outside shoulders. The site setting is rural and adjacent land uses include grassy rights-of-way, agricultural fields, residential properties, and recreational facilities. A railroad corridor operated by the Hoosier Valley Railroad Museum and a segment of the North Judson Erie Trail cross the project area. Utilities in the area include overhead electric lines along the north side of SR 10 and overhead and underground services along the south side of SR 10.

The need for this project is due to the deteriorating condition of the roadway. The existing pavement is experiencing rutting, potholing, and cracking, that if left uncorrected, will cause a costlier rehabilitation in the future. The proposed work will extend the service life of the existing pavement and improve rideability for the traveling public.

The project will remove 2 inches of existing asphalt and replace it with 2 inches of hot mix asphalt (HMA). Milled centerline and shoulder rumble strips will also be installed. One 12-inch culvert (CLV-010-075-45.64), located 0.07 mile east of the North Judson Erie Trail, will be replaced in-kind and portions of the approach guardrail for the bridge over Bogus Run (INDOT Structure No. 010-75-06886 A) will be temporarily removed during construction and reset. No bridge work is proposed. Less than 0.10 acre of right-of-way may be required for this project. No tree clearing or trimming is anticipated as part of this project. During construction, traffic will be maintained through phased lane closures and flaggers. Construction is anticipated to begin in the Spring of 2025.

Parsons environmental staff or representatives may conduct a waters investigation to determine the presence of jurisdictional streams and wetlands. A *Waters of the US Report* will be prepared. All applicable permits will be applied for and acquired before construction begins. Parsons will work in coordination with INDOT Ecology and Waterway Permitting Office (EWPO) to determine the presence and impacts to ecological resources.

This project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and federally threatened Northern long-eared bat (*Myotis septentrionalis*) and the *Indiana Bat and Northern Long-eared Bat Range-Wide Standard Informal Programmatic Consultation* is anticipated to be applied to this project. Project information was uploaded to the United States Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) website to identify if any species listed or proposed to be listed may be present in the area of the proposed action. An Official Species List was generated and no critical habitats, and no other species, other than aforementioned bats, were listed as threatened or endangered.

Regarding Section 106 of the National Preservation Act, the Minor Projects Programmatic Agreement (MPPA) applies to this project. Coordination with INDOT's Cultural Resources Office (CRO) will occur.

Please provide your response within thirty (30) calendar days from the date of this letter. If we do not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. If an extension to the response time is required, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact me at (317) 616-4712 or via email at Michelle.Greene@parsons.com or the INDOT Project Manager, Shawna DeGraff, at (855) 464-6368 or via email at SDegraff@indot.in.gov. Thank you for your consideration.

Sincerely,



Michelle Greene, AICP
Principal Environmental Planner
Parsons

Attachments:

Maps and Project Area Photographs

[See Appendix B for maps and photographs.](#)

The following agencies received Early Coordination Letters:

Federal Highway Administration
Federal Office Building
575 N Pennsylvania St., Rm. 254
Indianapolis, IN 46204

US Department of Housing & Urban Development
Metcalf Federal Bldg.
77 W Jackson Blvd. Rm. 2401
Chicago, IL 60604

US Fish and Wildlife Service Northern
Indiana Suboffice
P.O. Box 2616
Chesterton, IN 46304

National Park Service
Midwest Regional Office
601 Riverfront Dr.
Omaha, NE 68102

US Army Corps of Engineers
Indianapolis Regulatory Office
8902 Otis Ave, Suite S106B
Indianapolis, IN 46216

Natural Resources Conservation Service
6013 Lakeside Boulevard
Indianapolis, Indiana 46278

Indiana Department of Natural Resources
Division of Fish and Wildlife
402 W Washington St. Rm. W264, IGC South
Indianapolis, IN 46204

Indiana Geological and Water Survey*
611 N Walnut Grove
Bloomington, IN 47405

Indiana Department of Environmental Management
100 N. Senate Avenue
Indianapolis, IN 46204

Indiana Department of Transportation
LaPorte District
Environmental Services
315 Boyd Boulevard
LaPorte, IN 46350

Starke County Commissioner
6715 W. 400 S.
North Judson, IN 46366

Starke County Council
6715 W. 400 S.
North Judson, IN 46366

Starke County Emergency Management
53 E Mound Street
Knox, IN 46534

Starke County Health Department
108 N Pearl Street
Knox, IN 46534

Starke County Highway Department
3835 East 250 North
Knox, IN 46534

Starke County Planning Commission
53 East Mound Street
Knox, IN 46534

Starke County Sheriff
5435 E State Road 8
Knox, IN 46534

Starke County Surveyor
53 East Mound Street
Knox, IN 46534

Knox Community School Corporation
#2 Redskin Trail
Knox, IN 46534

Town of North Judson Town Council
310 Lane Street
North Judson, IN 46366

Town of North Judson Police Department
206 Keller Avenue
North Judson, IN 46366

Town of North Judson Fire Department
209 E. Main St.
North Judson, IN 46366

Town of North Judson
Water/Sewer/Street Department
310 Lane Street
North Judson, IN 46366

North Judson-San Pierre School Corporation
801 Campbell Drive
North Judson, IN 46366

Hoosier Valley Railroad Museum
507 Mulberry Street
North Judson, IN 46366

Prairie Trails Club
BLPOA Community Center
6996 South State Road 10
Knox, IN 46534

**State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment**

DNR#: ER-25882

Request Received: August 21, 2023

Requestor:

Michelle Greene
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204

Project:

SR 10 MA overlay, from 0.58 miles east of the west junction of SR 39 and ends at the east junction of SR 39, and 1 small structure (CLV-010-075-45.64) replacement over UNT Pine Creek, 0.07 miles east of the North Judson Erie Trail; Des 2100231

County/Site Info: Starke County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

This proposal will require the formal approval of our agency for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure) or qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 1997. Please include a copy of this letter with the permit application, if required.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Pavement Rehabilitation

Pavement rehabilitation projects typically do not have a significant impact on fish, wildlife, and botanical resources if best management practices (BMPs) are in place to limit the migration of polycyclic Aromatic hydrocarbons (PAHs) into local waterways. PAHs are a byproduct of asphalt and coal tar-based sealants and negatively impact aquatic systems. The use of sealants that are free of petroleum and coal tar-based products is encouraged whenever possible. Contaminated road runoff can significantly impact the aquatic environment through increased turbidity and release of sediment into the stream which can be harmful to fish and other aquatic organisms, their eggs, and their food supply. Where possible, road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species. We

recommend the use of pollutant trapping technology such as storm drain inserts to reduce the runoff of roadside pollutants.

B) Wildlife Passage

Small culverts can provide aquatic and terrestrial wildlife passage opportunities to reduce wildlife vehicle interactions and improve roadway safety. The replacement drainage structure, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for replacement structures is recommended whenever possible to improve wildlife/vehicle safety. Bank lines should be maintained or restored within structures to allow for wildlife passage above the ordinary high-water mark where appropriate. There are several techniques and materials for incorporating wildlife passage into the design of a crossing structure if maintaining or restoring banklines is not possible. All wildlife passage designs must include a smooth level pathway a minimum of 1-3 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (2, 53, 73, etc.) tied into existing elevations both upstream and downstream. The width and location of the wildlife pathway is dependent on the wildlife species using the area.

Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/ccrc/tool/fishxing-fish-passage-learning-systems>

<https://www.fs.usda.gov/wildlifecrossings/library/index.php>

https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/

<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>

C) Wetland Habitat

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and the US Army Corps of Engineers (USACE) 404 program.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Northern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Do not excavate in the waterway and minimize disturbance to bank vegetation and contain disturbance to within the project limits.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. All excavated material must be properly spread or completely removed from the project site such that erosion and off-site sedimentation of the material is prevented.
5. Do not deposit or allow construction/demolition materials or debris to fall or otherwise enter the waterway. Any incidental fallen material or debris in the waterway must be removed within 24 hours using best management practices, particularly lifting material out of the waterway and not dragging it across the streambed whenever possible.
6. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
7. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
8. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis @dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis

Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: September 20, 2023



Organization and Project Information

Project ID:

Des. ID:

Project Title: SR 10 Minor Structural Overlay Project

Name of Organization: Parsons

Requested by: Michelle Greene

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

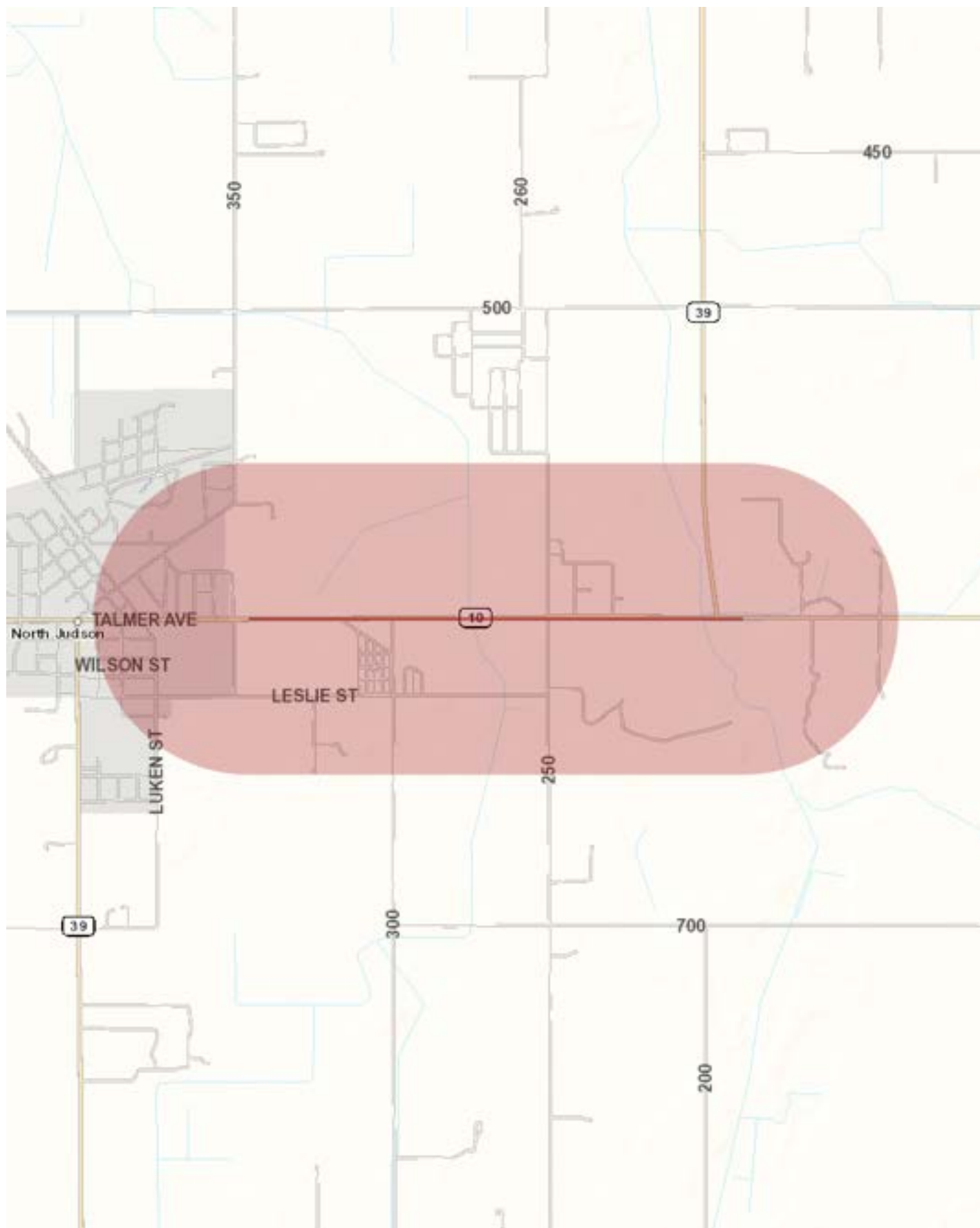
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428







INDIANA DEPARTMENT OF ENVIRONMENTAL MANAGEMENT

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204
(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Brian C. Rockensuess
Commissioner

August 30, 2023

Parsons
Attention: Michelle Greene
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204

Dear Michelle Greene:

Re: Wellhead Protection Area
Proximity Determination
Des No 2100231
State Road (SR) 10 Preventative Maintenance
Hot Mix Asphalt (HMA) Overlay Project
from 0.58 mile east of the west junction of SR 39
to the east junction of SR 39
Starke County, Indiana

Upon review of the above referenced project sites, it has been determined that the proposed project areas **are not located within** a Wellhead Protection Area. However, the proposed project area **is located within 2,200 feet** of a Wellhead Protection Area. If the contact information is needed for the WHPA, please contact the reference located at the bottom of the letter for the appropriate information. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases, we use a 3,000-foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project areas **are not located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

In the future, **please consider using this self-service tool** if it suits your needs. The Drinking Water Branch has a self-service tool which allows one to determine wellhead proximity without submitting the application form. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/> and use the instructions at the bottom of the page.

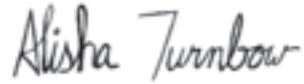


Please Reduce, Reuse, Recycle

Michelle Greene
Page 2

If you have any additional questions, please feel free to contact me at the address above or at 317-233-9158 and aturnbow@idem.in.gov.

Sincerely,

A handwritten signature in black ink that reads "Alisha Turnbow". The script is cursive and fluid.

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality

September 28, 2023

Michelle Greene
101 West Ohio Street, Suite 2121
Indianapolis, Indiana 46204

Dear Ms. Greene:

The proposed SR 10 HMA Overlay Preventative Maintenance Project in Starke County, Indiana (Des. No. 2100231), as referred to in your letter received on September 19, 2023, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN

Digitally signed by JOHN ALLEN
Date: 2023.09.28 12:18:13 -04'00'

JOHN ALLEN
State Soil Scientist

From: [McCloskey, Elizabeth](#)
To: [Greene, Michelle \[NN-US\]](#)
Subject: Re: [EXTERNAL] Early Coordination, Des. No. 2100231, Starke County, Indiana
Date: Tuesday, August 22, 2023 9:46:50 AM
Attachments: [image001.png](#)

Good morning, because the proposed project will have minor impacts on natural resources, and no Federally listed endangered species are known to be present, the U.S. Fish and Wildlife Service will not be providing a comment letter.

Elizabeth McCloskey
U.S. Fish and Wildlife Service
Northern Indiana Suboffice
Ecological Services
Chesterton, Indiana

From: Michelle.Greene@parsons.com <Michelle.Greene@parsons.com>
Sent: Monday, August 21, 2023 2:50 PM
To: McCloskey, Elizabeth <elizabeth_mccloskey@fws.gov>
Subject: [EXTERNAL] Early Coordination, Des. No. 2100231, Starke County, Indiana

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Good Afternoon,

Please see the attached early coordination letter for the following project:

Project: SR 10 Preventative Maintenance HMA Overlay Project
Des. No. 2100231
Location: from 0.58 mile east of the western Junction of SR 39 to the eastern Junction of SR 39
County: Starke

Thank you so much,

Michelle Greene, AICP
Principal Planner
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204
Michelle.Greene@parsons.com
Direct: 317-616-4712 | Mobile: 813-951-5119

From: [Graf, Jennifer \[US-US\]](#)
To: [Graf, Jennifer \[US-US\]](#)
Subject: FW: [EXTERNAL] Re: Early Coordination Letter Response, Des. 2100231, SR 10 HMA Overlay Project (Starke County, IN)
Date: Wednesday, September 3, 2025 7:41:51 AM

From: Stephen Lucas skadad2008@gmail.com
Sent: Monday, August 12, 2024 10:14 AM
To: DeGraff, Shawna SDegraff_indot.IN.gov
Cc: Greene, Michelle NN-US Michelle.Greene_parsons.com ; Brandon Burgoa brburgoa_indot.in.gov
Subject: E TERNAL Re: Early Coordination Letter Response, Des. 2100231, SR 10 MA Overlay Project (Starke County, IN)

Shawna DeGraff

Good morning,

The Prairie Trails Club offered public comments for an INDOT project on Indiana State Road 10 in Starke County that was indicated as scheduled for 2025. Our comments focused on safety concerns for a bike trail crossing that was originally marked but where those markings have been obliterated. The PTC sought replacement of the markings consistent with original plans for the bike trail crossing that had been approved by the State.

The segment of SR 10 slated for the project has recently been resurfaced. Center and edge striping has been completed. Markings for the bike trail crossing have not been restored.

Is there still a 2025 project in the works, or is the 2024 project in lieu of that project as INDOT determined not to restore the bike crossing markings

I would be most appreciative if you would inform me of the status so that I can update interested persons locally.

Respectfully,

Steve Lucas, Chair
Board of Directors
Prairie Trails Club Inc.
PO Box 72
North Judson, IN 46366

317-437-7032

On Sep 8, 2023, at 8:02 AM, DeGraff, Shawna SDegraff_indot.IN.gov wrote:

Good morning,

I received your email. Thank you

Respectfully,

Shawna DeGraff

Project Manager

Capital Program Management INDOT LaPorte District

315 East Boyd Boulevard

LaPorte, IN 46350

Cell: (219) 214-3442

Email: sdegraff@indot.in.gov

From: Stephen Lucas skadad2008@gmail.com

Sent: Thursday, September 7, 2023 7:20 AM

To: Michelle.Greene@parsons.com; DeGraff, Shawna
SDegraff@indot.IN.gov

Cc: Burgoa, Brandon S BrBurgoa@indot.IN.gov ; Robert Albert
albert46511@gmail.com ; robert.barcus@hoosiervalley.org; Stephen Lucas
skadad2008@gmail.com

Subject: Re: Early Coordination Letter Response, Des. 2100231, SR 10 MA
Overly Project (Starke County, IN)
All

I'm writing as a follow-up to the email below sent on August 28. I may have been remiss in not having included Shawna DeGraff in the prior email so am including her now. I would be appreciative if INDOT would acknowledge receipt of this email. Thanks.

Stephen L. Lucas, Chair
Board of Directors
Prairie Trails Club Inc.
PO Box 72
North Judson, IN 46366

317-437-7032

skadad2008@gmail.com

On Aug 28, 2023, at 11:51 AM, Stephen Lucas <skadad2008@gmail.com>
wrote:

August 28, 2023

Michelle Greene, AICP
Principal Environmental Planner Parsons
Indiana Department of Transportation 100 North Senate Avenue
Room N758-ES
Indianapolis, IN 46204

Re: Early Coordination Letter, Des. 2100231, State Road (SR) 10 Preventative Maintenance of Hot Mix Asphalt (HMA) Overlay Project from 0.58 mile east of the west junction of SR 39 to the east junction of SR 39 Starke County, Indiana

To: Michelle Greene:

Thank you for the opportunity to comment regarding Early Coordination with respect to the referenced HMA Overlay Project scheduled to take place in the spring of 2025. This comment is submitted by the Prairie Trails Club Inc. (PTC), a 501(c)(3) non-profit that is registered in Indiana. The PTC mission is to advance, promote, and maintain greenways and blueways in northern Indiana. In particular, the PTC seeks to support the Erie Trail in Starke County and its approved connectors.

The PTC has no objection to the HMA Overlay Project and concurs with the conclusion by INDOT that the project is needed. The PTC urges, however, that trail crossing markings be restored as designed in the North Judson Erie Trail S.R. 10 Trail Road Crossing Detail as identified by Territorial Engineering, LLC on March 2, 2009. Attached for your convenience is a copy of p. 13 of the North Judson Erie Trail and Erie Monterey Trail Shared Use Trail Project 2008 (February 2009) as approved and funded by the State of Indiana. The trail within Starke County is now identified by signage as simply the "Erie Trail".

In particular, striping on S.R. 10 is essential. The PTC is aware the specifications for striping may have been modified since 2009 and would support INDOT's application of modified striping materials or locations.

The PTC urges that the need for appropriate crossing markings is more pressing than it was in 2009. At that time the Erie Trail was a three-mile segment with mostly local use. Today the Erie Trail is eleven miles long and hosts transAmerican routes the American Discovery Trail, Great American Rail Trail, and US Bike Route 35 with significant medium or long-distance trekkers unfamiliar with the surroundings. The need for providing a safe environment for walkers, bicyclists, and operators of vehicular traffic is greater than ever.

Again, thank you for the opportunity to comment of this subject. Please send me confirmation that you have received this email.

Respectfully,

Stephen L. Lucas, Chair
Board of Directors
Prairie Trails Club Inc.
PO Box 72

From: [Stephen Lucas](#)
To: [Greene, Michelle \[NN-US\]](#)
Cc: [Stephen Lucas](#)
Subject: [EXTERNAL] Re: Early Coordination, Des. No. 2100231, Starke County, Indiana
Date: Monday, August 21, 2023 7:28:33 PM
Attachments: [image001.png](#)
[SR 10 ECL Letter Merge all ltrs Part25a.pdf](#)

Good Evening, Michelle Greene,

I am writing as a courtesy to acknowledge receipt of this email with letter attached.

If you also sent or planned to send a paper copy of the communication to the address shown on the letter, the mailing may be returned showing lack of service.

Your letter will be shared with the Board of Directors of the Prairie Trails Club, Inc., a 501(c)(3) non-profit corporation, during a meeting anticipated for this Thursday (August 24). You may use this email address or the mailing address shown below should you determine to make further communications to the Prairie Trails Club.

Please let me know if you have questions or comments.

Respectfully,

Stephen L. Lucas, Chair
Board of Directors
Prairie Trails Club, Inc.
PO Box 72
North Judson, IN 46366

(317) 437-7032
skadad2008@gmail.com

On Aug 21, 2023, at 3:26 PM, Michelle.Greene@parsons.com wrote:

Good Afternoon,

Please see the attached early coordination letter for the following project:

Project: SR 10 Preventative Maintenance HMA Overlay Project
Des. No. 2100231
Location: from 0.58 mile east of the western Junction of SR 39 to the eastern Junction of SR 39
County: Starke

Thank you so much,



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

August 09, 2023

Project Code: 2023-0114467

Project Name: Des. 2100231 SR 10 Preventative Maintenance (HMS Overlay) Project

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see <https://www.fws.gov/birds/policies-and-regulations.php>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see <https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2023-0114467
Project Name: Des. 2100231 SR 10 Preventative Maintenance (HMS Overlay) Project
Project Type: Road/Hwy - Maintenance/Modification
Project Description: The Indiana Department of Transportation (INDOT), with federal funding, proposes a preventative maintenance project along 1.48 miles of SR 10 from 0.08 mile east of W 350 S to SR 39 in Starke County, Indiana.

Within the project area, SR 10 is classified as a Major Collector and consists of two 11-foot travel lanes (one lane in each direction) and 5-foot paved outside shoulders. The site setting is rural. Adjacent land uses include grassy right-of-way, agricultural fields, residential properties, and a recreational trailhead. A railroad corridor operated by the Hoosier Valley Railroad Museum and a segment of the North Judson Erie Trail cross the project area. Utilities in the area include overhead electric lines along the north side of SR 10 and overhead and underground services along the south side of SR 10.

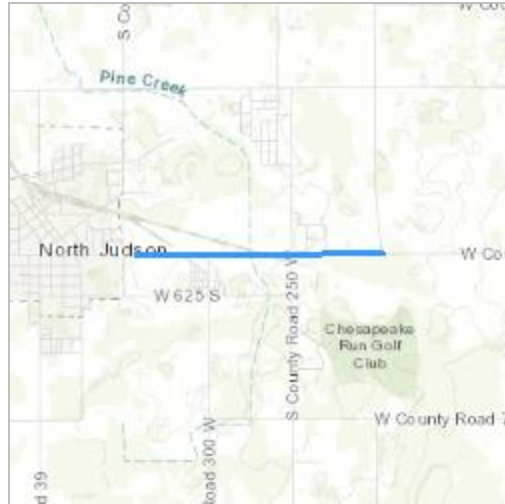
The need for the project is due to the deteriorating condition of the roadway. According to the Abbreviated Engineering Report, dated January 15, 2021, the existing pavement is experiencing rutting, potholing, and cracking, that if left uncorrected, will cause a costlier rehabilitation in the future. The proposed work will extend the life of the existing pavement and improve rideability for the traveling public.

The project will remove 2 inches of existing asphalt pavement and be replaced with 2 inches of hot mix asphalt (HMA). Milled centerline and shoulder rumble strips will be installed. Six culverts and one bridge are located within the project limits One 18-inch culvert (CLV-010-075-45.64), located 0.07 mile east of the North Judson Erie Trail crossing, will be replaced in-kind and portions of the approach guardrail for the bridge over Bogus Run (INDOT Structure 010-75-06886 A) will be reset. All work will occur within existing INDOT right-of-way. No work is proposed for the bridge or the remaining culverts.

No new permanent right-of-way will be required for this project and no tree clearing will occur. During construction, traffic will be maintained through phased lane closures and flaggers. One partial-day full-road closure is also anticipated to accommodate the culvert replacement. Construction is anticipated to begin in the Spring of 2025.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.21517715,-86.75040995,14z>



Counties: Starke County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 5 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

The birds listed below are birds of particular concern either because they occur on the [USFWS Birds of Conservation Concern \(BCC\)](#) list or warrant special attention in your project location. To learn more about the levels of concern for birds on your list and how this list is generated, see the FAQ [below](#). This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the [E-bird data mapping tool](#) (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found [below](#).

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
American Golden-plover <i>Pluvialis dominica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds elsewhere
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.	Breeds Oct 15 to Aug 31
Bobolink <i>Dolichonyx oryzivorus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 20 to Jul 31
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Mar 15 to Aug 25
Golden Eagle <i>Aquila chrysaetos</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1680	Breeds elsewhere
Lesser Yellowlegs <i>Tringa flavipes</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9679	Breeds elsewhere
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Sep 10
Ruddy Turnstone <i>Arenaria interpres morinella</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA	Breeds elsewhere
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA	Breeds elsewhere
Short-billed Dowitcher <i>Limnodromus griseus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9480	Breeds elsewhere

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the

FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)

Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

No Data (—)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.

■ probability of presence ■ breeding season | survey effort — no data



Additional information can be found using the following links:

- Birds of Conservation Concern <https://www.fws.gov/program/migratory-birds/species>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>

MIGRATORY BIRDS FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly

important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the list of migratory birds that potentially occur in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [Rapid Avian Information Locator \(RAIL\) Tool](#).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering or migrating in my area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may query your location using the [RAIL Tool](#) and look at the range maps provided for birds in your area at the bottom of the profiles provided for each bird in your results. If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

What if I have eagles on my list?

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of

certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

RIVERINE

- [R2UBFx](#)

FRESHWATER EMERGENT WETLAND

- [PEM1A](#)

IPAC USER CONTACT INFORMATION

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United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

August 09, 2023

Project code: 2023-0114467

Project Name: Des. 2100231 SR 10 Preventative Maintenance (HMA Overlay) Project

Subject: Consistency letter for the 'Des. 2100231 SR 10 Preventative Maintenance (HMA Overlay) Project' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated August 09, 2023 to verify that the **Des. 2100231 SR 10 Preventative Maintenance (HMA Overlay) Project** (Proposed Action) may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to section 7(a)(2) of the ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

This "may affect - not likely to adversely affect" determination becomes effective when the lead Federal action agency or designated non-federal representative requests the Service rely on the PBO to satisfy the agency's consultation requirements for this project.

Please provide this consistency letter to the lead Federal action agency or its designated non-federal representative with a request for review, and as the agency deems appropriate, submit for concurrence verification through the IPaC system. The lead Federal action agency or designated non-federal representative should log into IPaC using their agency email account and click "Search by record locator". They will need to enter the record locator **491-130168789**.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet bats are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service. If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate
- Tricolored Bat *Perimyotis subflavus* Proposed Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

Des. 2100231 SR 10 Preventative Maintenance (HMA Overlay) Project

DESCRIPTION

The Indiana Department of Transportation (INDOT), with federal funding, proposes a preventative maintenance project along 1.48 miles of SR 10 from 0.08 mile east of W 350 S to SR 39 in Starke County, Indiana.

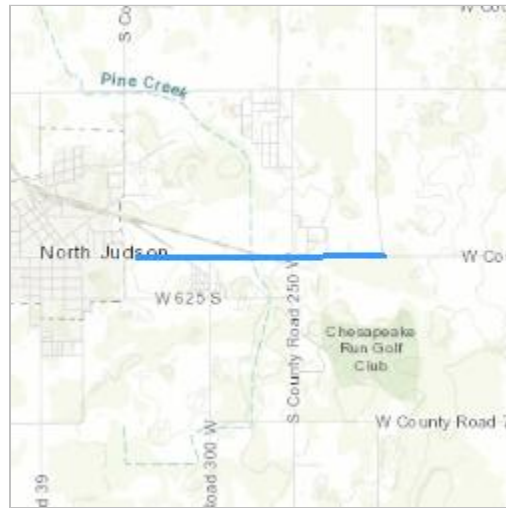
Within the project area, SR 10 is classified as a Major Collector and consists of two 11-foot travel lanes (one lane in each direction) and 5-foot paved outside shoulders. The site setting is rural. Adjacent land uses include grassy right-of-way, agricultural fields, residential properties, and a recreational trailhead. A railroad corridor operated by the Hoosier Valley Railroad Museum and a segment of the North Judson Erie Trail cross the project area. Utilities in the area include overhead electric lines along the north side of SR 10 and overhead and underground services along the south side of SR 10.

The need for the project is due to the deteriorating condition of the roadway. According to the Abbreviated Engineering Report, dated January 15, 2021, the existing pavement is experiencing rutting, potholing, and cracking, that if left uncorrected, will cause a costlier rehabilitation in the future. The proposed work will extend the life of the existing pavement and improve rideability for the traveling public.

The project will remove 2 inches of existing asphalt pavement and be replaced with 2 inches of hot mix asphalt (HMA). Milled centerline and shoulder rumble strips will be installed. Six culverts and one bridge are located within the project limits One 18-inch culvert (CLV-010-075-45.64), located 0.07 mile east of the North Judson Erie Trail crossing, will be replaced in-kind and portions of the approach guardrail for the bridge over Bogus Run (INDOT Structure 010-75-06886 A) will be reset. All work will occur within existing INDOT right-of-way. No work is proposed for the bridge or the remaining culverts.

No new permanent right-of-way will be required for this project and no tree clearing will occur. During construction, traffic will be maintained through phased lane closures and flaggers. One partial-day full-road closure is also anticipated to accommodate the culvert replacement. Construction is anticipated to begin in the Spring of 2025.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.21517715,-86.75040995,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

No

10. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

11. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

12. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

14. Does the project include slash pile burning?

No

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

No

16. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

17. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

18. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

19. Will the project install new or replace existing **permanent** lighting?
No
20. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?
No
21. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.
Yes
22. Will the project raise the road profile **above the tree canopy**?
No
23. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?
Automatically answered
Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO
24. **General AMM 1**
Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?
Yes
25. **Lighting AMM 1**
Will *all* **temporary** lighting be directed away from suitable habitat during the active season?
Yes

PROJECT QUESTIONNAIRE

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?
N/A
2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?
N/A

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on July 27, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion \(dated March 23, 2023\) for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

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State: IN

Zip: 46204

Email: michelle.greene@parsons.com

Phone: 8139515119

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

Appendix D

Section 106 of the National Historic Preservation Act

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B-1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B-9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I. INDOT-CRO staff will be responsible for completion of Part II.*

Original Submission Date: 9/26/2024

Amended Submission Date*:

Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use **red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization):

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
Zionsville, IN 46077
Linda@weintrautinc.com
317-733-9770

Project Designation Number: 2100231

Route Number: State Road (SR) 10

Feature crossed (if applicable):

City/Township: Town of North Judson/Wayne Township

County: Starke County

Project Description: The proposed project area is located on State Road (SR) 10, from approximately 0.58 mile east of West Junction of SR 39 to East Junction of SR 39, located in North Judson in Wayne Township, Starke County, Indiana. More specifically, the project is located in Sections 14, 15, 16, 21, 22 and 23 of Township 32 North, and Range 3 West, as shown on the United States Geological Survey (USGS) 7.5' North Judson and Denham, Indiana topographic quadrangles.

The proposed project includes resurfacing SR 10 throughout the project area, replacing the existing 12-inch diameter corrugated metal pipe (CMP) (CLV-29637) with a new 12-inch diameter CMP and installing riprap at the culvert outlet, and resetting the guardrail on the approaches of the existing SR 10 bridge within the project limits (INDOT Structure No. 010-75-06886 A).

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type:

Feature Crossed	Str Number	NBI Number	Type
Drainage	CLV 010-075-45.64/ 010-075-45.65	NA	CMP
Bogus Run		NBI-002960	

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (<https://www.in.gov/indot/2531.htm>)?

☐ Yes ☒ No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

☐ Yes ☐ No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

☒ Yes ☐ No

If yes was checked above, please check all that apply:

☐ Permanent ☐ Temporary ☒ Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way: 1.34 acres of right of way will be taken as reacquisition. The right of way will be taken from the north side of State Road 10 around CLV 010-075-45.64 for grading and the addition of riprap (see plans).

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

☒ Yes ☐ No

Archaeology (check one):

☐ All proposed activities are presumed to occur in previously disturbed soils.*

**INDOT-CRO will notify you if project area includes undisturbed soils and requires an archaeological reconnaissance.*

☒ Project takes place in undisturbed soils and the archaeology report is included with the submission.*

**If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO Archaeology Team Lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (INDOT will highlight applicable conditions in yellow):

B-9. Installation, replacement, repair, lining, or extension of culverts and other drainage structures under the conditions listed below **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

i. Work occurs in previously disturbed soils; *OR*

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present

within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

One of the conditions below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work does not involve installation of a new culvert and other drainage structure, and there are no impacts to unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and retaining walls, under one of the following conditions (*Condition a, Condition b, or Condition c must be satisfied*):
 - a. The structure exhibits no wood, stone, or brick structures or parts therein; *OR*
 - b. The structure exhibits only modern wood, stone, or brick structures or parts therein; *OR*
 - c. The structure exhibits non-modern wood, stone, or brick structures or parts therein and the following conditions are met (*BOTH Condition 1 AND Condition 2 must be met*):
 1. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
 2. The structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. Under this condition, a qualified professional (meeting the Secretary of Interior's Professional Qualification standards [48 Federal Register (FR) 44716]) must prepare an analysis and justification that the structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. This documentation must be reviewed and approved by INDOT Cultural Resources Office.
- ii. Work involves the installation of a new culvert and other drainage structures *AND/OR* there may be impacts to unusual features, including historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and retaining walls, under the following conditions (*BOTH Condition a and Condition b must be satisfied*):
 - a. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
 - b. The subject structure exhibits one of the characteristics described below (*Condition 1, Condition 2 or Condition 3 must be satisfied*).
 1. The structure exhibits no wood, stone, or brick structures or parts therein; *OR*
 2. The structure exhibits only modern wood, stone, or brick structures or parts therein; *OR*
 3. The structure exhibits non-modern wood, stone, or brick structures or parts therein but lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. Under this condition, a qualified professional (meeting the Secretary of Interior's Professional Qualification standards [48 Federal Register (FR) 44716]) must prepare an analysis and justification that the structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. This documentation must be reviewed and approved by INDOT Cultural Resources Office.

Check ☐ if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included.

Check ☐ if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included.

Part II: Completed by INDOT-CRO

Information reviewed (please check all that apply):

General project location map ☒ USGS map ☒ Aerial photographs ☒ Soil survey data ☒

General project area photos ☒ Archaeology Reports ☒ Historic Property Reports ☐

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report ☒

Bridge inspection information/iTAMS ☒ Historic Bridge Inventory Database ☒

SHAARD ☐ SHAARD GIS ☒ Streetview Imagery ☒ County GIS Data/Property Cards ☒

Other (please specify):

Kidwell, Aaron and Craig Arnold

2025 Phase Ia Archaeological Reconnaissance: Proposed HMA Overlay and Preventative Maintenance on State Road 10, from approximately 0.58 mile east of West Junction State Road 39 to east Junction State Road 39, in North Judson, Wayne Township, Starke County, Indiana (INDOT Des. No. 2100231). Report on file, Indiana Department of Transportation, Cultural Resources Office, Indianapolis, IN.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. Yes ☐ No ☒

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. Yes ☐ No ☒

Additional Comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Starke County. No listed resources are present immediately adjacent to the project area, a distance that serves as an adequate area of potential effects given the project scope and terrain.

The National Register & Indiana Historic Sites and Structures Inventory (IHSSI) information for Starke County is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBM). All sites were reviewed through the IHBBM, which contains the most recently updated SHAARD information. There is one (1) documented property located immediately adjacent to the HMA overlay portion of the project area:

- IHSSI# 149-466-35015; Highland Cemetery; SW Corner SR 10; c. 1930, 1950; rated "Notable."

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register-eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "Notable" might possess the necessary level of significance after further research. Properties rated "Outstanding" usually possess the necessary level of significance to be considered National Register-eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

It should be noted that the remainder of this review focuses only on the project areas related to culvert replacement/installation. The rest of the project is limited to Category A-4 and Category A-6 activities only.

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Starke County GIS website. The project area is located along SR 10 in a rural setting outside of North Judson. No above-ground structures were observed immediately adjacent to the B-9 portion of the project area.

CLV-29637 was not identified in a review of the IHBBCM. The structure was reviewed through photographs provided by the consultant that demonstrate that the structure is a 12-inch steel corrugated pipe. The structure is not included in INDOT's Total Assets Management System (iTAMS) due to its small size. Photographs confirm the structure does not exhibit any wood, stone, or brick structures or parts therein. Additionally, it does not appear to possess any historical or engineering significance.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia archaeological reconnaissance submitted by Weintraut on behalf of American Structurepoint, Inc. (Kidwell and Arnold 2025).

A 9.85-acre survey area was examined through a combination of systematic shovel probing (n=20) and visual inspection of disturbed areas. The area encompassing the intersection of SR 10 has been previously disturbed from the construction of the state road, existing railroad tracks, road grade and fill, existing culvert with associated drainage, roadside ditching, residential infrastructure, landscaping, paved driveways, and buried utilities. Shovel test probes were placed on the north and south sides of SR 10 in a recently tilled agricultural field and in an open, grassy area in 15 m intervals. As a result, one shovel test was found to be disturbed while the rest were negative. No archaeological sites were documented as a result of the survey and no further investigation is recommended (Kidwell and Arnold 2025).

Therefore, there are no archaeological concerns as long as the project scope and footprint do not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Indiana Department of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Emily Minett, Clint Kelly, and KayLee Blum

INDOT Approval Date: 2/26/2025

Amendment Approval Date (if applicable):

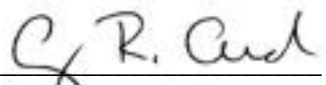
****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*



**Phase Ia Archaeological Reconnaissance: Proposed HMA Overlay
and Preventative Maintenance on State Road 10, from approximately
0.58 mile east of West Junction State Road 39 to East Junction State
Road 39, in North Judson, Wayne Township, Starke County, Indiana,
INDOT Des. No. 2100231**

Prepared for
**American Structurepoint and Indiana Department of Transportation/
Federal Highway Administration**

Prepared by
WEINTRAUT & ASSOCIATES, INC.
Authors: Aaron Kidwell, B.A. | Craig R. Arnold, M.A. carnold@weintrautinc.com


Principal Investigator: Craig R. Arnold

February 19, 2025

The standard is screened cores / auger probes using ¼" size mesh. If cores / auger probes were not screened, or a different size mesh was utilized, an explanation must be provided in the methods below.

Describe methods.

Additional field investigation comments

The survey was conducted in accordance with Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology Guidebook (IDNR, DHPA 2024b) and the INDOT Cultural Resources Manual (INDOT, CRO 2024).

RESULTS

Summary of relevant regional culture background

SHAARD lists over 200 archaeological sites having been recorded within Starke County, Indiana.

Cultural manifestations near the project area reflect the general cultural sequence of northwest Indiana from the Paleoindian through Historical eras. Within Starke County, there are: one Paleoindian; two Early Archaic; no Middle Archaic; two Late Archaic; two Early Woodland; two Middle Woodland; 13 Late Woodland; six Late Woodland/Mississippian; three Mississippian; 125 Unidentified Prehistoric; and 17 Historic sites (IDNR/DHPA 2024a).

Significant archaeological sites from the Archaic to the Protohistoric periods have been documented in Starke County or adjacent counties. In Starke County, the Round Lake Site consists of five clustered sites (12ST49, 12ST50, 12ST97, 12ST99, and 12ST100) exhibiting cultural material extending from the Late Paleoindian to the Upper Mississippian periods. Site 12ST50 consists of four or five mounds called the Round Lake Mound Group. One mound was previously heavily disturbed by road construction in the early 1900s, and two mounds were excavated by avocational archaeologists in 1931 (Lucas et al 2002). The excavation at the Round Lake Mound Group encountered human remains (De Paepe 1959:26-27; Lucas 1972:93). Only cultural material has been encountered at the other sites and the cultural material has been compared to the Weise Site (12PR35) in adjacent Porter County (Lucas 1972:96; Lucas et al 2002). In Marshall County, the Rouch Site (12MR7) consists of a lithic workshop with cultural materials dated to the Middle Archaic to the Late Woodland/Mississippian eras. In total, five features, all concentrations of fire-cracked rock, were encountered, but no materials associated with a village habitation site were encountered (Bellis 1975). In LaPorte County, the Goodall Site (12LE9) consists of a Middle Woodland mound group and habitation site. Ceramic types from the Goodall site, including Havana Plain, Naples Stamped, Havana zoned, and Hopewell ware, are consistent with the Goodall Tradition as a regional variant of the Havana Hopewell culture (Schurr 1998 22-26).

During the early Historic period, the Miami and the Wea are geographically associated with the northern tributaries of the Wabash River while the Kickapoo and Mascouten appear in the early 18th century in northwestern Indiana where prairies extend from Illinois into Indiana (Jones and Johnson 2016:17). The Potawatomi primarily occupied extreme northern Indiana above the Kankakee River but would extend further south towards the Wabash River later in the period (Jones and Johnson 2016:17). By the 1800s, territory attributed to the Potawatomi included lands in northern Illinois, southeastern Wisconsin, northern Indiana, southern Michigan, and northwestern Ohio (Milwaukee Public Museum 2025). In 1838, the remaining Potawatomi in Indiana were forcibly removed by gunpoint to Kansas in what would be known as the Potawatomi Trail of Death (Citizen Potawatomi Nation Cultural Heritage Center 2025; Kansas Historic Society 2012; Lopez 2024). The Chief Menominee Memorial Site in adjacent Marshall County is located near the beginning of the Trail of Death and was built as a remembrance to the Potawatomi in 1909 (Lopez 2024).

Early European involvement in Starke County was primarily centered on exploiting the wild game and fur-bearing animals of the Grand Kankakee Marsh (Historic Landmarks Foundation of Indiana [HLFI] 2005:12; Werich 1920). The Indiana Legislature established Starke County in 1835, but early settlement was impeded due to the presence of the wet marshes inundating the county with water during most months of the year (HLFI 2005:12; Starke County Centennial Committee 1950:5). The county was officially organized in 1850, and the town of Knox became the first settlement and the county seat in the same year (HLFI 2005:12; Starke County Centennial Committee 1950:9). The dredging of the Grand Kankakee Marsh along with channelizing the Kankakee River destroyed many of the wetlands while allowing for the introduction of intensive agriculture, and railroad lines, which connected the region to the Chicago area, were all major events in the early development of Starke County (HLFI 2005:12-13; McCormick 1902:44-49).

The town of North Judson is located at the western end of the survey area. North Judson has an extensive history tied to the railroad industry. The town was laid out in 1866 at a railroad junction and four different railroad companies would combine to send between 125-188 trains daily through the town, which created an economic boom in the early 20th Century (HLFI:79;

Hoosier Valley Railroad Museum 2024). Eventually, the early railroads would consolidate which led to decline in the 1960s; all four railroads would cease activities by the end of the 1980s (HLFI:79-80; Hoosier Valley Railroad Museum 2024). Agriculturally, mint has been a major product in Wayne Township, but other crops such as hay, corn, soybeans, blueberries, strawberries, onions, and potatoes have also been important commodities to the area (HLFI:75; Starke County Centennial Commission 1950:19-20).

A review of historic maps reveals the following about the reconnaissance area. An 1876 map depicts only one railroad line crossing northwest to southeast through North Judson as well as the Pine Creek and Bogus Run drainages east of town (Baskin, Forster, and Company 1876). An 1898 map shows the precursor of SR 10 and the Chicago and Erie Railroad line crossing through the survey area, but no structures are depicted near the roadway or railroad line within the survey area (Edwards 1898). The Chicago and Erie Railroad line opened between 1881 and 1883, but the line was railbanked and most of the track was removed in 2004 (Hoosier Valley Railroad Museum 2024). The railroad corridor now exists as the North Judson Erie Trail multi-use path and greenway. Additionally, the map has Pine Creek and Bogus Run labeled as drainage ditches, and a new drainage ditch called the Kellar Arm is depicted (Edwards 1898).

Records check (Check all that apply)

- ☐ The project area does not have the potential to contain archaeological resources. *Provide explanation / justification.*
- ☐ There are previously recorded archaeological resources within the project area, but those resources do not warrant additional archaeological investigation. *Provide explanation / justification.*
- ☒ The project area contains previously recorded archaeological resources that warrant additional investigation and/or the project area has the potential to contain archaeological resources. *Provide explanation / justification.*
- ☒ Based upon the records check results, a reconnaissance has been conducted.
- ☐ A cemetery is located within or adjacent to the project area.

Explanation / justification

The potential for undisturbed and unsurveyed ground within the project area suggested that the undertaking had the potential to impact undocumented archaeological resources. As a result, a Phase Ia archaeological reconnaissance of the project area was determined to be necessary.

Phase 1a archaeological reconnaissance (Check all that apply)

- ☐ No Phase 1a reconnaissance was conducted.
- ☒ Phase 1a reconnaissance located no archaeological resources.
- ☐ Previously recorded sites were in the project area.
- ☐ Artifacts and/or features at a previously recorded site(s) within the project area were not discovered. *List the site(s) below.*
- ☐ Phase 1a reconnaissance has identified landforms conducive to buried archaeological deposits. *Describe below.*

List sites.

N/A

Describe landforms.

N/A

Number of shovel probes excavated

20

Number of cores / auger probes

0

Describe disturbances. Attach photographs documenting disturbances.

Resurfacing the roadway is planned for this segment of SR 10, and the survey area overlaps much of the existing road ROW. Therefore, the primary disturbance within the survey area is the SR 10 road corridor. Utilities such as natural gas, fiber optic, and overhead power lines parallel or cross the roadway. A new CMP culvert with riprap at the outlets is planned for the area between the old Chicago and Erie Railroad track and South CR 250 West. The survey extends beyond the ROW in this area and shovel test probes were possible. The primary disturbances in this area included a fiber optic line, the old Chicago and Erie Railroad grade with the North Judson Erie Trail multi-use path now utilizing this corridor, overhead power lines, and a large natural gas pipeline. Additionally, the gravel parking lot and an art installation for the trail extended into the survey area. Figures 10 to 19 are representative photographs of field conditions at the time the survey was conducted.

Actual area surveyed (hectares)

3.99

Actual area surveyed (acres)

9.85

Explain results of fieldwork.

The entirety of the project area totals 3.99 ha (9.85 ac) while the survey area physically reconnoitered by W&A totals only 1.01 ha (2.49 ac), centered around the abandoned rail bed (see Figures 3 to 9).

In total, there were 20 shovel test probes conducted during fieldwork (see Figure 6). A typical soil profile exposed during shovel testing consisted of a black (10YR 2/1) sandy silt A horizon, extending to depths between 20 cm (7.9 in) and 35 cm (13.8 in). The A-horizon soils were typically situated above a silty clay subsoil having a brown (10YR 5/3) color. Two shovel test probes were disturbed. One of these was located near the North Judson Erie Trail gravel parking lot, and one was located near the large gas pipeline. No deep soils were encountered that are likely to contain buried archaeological deposits, and therefore, no Phase Ic subsurface reconnaissance is recommended. No cultural materials were recovered from the shovel test probes, and no new archaeological sites were recorded.

RECOMMENDATIONS
<p>Records check <i>(Check all that apply)</i></p> <p><input type="checkbox"/> No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.</p> <p><input type="checkbox"/> A Phase 1a archaeological reconnaissance is recommended.</p> <p><input checked="" type="checkbox"/> Based upon the records check results, a Phase 1a archaeological reconnaissance was recommended and has been conducted.</p> <p><input type="checkbox"/> A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.</p>
<p>Phase 1a archaeological reconnaissance <i>(Check all that apply)</i></p> <p><input checked="" type="checkbox"/> It is recommended that the project be allowed to proceed as planned because the Phase 1a archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.</p> <p><input type="checkbox"/> It is recommended that Phase 1c archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase 1a archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.</p>
<p>Other recommendations / commitments</p> <p>W&A located no cultural material or deposits within the current survey area. Therefore, no further archaeology work is recommended as necessary prior to construction.</p>

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

REQUIRED ATTACHMENTS
<p><input checked="" type="checkbox"/> Figure showing project location within Indiana</p> <p><input checked="" type="checkbox"/> USGS topographic map showing the project area <i>(1:24,000 scale)</i></p> <p><input checked="" type="checkbox"/> Aerial photograph showing the project area, land use and survey methods</p> <p><input checked="" type="checkbox"/> Photographs of the project area, including, if applicable, photographs documenting disturbances</p> <p><input checked="" type="checkbox"/> Project plans <i>(if available)</i></p>
<p>Other attachments</p> <p>Information shown on maps is not warranted for accuracy or merchantability. GIS data used to create the maps are from best-known sources existing at this time. However, experience shows that many national datasets are not all inclusive and can have differing projections, precision, and geographic control points. Use of these maps should be limited to planning only, and should not replace field review or background checks with other sources. The accompanying maps within this report are intended to serve as an aid in graphic representation only. The maps created for this report do not represent a legal document.</p>
<p>References cited <i>(See short report instructions for required references to be consulted)</i></p> <p>Baskin, Forster, and Company 1876 Maps of Indiana Counties in 1876. Published by Baskin, Forster, and Company. Electronic document, https://historicmapworks.com/Map/US/67850/Starke+County/Indiana+Counties+1876/Indiana/, accessed July 2024.</p> <p>Bellis, James O. 1975 The Rouch Site: A Lithic Workshop Site in Marshall County, Indiana. Indiana Archaeological Bulletin, Indiana Historical Society, Volume 1, Number 2, February 1975.</p> <p>Brown, Jeffrey G. and Janis K. Kearney 1991 An Archaeological Reconnaissance of the Proposed Wastewater Treatment Facility Located on the East Side of County Road 400 West, at the Northeast Edge of North Judson, Starke County, Indiana. Prepared for Clyde E. Williams & Associates, Inc. by Glenn A. Black Laboratory of Archaeology, Indiana University, Bloomington, Indiana.</p> <p>Citizen Potawatomi Nation Cultural Heritage Center 2025 Trail of Death. Electronic document, https://www.potawatomiheritage.com/encyclopedia/trail-of-death/, accessed January 2025.</p> <p>De Paepe, Duane 1959 An Archaeological Survey of Starke County, Indiana. Indiana Historical Bureau, Indianapolis, Indiana.</p> <p>Edwards, J. P. 1898 Edward's map of Starke Co., Indiana. Knox, Ind.: John P. Edwards. [Map] Retrieved from the Library of Congress, https://www.loc.gov/item/2013593165/.</p> <p>Historic Landmarks Foundation of Indiana [HLFI] 2005 Starke County Interim Report.</p>

Category A consists of projects that, by their nature, have no effect on properties listed in or eligible for inclusion in the National Register of Historic Places (hereinafter referred to as the “National Register”) and do not require review by INDOT Cultural Resources Office. All of the work under this Category must occur in previously disturbed soils, which are defined as soils that have been completely altered or displaced by earthmoving or other modern manipulation.

1. Any work on bridges limited to substructure or superstructure elements without replacing, widening, or elevating the superstructure under the conditions listed below (***BOTH Conditions A and B must be met***). This category **does not** include bridge replacement projects (when both superstructure and substructure are removed):
 - A. The project takes place in previously disturbed soils; *AND*
 - B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied:
 - i. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.
2. All work within interchanges and within medians of divided highways in previously disturbed soils.
3. Replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone or brick structures or parts therein and are in previously disturbed soils.
4. Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
5. Repair, in-kind replacement or upgrade of existing lighting, signals, signage, and other traffic control devices in previously disturbed soils.
6. Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
7. Repair or in-kind replacement of fencing and hardscape landscaping elements and/or replacement of existing plant materials in previously disturbed soils and installation of new fencing and hardscape landscaping elements and plant materials limited to locations within interstate right-of way within previously disturbed soils.
8. Installation of new or modification of existing traffic control devices and systems, including signs, signals, markings, illumination, other warning devices and their supports, to improve safety at railway crossings in previously disturbed soils.
9. Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.

Appendix E

Red Flag Investigation and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Excerpts

Date: October 24, 2023

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Michelle Greene, AICP
Parsons
101 W Ohio Street, Ste 2121
Indianapolis, IN 46204
michelle.greene@parsons.com

Re: RED FLAG INVESTIGATION
DES # 2100231, State Project
Preventative Maintenance Project
State Road (SR) 10, from 0.58 Mile East of Western Junction of SR 39 to Eastern Junction of SR 39
Starke County, Indiana

Hybrid RFI format
per INDOT

PROJECT DESCRIPTION

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) intends to proceed with a roadway resurfacing project along 1.48 miles of SR 10 from 0.08 mile east of W 350 S to SR 39 in Starke County. The project is located in Sections 14, 15, 16, 21, 22, and 23 of Township 32 North, Range 3 West of the Denham and North Judson USGS 7.5 Minute Quadrangle Map. Within the project area, SR 10 consists of two 11-foot travel lanes (one in each direction) and 5-foot paved shoulders. The site setting is rural.

The project will remove 2 inches of existing asphalt pavement and replace it with 2 inches of hot mix asphalt (HMA). In addition, one 15-inch culvert (CLV-010-075-45.64), located 0.07 mile east of the North Judson Erie Trail, will be replaced in-kind, and the approach guardrail for the bridge over Bogus Run (INDOT Structure 010-75-06886 A) will be temporarily removed and reset. No bridge work is proposed. Less than 0.10 acre of right-of-way may be required. No tree clearing is anticipated.

Bridge Work Included in Project: Yes ☐ No ☒ Structure #(s):

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☒ No ☐ Structure #(s) CLV-010-075-45.64

Proposed right of way: Temporary ☐ # Acres _____ Permanent ☒ # Acres <0.10 , Not Applicable ☐

Type and proposed depth of excavation: Up to 18 inches below ground-surface (ft bgs) for culvert replacement.

Maintenance of traffic (MOT): The MOT for the project will require one-way traffic control and flaggers during the resurfacing. A partial-day, full road closure may occur during the culvert replacement and local traffic will be detoured using S 250 W, E 625 S, and S 300 W.

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	2
Airports ¹	N/A	Pipelines	3
Cemeteries	N/A	Railroads	3
Hospitals	N/A	Trails	1
Schools	N/A	Managed Lands	N/A

¹ In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

CLV-010-075-45.64

Recreational Facilities: One (1) recreational facility, the North Judson Erie Trail Rest Park, is adjacent to the south of the culvert. Coordination with the Hoosier Valley RR Museum and the Prairie Trail Club will occur.

ECOLOGICAL INFORMATION SUMMARY

The Starke County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_starke.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Due to the nature of project activities, this project may fall under the guidelines set forth under the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. However, if a Waters of the US (WOTUS) Report is prepared for the project, coordination will need to occur with IDNR at a minimum. Results of the WOTUS report may indicate the need to coordinate with USFWS.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The site setting is rural. Land adjacent to the project area consists of residential properties and farm fields. No information on the presence or absence of bats is available for the maintenance pipes; therefore, additional investigation will be necessary to confirm the presence or absence of bats in the structure will be necessary. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

INFRASTRUCTURE

CLV-010-075-45.64

Recreational Facilities: The North Judson Rest Park is adjacent to the south of the culvert. Coordination with the Hoosier Valley RR Museum and the Prairie Trail Club will occur.

WATER RESOURCES

Direct coordination with INDOT ESD Ecology and Waterway Permitting will occur on all water resources except for the IDEM 303d Listed Streams and Lakes (Impaired).

MINING/MINERAL EXPLORATION: N/A


HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: If a WOTUS Report is prepared for the project, coordination will need to occur with IDNR at a minimum. Results of the WOTUS report may indicate the need to coordinate with USFWS. Additional investigation to confirm the presence or absence of bats in the structure will be necessary. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT ESD concurrence: Peter Washburn (Signature)

Digitally signed by
Peter Washburn
Date: 2023.10.25
10:04:25 -04'00'

Prepared by: Michelle Greene, AICP


Principal Environmental Planner
Parsons

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

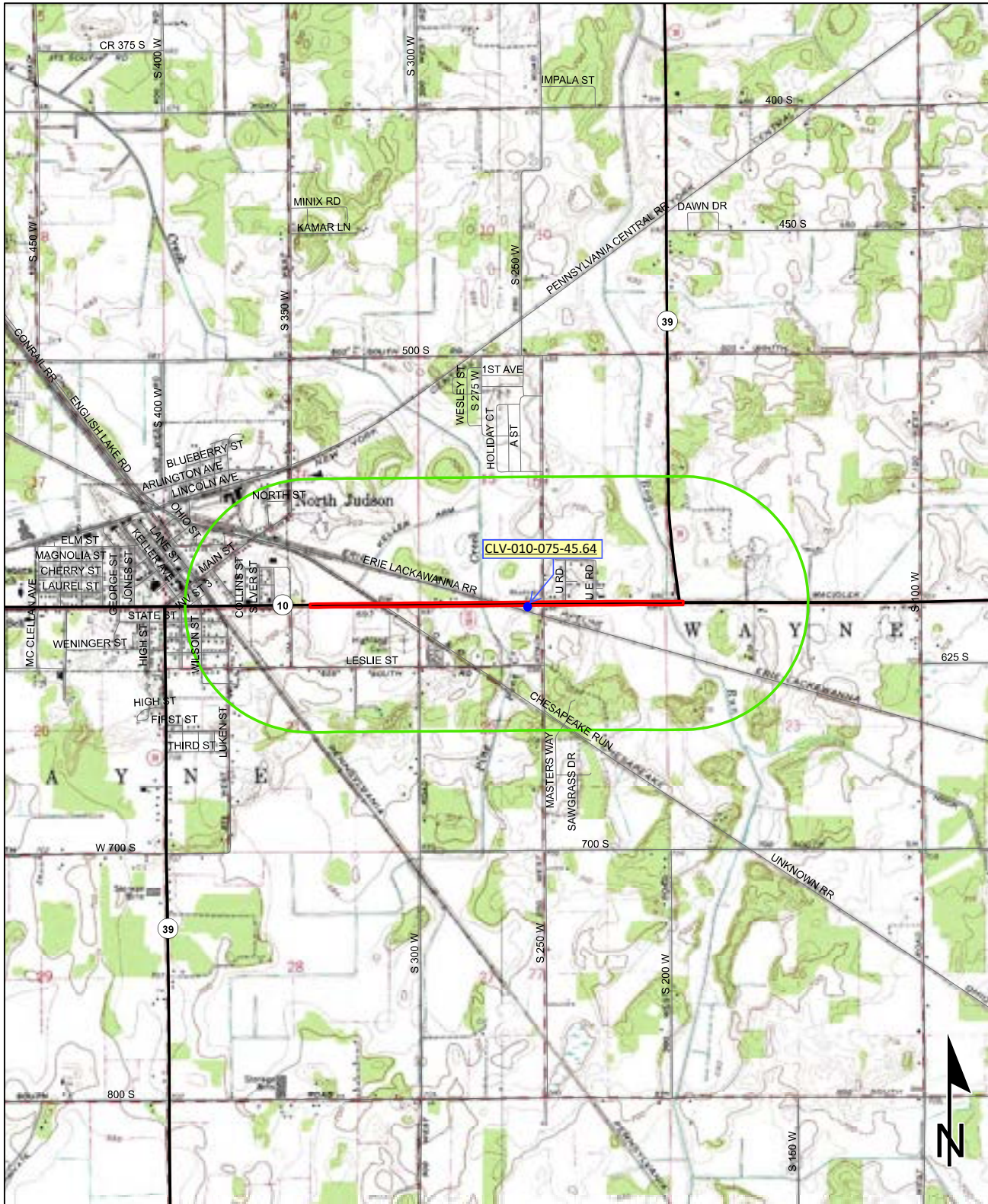
INFRASTRUCTURE: YES

WATER RESOURCES: N/A

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

Red Flag Investigation - Site Location
SR 10, From 0.58 Mile East of Western Junction of SR 39 to Eastern Junction of SR 39
Des. 2100231, Preventative Maintenance
Starke County, Indiana

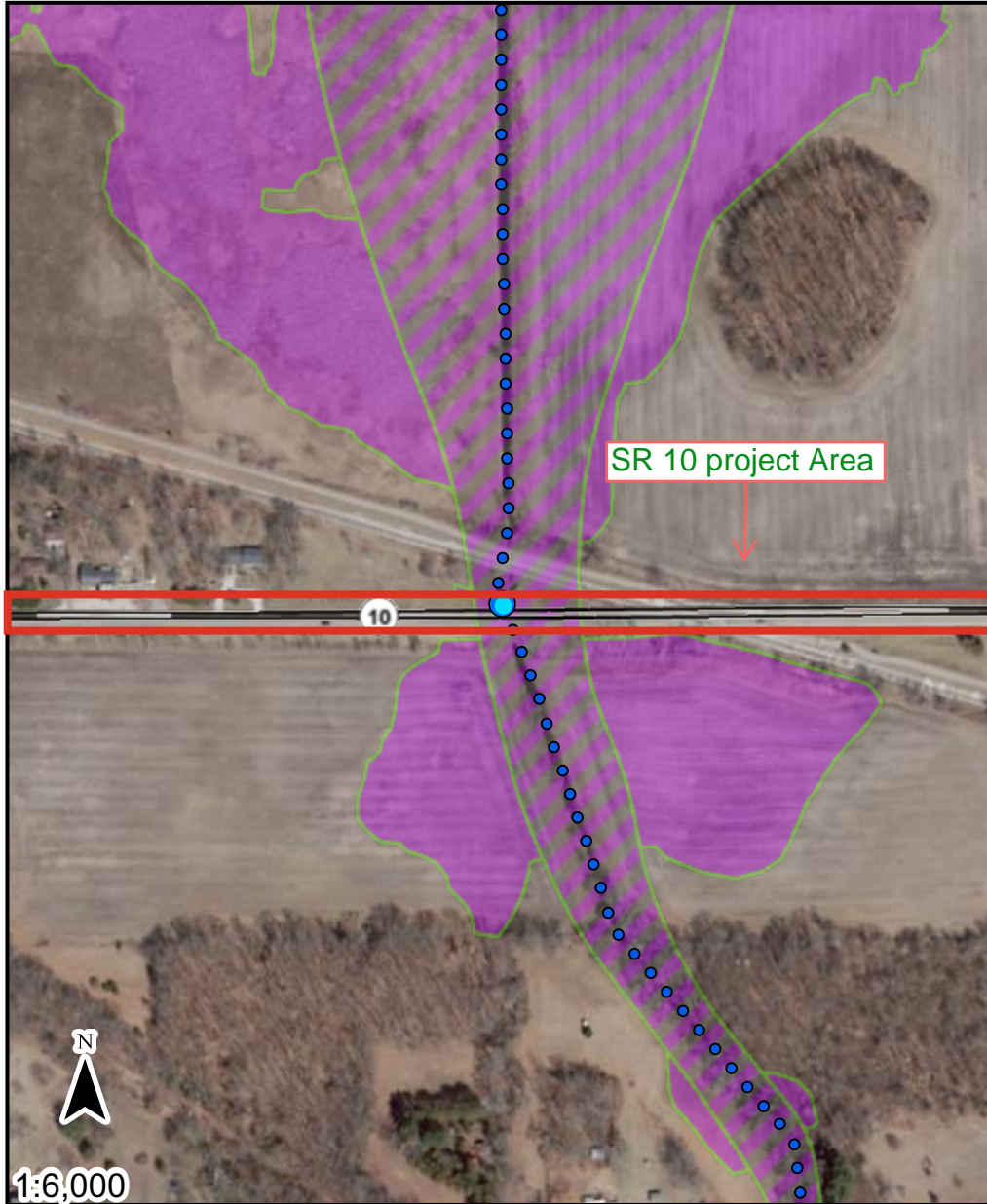


Sources: 0.45 0.23 0 0.45 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

DENHAM & NORTH JUDSON
QUADRANGLES INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Appendix F

Water Resources



- Point of Interest
- Base Flood Elevation Point
- CreateINFIPReport_PointOfInterest

POI

- POI
- 1.0

FloodHazard_BestAvai_DN

- DNR Approximate Floodway
- DNR Approximate Fringe
- Not Mapped

Long: -86.75159501122258

Lat: 41.21527596673789

The information provided below is based on the point of interest shown in the map above.

County: **Starke**

Approximate Ground Elevation: **694.4 feet (NAVD88)**

Stream Name:

Base Flood Elevation: **690.1 Feet (NAVD88)**

Pine Creek

Drainage Area: **Not Available**

Best Available Flood Hazard Zone: **DNR Approximate Floodway**

National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **yes**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **Boz Williams,**

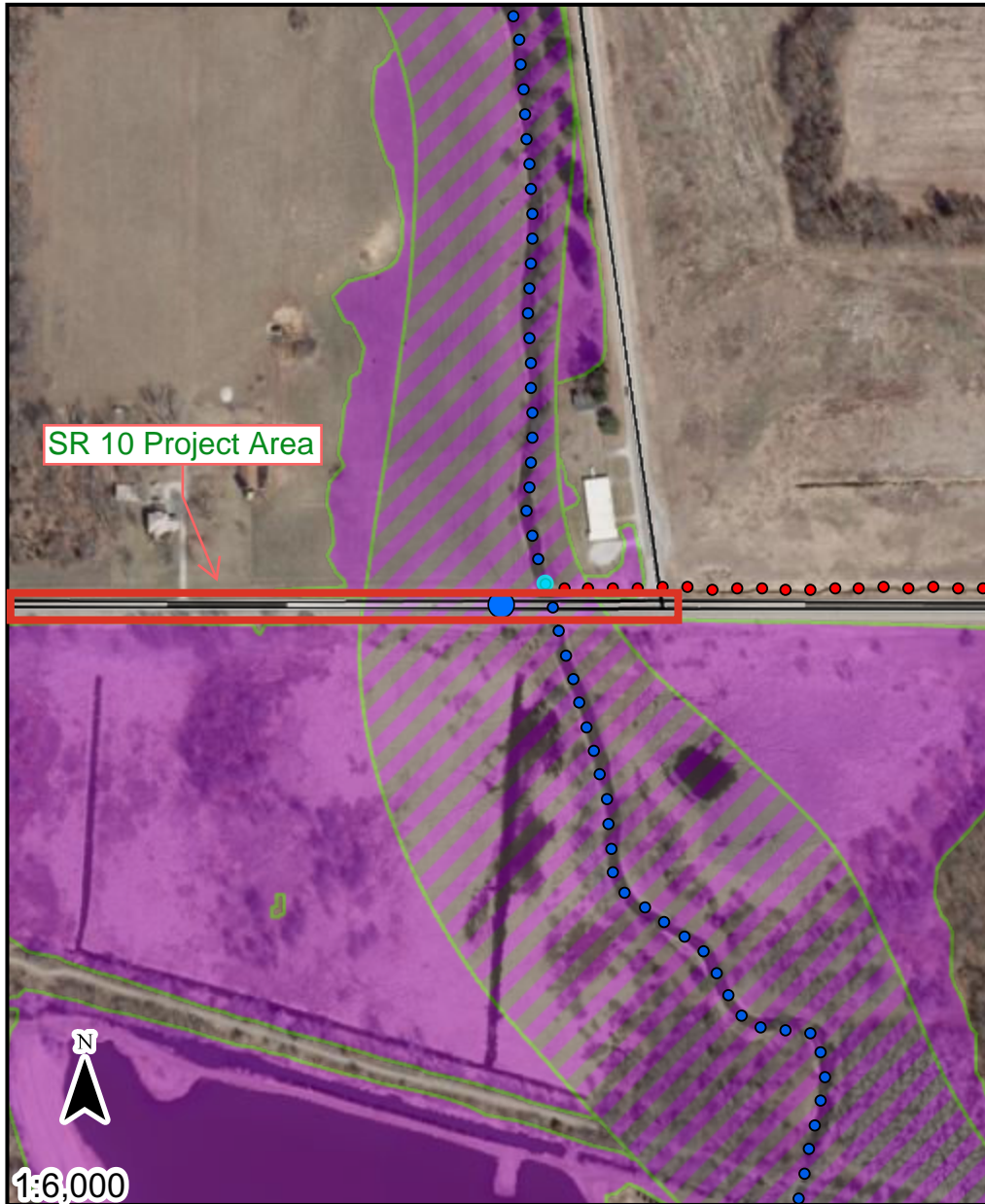
Community Jurisdiction: **Starke County, County proper**

Phone: **(574) 772-9133**

Email: **plancomm75@starke.in.gov**

US Army Corps of Engineers District: **Detroit**

Date Generated: 4/24/2024



- Point of Interest
- Base Flood Elevation Point
- CreateINFIPReport_PointOfInterest

POI

- POI
- 1.0
- 1.5

FloodHazard_BestAvai_DN

- DNR Approximate Floodway
- DNR Approximate Fringe
- Not Mapped

Long: -86.73747586296788

Lat: 41.21524368412296

The information provided below is based on the point of interest shown in the map above.

County: **Starke**

Approximate Ground Elevation: **687.9 feet (NAVD88)**

Stream Name:

Base Flood Elevation: **686.4 Feet (NAVD88)**

Bogus Run

Drainage Area: **Not Available**

Best Available Flood Hazard Zone: **DNR Approximate Floodway**

National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **yes**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **Boz Williams,**

Community Jurisdiction: **Starke County, County proper**

Phone: **(574) 772-9133**

Email: **plancomm75@starke.in.gov**

US Army Corps of Engineers District: **Detroit**

Date Generated: 4/24/2024

**Wetland Delineation and Waters Report
SR 10 Road Improvement in Starke County, Indiana
Road Improvement Project
Des. No. 2100231**

Asset ID Nos: CLV-29637 & 010-75-06886 A

Prepared by: Joshua Iddings and Preeti Samra

Contact Information: psamra@structurepoint.com, 317-547-5580

American Structurepoint, Inc.

Completed Date: February 20, 2024

1.0 Introduction

American Structurepoint, Inc. was contracted by the Indiana Department of Transportation (INDOT) LaPorte District to perform a wetland delineation and waters investigation for the State Road (SR) 10 Road Improvement project located in North Judson in Wayne Township, Starke County, Indiana.

Date of Field Reconnaissance: August 8, 2023

The proposed overall project includes resurfacing of SR 10 throughout the project corridor; however, the investigated areas for this delineation consist of the two areas surrounding a culvert and the SR 10 bridge which would require work outside of pavement. All other work will be confined to the existing pavement. The investigated areas for the two undertakings were set based on preliminary coordination with the project designers, the project scope as understood prior to field investigation, and were set to encompass all proposed work and areas needed for access. The location and approximate boundaries of the investigated areas can be seen in the attached maps and aerial photographs (Appendix A).

Project Location: CLV-29637

Latitude/Longitude		41.215210, -86.749067
North Judson and Denham, Indiana 7.5 Minute Quadrangles		
Sections	Township	Range
15 & 22	32 N	3 W

Project Description: The current proposed project would consist of replacing the existing 12-inch diameter corrugated metal pipe (CMP) (CLV-29637) with a new 12-inch diameter CMP. Riprap is anticipated to be installed at the culvert outlet. The investigated area for CLV-29637 extends approximately 50 feet north and south from the centerline of SR 10 and approximately 0.3 mile east from the intersection of County Road (CR) 300 and approximately 0.06 mile west from the intersection of CR 250.

Project Location: Bridge 010-75-06886 A

Latitude/Longitude		41.215236, -86.737235
Denham, Indiana 7.5 Minute Quadrangles		
Sections	Township	Range
14, 15, 22, & 23	32 N	3 W

Project Description: The current proposed project would consist of resetting the guardrail on the approaches of the existing SR 10 bridge within the project limits (INDOT Structure No. 010-75-06886 A). The

scope may include additional grading and scour protection as design progresses. The investigated area for Bridge 010-75-06886 A extends approximately 50 feet north and south from the centerline of SR 10 and approximately 50 feet west of the intersection of SR 39 and continues east to the intersection of SR 39.

The proposed project is located in Land Resource Region (LRR) L, as recognized by the U.S. Department of Agriculture. As such, this wetland delineation was conducted in accordance with the *Corps of Engineers Wetland Delineation Manual* (Environmental Laboratory, 1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Northcentral and Northeast Region* (U.S. Army Corps of Engineers, 2011).

2.0 Site Characterization – Records Review

2.1 USGS Topographic Mapping

2.1.1 CLV-29637

The topographic map depicts roads passing through primarily cleared, gently rolling landscape as indicated by contour lines within the investigated area for CLV-29637.

2.1.2 Bridge 010-75-06886 A

The topographic map depicts roads passing through primarily cleared, gently rolling landscape as indicated by contour lines, with two streams or other resources mapped within the investigated area for Bridge 010-75-06886 A. These features were field verified as two streams, Maciolek Ditch and Bogus Run, during the August 8, 2023, field investigation.

2.2 National Wetlands Inventory (NWI) Mapping

2.2.1 CLV-29637

The NWI Mapping was reviewed for the investigated area. There are no NWI wetlands mapped within or adjacent to the investigated area. The nearest NWI wetland is mapped approximately 0.18 mile southeast of the investigated area for CLV-29637, and is classified as Palustrine, Emergent, Temporarily Flooded (PEMA) under the Cowardin Classification System.

2.2.2 Bridge 010-75-06886 A

The NWI Mapping was reviewed for the investigated area. Two NWI wetlands are mapped within or adjacent to the investigated area. The first wetland polygon is located adjacent to the southwest quadrant of SR 10 and Bogus Run and is classified as PEM1Ad under the Cowardin Classification System. The second wetland polygon is located adjacent to the southeast quadrant of SR 10 and Bogus Run and is classified as PEM1C under the Cowardin Classification System.

2.3 County Soil Survey

2.3.1 CLV-29637

The *NRCS Soil Survey Geographic Database (SSURGO)* was reviewed to determine soil classification within the investigated area. Soil types mapped within the investigated area include:

Soil Map Unit Summary for CLV-29637			
Map Unit Name	Map Unit Symbol	SSURGO Hydric Rating by Map Unit	NRCS Hydric Soil Category
Maumee sand	Me	100	Hydric
Toto muck, drained	To	100	Hydric

2.3.2 Bridge 010-75-06886 A

The NRCS SSURGO was reviewed to determine soil classification within the investigated area. Soil types mapped within the investigated area include:

Soil Map Unit Summary for 010-75-06886 A			
Map Unit Name	Map Unit Symbol	SSURGO Hydric Rating by Map Unit	NRCS Hydric Soil Category
Adrian muck, drained, prairie peninsula, 0 to 1 percent slopes	AbhAP	100	Hydric
Houghton muck, drained, prairie peninsula, 0 to 1 percent slopes	HplrA	100	Hydric
Prochaska loamy sand, occasionally flooded	Px	100	Hydric

2.4 Aerial Photography

Aerial photography from 2021 ESRI World Imagery was reviewed prior to the August 8, 2023, site visit. The 2021 aerial photography depicts the investigated areas as primarily maintained right-of-way and right-of-way crops. The 2021 aerial photography depicts the investigated areas as they were encountered during the August 8, 2023, field investigation.

2.5 Floodways and Floodplains

2.5.1 CLV-29637

The Indiana Department of Natural Resources (IDNR) Flood Information Portal was reviewed for the investigated area. There are no floodplains mapped within the investigated area for CLV-29637. The nearest mapped floodplain is located approximately 120 feet northwest of the investigated area. The IDNR Approximate Floodway is associated with Pine Creek.

2.5.2 Bridge 010-75-06886 A

The IDNR Flood Information Portal was reviewed for the investigated area. One floodway, associated with Bogus Run, crosses the investigated area for Bridge 010-75-06886 A. The IDNR Approximate Floodway extends east and west from Bogus Run within the investigated area. The floodway is primarily maintained grassy lawn.

2.6 National Hydrography Dataset Flow Lines

2.6.1 CLV-29637

Two United States Geological Survey (USGS) National Hydrography Dataset (NHD) flow lines are present within the investigated area. No unclassified NHD flow lines are present within the investigated area for CLV-29637.

NHD Summary for CLV-29637		
NHD Flow Line Name	Location	Field Verified
Canal/Ditch	Located along the northern shoulder of SR 10.	No, no streams or roadside ditches were observed at this location (Photos 7-10).
Canal/Ditch	Located along the southern shoulder of SR 10.	No, no streams or roadside ditches were observed at this location (Photos 3-6).

2.6.2 Bridge 010-75-06886 A

Four NHD flow lines are present within the investigated area. No unclassified NHD flow lines are present within the investigated area for 010-75-06886 A.

NHD Summary for 010-75-06886 A		
NHD Flow Line Name	Location	Field Verified
Canal/Ditch	Located in the northeast quadrant of SR 10 and Bogus Run.	Field Verified as Maciolek Ditch (Photos 26-28).
Canal/Ditch	Located in the southeast quadrant of SR 10 and Bogus Run.	Field verified as Wetland B (Photos 29-31).
Artificial Path	Crossing north to south beneath SR 10 through the center of the project.	Field Verified as Bogus Run (Photos 16-18 & 35).
Canal/Ditch	Located in the southwest quadrant of SR 10 and Bogus Run.	Field verified as Wetland C (Photos 36-39).

2.7 Legal Drain

The Starke County Surveyor's GIS website (Services Index / GIS Data & Property Record Cards / Starke County, Indiana (starkecountyin.gov)) was reviewed for the presence of legal drains within the investigated areas. No legal drains are present within the investigated areas.

2.8 12-Digit Hydrologic Unit Code

The USGS 12-Digit Hydrologic Unit Code (HUC) mapping was reviewed for the investigated areas. The investigated areas are located within the Pine Creek-Bogus Run 12-Digit HUC (071200010604).

3.0 Field Reconnaissance

The SR 10 Road Improvement project was examined for the presence of wetlands and waters of the U.S. on the site on August 8, 2023. Data points were strategically placed to identify appropriate boundaries of delineated wetlands and to determine the presence or absence of jurisdictional wetlands and waters of the U.S. Three wetlands (Wetland A, Wetland B, and Wetland C) totaling 0.108 acre (527 linear feet) and two streams (Maciolek Ditch and Bogus Run) totaling 336 linear feet (0.066 acre) were identified within the investigated areas. Data sheets and a map indicating the location of data points documenting the field investigation are included in the appendix.

3.1 Wetlands

3.1.1 CLV-29637

No wetlands were delineated within the investigated area for CLV-29637. The investigated area was thoroughly reviewed for areas of hydrophytic vegetation and wetland hydrology. No evidence of wetland hydrology was documented. Additionally, dominant vegetation within the investigated area includes *Lonicera maackii* (amur honeysuckle, FACU), *Erigeron canadensis* (horseweed, FACU), and *Setaria faberi* (green foxtail, FACU).

SR 10 is elevated through the investigated area in comparison to the surrounding landscape. As such, the investigated area generally consists of steep side slopes along the roadway which readily shed water and discourages wetland development. In addition, no wetlands were mapped within the investigated area on the USGS topographic map or the NWI map.

3.1.2 Bridge 010-75-06886 A

Wetland A

Wetland A is an emergent wetland located within a roadside ditch in the northeast quadrant of SR 10 and Bogus Run. Wetland A is located on a bench above the ordinary high water mark (OHWM) of Maciolek Ditch that extends 133 linear feet along the northern bank of the stream before ending at the OHWM of Bogus Run. Wetland A derives water from the adjacent roadway and via Maciolek Ditch. The wetland immediately abuts Maciolek Ditch, an intermittent stream, which drains north to Bogus Run. Bogus Run then continues north outside of the investigated area eventually draining into the Kankakee River, a TNW. Therefore, it is anticipated that Wetland A would be considered a jurisdictional water of the U.S. Due to direct connectivity to a TNW, it is likely that Wetland A is a WOTUS and under the jurisdictional authority of the USACE. The USACE makes all final determinations regarding jurisdiction.

Wetland A is 0.022 acre and would be considered Palustrine, Emergent, Persistent, Temporarily Flooded (PEM1A) under the Cowardin Classification System.

Wetland A would be considered a poor quality wetland due to dominance of invasive species. For reference to field data collected for this wetland see Data Point (DP) 1 included in Appendix C. DP 2 included in Appendix C is representative of the upland areas surrounding Wetland A.

Wetland B

Wetland B is an emergent wetland located within a roadside ditch in the southeast quadrant of SR 10 and Bogus Run. Wetland B begins approximately 16 feet west of the eastern boundary of the investigated area and extends west within the roadside ditch for 154 linear feet before ending at the OHWM of Bogus Run. Wetland B drains west and abuts Bogus Run, a perennial stream, which continues north outside of the investigated area eventually draining into the Kankakee River, a TNW. Therefore, it is anticipated that Wetland B would be considered a jurisdictional water of the U.S. Due to direct connectivity to a TNW, it is likely that Wetland B is a WOTUS and under the jurisdictional authority of the USACE. The USACE makes all final determinations regarding jurisdiction.

Wetland B is 0.029 acre and would be considered PEM1A under the Cowardin Classification System.

Wetland B would be considered a poor quality wetland due to dominance of invasive species and is associated with a roadside ditch used for drainage. For reference to field data collected for this wetland see DP 3 included in Appendix C. DP 4 is included in Appendix C is representative of the upland areas surrounding Wetland B.

Wetland C

Wetland C is an emergent wetland located within a roadside ditch in the southwest quadrant of SR 10 and Bogus Run. Wetland C begins at the OHWM of Bogus Run and extends west 240 linear feet before ending at RSD 3. Wetland C ends as the slope of the roadside ditch steepens. Overall, the slope of RSD 3 is steeper and the ditch is cut to a shallower depth than Wetland C. This results in a noticeable change in topography and vegetation change which was used to delineate the boundaries of the wetlands. The wetland derives water from the adjacent roadway and Bogus Run. Wetland C drains east to and abuts Bogus Run, a perennial stream, which continues north outside of the investigated area eventually draining into the Kankakee River, a TNW. Therefore, it is anticipated that Wetland C would be considered a jurisdictional water of the U.S. Due to direct connectivity to a TNW, it is likely that Wetland C is a WOTUS and under the jurisdictional authority of the USACE. The USACE makes all final determinations regarding jurisdiction.

Wetland C is 0.057 acre and would be considered PEM1A under the Cowardin Classification System.

Wetland C would be considered a poor quality wetland due to dominance of invasive species and is associated with a roadside ditch used for drainage. For reference to field data collected for this wetland see DP 5 included in Appendix C. DP 6 included in Appendix C is representative of the upland areas surrounding Wetland C.

3.2 Non-Wetland Data Point

DP 7 is a non-wetland data point collected due to the presence of hydrophytic vegetation within the roadside ditch in the northwest quadrant of SR 10 and Bogus Run. DP 7 did not have the vegetation, hydrology, or soil to be determined a wetland. Local topography was convex and gently sloped towards Bogus Run preventing ponding and thus inhibiting wetland formation. Refer to the Wetlands Summary table above for additional details.

Wetland Summary													
Wetland ID	Type	Acreage/ Linear Feet	Quality	Likely WOTUS?	DP	Lat/Long	Photo ID	Associated Structure ID	Dominant Vegetation	Hydric Soil Indicator(s)	Hydrology Indicator(s)	Within Wetland?	Notes
Wetland A	EM	0.022/ 133	Poor	Yes	DP 1	41.215347/ -86.736750	19-22	010-75-06886 A	<i>Phalaris arundinacea</i> , <i>Sambucus nigra</i> *	A1	A2, A3, D5	Yes	Wetland A is wholly contained within the investigated area and appears to be associated with the roadway surface drainage system constructed within mapped hydric soil and exhibits dominant hydrophytic vegetation confined to the ditchline
					DP 2	41.215383/ -86.736947	23-25	010-75-06886 A	<i>Phalaris arundinacea</i> , <i>Convolvulus arvensis</i> , <i>Lonicera maackii</i> *, <i>Sambucus nigra</i> *	S5	NA	No	DP 2 is representative of the upland areas surrounding Wetland A.
Wetland B	EM	0.029/ 154	Poor	Yes	DP 3	41.215113/ -86.736875	29-31	010-75-06886 A	<i>Phragmites australis</i>	S5, S7	A2, A3, D5	Yes	Wetland B is wholly contained within the investigated area and appears to be associated with the roadway surface drainage system constructed within mapped hydric soil and exhibits dominant hydrophytic vegetation confined to the ditchline
					DP 4	41.215084/ -86.736715	32-34	010-75-06886 A	<i>Bromus inermis</i> , <i>Phragmites australis</i> , <i>Lonicera maackii</i> *	NA	NA	No	DP 4 is representative of the upland areas surrounding Wetland B.
Wetland C	EM	0.057/ 240	Poor	Yes	DP 5	41.215109/ -86.737719	36-39	010-75-06886 A	<i>Phalaris arundinacea</i>	F6	A2, A3, D5	Yes	Wetland C is wholly contained within the investigated area and appears to be associated with the roadway surface drainage system constructed within mapped hydric soil and exhibits dominant hydrophytic vegetation confined to the ditchline
					DP 6	41.215085/ -86.737875	40-42	010-75-06886 A	<i>Festuca rubra</i> , <i>Bromus inermis</i> , <i>Elaeagnus umbellata</i> *, <i>Juniperus virginiana</i> , <i>Quercus imbricaria</i>	NA	NA	No	DP 6 is representative of the upland areas surrounding Wetland C.
NA	NA	NA	NA	No	DP 7	41.215319, -86.737389	13-15	010-75-06886 A	<i>Phalaris arundinacea</i> , <i>Bromus inermis</i> <i>Schedonorus arundinaceus</i>	NA	NA	No	DP 7 is a non-wetland data point collected due to the presence of hydrophytic vegetation within the roadside ditch in the northwest quadrant of SR 10 and Bogus Run.

*Although the wetland included shrubs these were not a dominant component of the absolute cover of the wetland.

3.3 Drainage Features, Streams, and Other Potential Waters of the U.S.

3.3.1 CLV-29637

No streams were delineated within the investigated area for CLV-29637.

3.3.2 Bridge 010-75-06886 A

Maciolek Ditch

Maciolek Ditch Enters the investigated area as it is conveyed beneath SR 39 via CV 039-075-156.00. The stream is depicted on the USGS Denham, IN 7.5 minute topographic quadrangle as an intermittent stream. StreamStats (<https://www.water.usgs.gov/osw/streamstats/>) reports the upstream drainage area of Maciolek Ditch as approximately 1.987 square miles. Maciolek Ditch was flowing during field investigation on October 8, 2023, and stream flow appears to be intermittent as indicated by the USGS topographic mapping. Maciolek Ditch flows west for approximately 209 linear feet and drains to Bogus Run within the project area. Bogus Run then continues north outside of the investigated area eventually draining into the Kankakee River, a TNW. Therefore, it is anticipated Maciolek Ditch would be considered a jurisdictional waters of the U.S.

A stream assessment (SA1) was completed outside of the zone of influence of CV 039-075-156.00 at coordinates 41.215327, -86.736764. Maciolek Ditch's dominant substrate was sand. The OHWM of Maciolek Ditch at the assessment location was 6.5 feet wide by 1.25 feet deep. Maciolek Ditch would be considered poor quality stream due to channelization and lack of riparian buffer. Maciolek Ditch would be classified as a Riverine, Intermittent, Streambed, Sand (R4SB4) according to the Cowardin Classification System.

Bogus Run

Bogus Run enters the project area south of Bridge 010-75-06886 A and flows north through the investigated area. The stream is depicted on the USGS Denham, IN 7.5 minute topographic quadrangle as a perennial stream. StreamStats (<https://www.water.usgs.gov/-osw/streamstats/>) reports the upstream drainage area of Bogus Run as approximately 27.307 square miles. Bogus Run was flowing during field investigation on October 8, 2023, and stream flow appears to be perennial as indicated by USGS topographic mapping. Bogus Run flows north for approximately 127 linear feet and continues north outside of the investigated area, eventually draining into the Kankakee River, a TNW. Therefore, it is anticipated Bogus Run would be considered a jurisdictional water of the U.S.

A stream assessment (SA2) was completed outside of the zone of influence of CV 010-75-06886 A at coordinates 41.215360, -86.737162. Bogus Run's dominant substrate was sand and artificial (riprap). The ordinary high OHWM of Bogus Run at the assessment location was 16 feet wide by 2.3 feet deep. Bogus Run would be considered poor quality stream due to channelization. Bogus Run would be classified as a Riverine, Upper Perennial, Streambed, Sand (R3SB4) according to the Cowardin Classification System.

Aquatic Resources Summary: Streams											
Delineated Resource	Photos	Lat/ Long	OHWM Width	OHWM Depth	USGS Blue Line & Type	Riffle/Pool Presence	Quality	Substrate	Likely Jurisdiction	Total Linear Feet	Total Acres
Maciolek Ditch	26-28	41.215327, -86.736764	6.5	1.25	Yes, INT	Yes/ Yes	Poor	Sand, Gravel, Artificial	Water of the U.S.	209	0.024
Bogus Run	16-18, 35	41.215360, -86.737162	16	2.3	Yes, PER	Yes/ Yes	Poor	Sand, Artificial	Water of the U.S.	127	0.042
Total										336	0.066

3.4 Other Features (Roadside Ditches)

Surface drainage systems (constructed roadside ditches) are present and noted below. Unless otherwise noted the roadside ditches (RSD) were inspected and were determined to not exhibit defined bed and bank or a continuous OHWM. Therefore, these features are not presumed to be jurisdictional waters of the U.S.

Other Features Summary				
Feature	Photos	Lat/Long	Type	Total Linear Feet
RSD 1	4, 6	41.215082, -86.748703	Vegetated	486
RSD 2	12	41.215318, -86.737663	Vegetated	228
RSD 3	43	41.215128, -86.738055	Vegetated	18
Total				732

3.5 Wildlife Evidence and Concerns

3.5.1 CLV-29637

No wildlife or signs of wildlife were observed during the August 8, 2023, field investigation within the investigated area for CLV-29637. Due to the size of the culvert, wildlife passage through the structure is unlikely. No evidence of use by bats or birds was observed on any of the structures in the investigated area.

3.5.2 Bridge 010-75-06886 A

There are potential wildlife crossing areas on both east and west banks of Bogus Run underneath INDOT Bridge No. 010-75-06886 A for wildlife to cross under SR 10. Although no wildlife or signs of wildlife were observed during the August 8, 2023, field investigation, the corridor under INDOT Bridge No. 010-75-06886 A is likely utilized by wildlife as a passage beneath SR 10. Additionally, Maciolek Ditch enters the investigated area for 010-75-06886 A as it is conveyed beneath SR 39 via CV 039-075-156.00. Due to the size of the culvert and the base flow which spans the entire width of the box culvert, wildlife passage through the structure is unlikely. No evidence of use by bats was observed on any of the structures in the investigated area. Evidence of birds (swallow nests) was observed on the structure.

3.6 Existing Riprap

3.6.1 CLV-29637

Existing riprap was not documented at the inlet or outlet of CLV-29637 (Photos 5 & 8).

3.6.2 Bridge 010-75-06886 A

Existing riprap was documented along SR 10 under both abutments of INDOT Structure No. 010-75-06886 A (Photo 18). Additionally, existing riprap was documented along the channel of Maciolek Ditch at the outlet of CV 039-075-156.00 which conveys the stream beneath SR 39 (Photos 27 & 28).

4.0 Conclusions

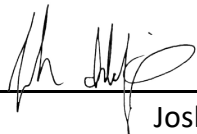
Three wetlands (Wetland A, Wetland B, and Wetland C) totaling 0.108 acre (527 linear feet) and two streams (Maciolek Ditch and Bogus Run) totaling 336 linear feet (0.066 acre) were delineated within the investigated areas. The wetlands and streams all directly abut Bogus Run, which flows north and appears to drain into the Kankakee River, a TNW. Therefore, these wetlands and streams are anticipated to be a jurisdictional waters of the U.S.

All jurisdictional waters of the U.S. are under the regulatory authority of the USACE under Section 404 of the Clean Water Act. Every effort should be taken to avoid and minimize impacts to the waterway and wetlands. If impacts are necessary, then mitigation may be required. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgment based on the guidelines set forth by the USACE.


5.0 Acknowledgement

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator's training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Northcentral and Northeast Region* (U.S. Army Corps of Engineers, 2011), the *USACE Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.

AUTHORS:

 02/20/2024

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 02/20/2024

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In order to reduce the number of pages in the CE document, the following are not included with this report: location and topographic maps, mapped soils, StreamStats reports, IDNR floodplain maps, photographs, and data sheets.



AMERICAN
STRUCTUREPOINT
INC.

Figure 4: NWI and NHD Mapping (1 of 2)

INDOT LaPorte District
315 E. Boyd Blvd.
LaPorte, IN 46350

SR 10 Road Improvement, Des. No. 2100231
CLV-29637

Location: North Judson
Township: Wayne
County: Starke
State: Indiana

Date: 12/26/2023



Figure 4: NWI and NHD Mapping (2 of 2)


SR 10 Road Improvement, Des. No. 2100231
010-75-06886 A

INDOT LaPorte District
315 E. Boyd Blvd.
LaPorte, IN 46350

Location: North Judson
Township: Wayne
County: Starke
State: Indiana

Date: 02/14/2024



	Figure 7: Water Resources and Photo Location Map (1 of 2)		SR 10 Road Improvement, Des. No. 2100231 CLV-29637 Location: North Judson Township: Wayne County: Starke State: Indiana Date: 12/26/2023
	INDOT LaPorte District 315 E. Boyd Blvd. LaPorte, IN 46350		

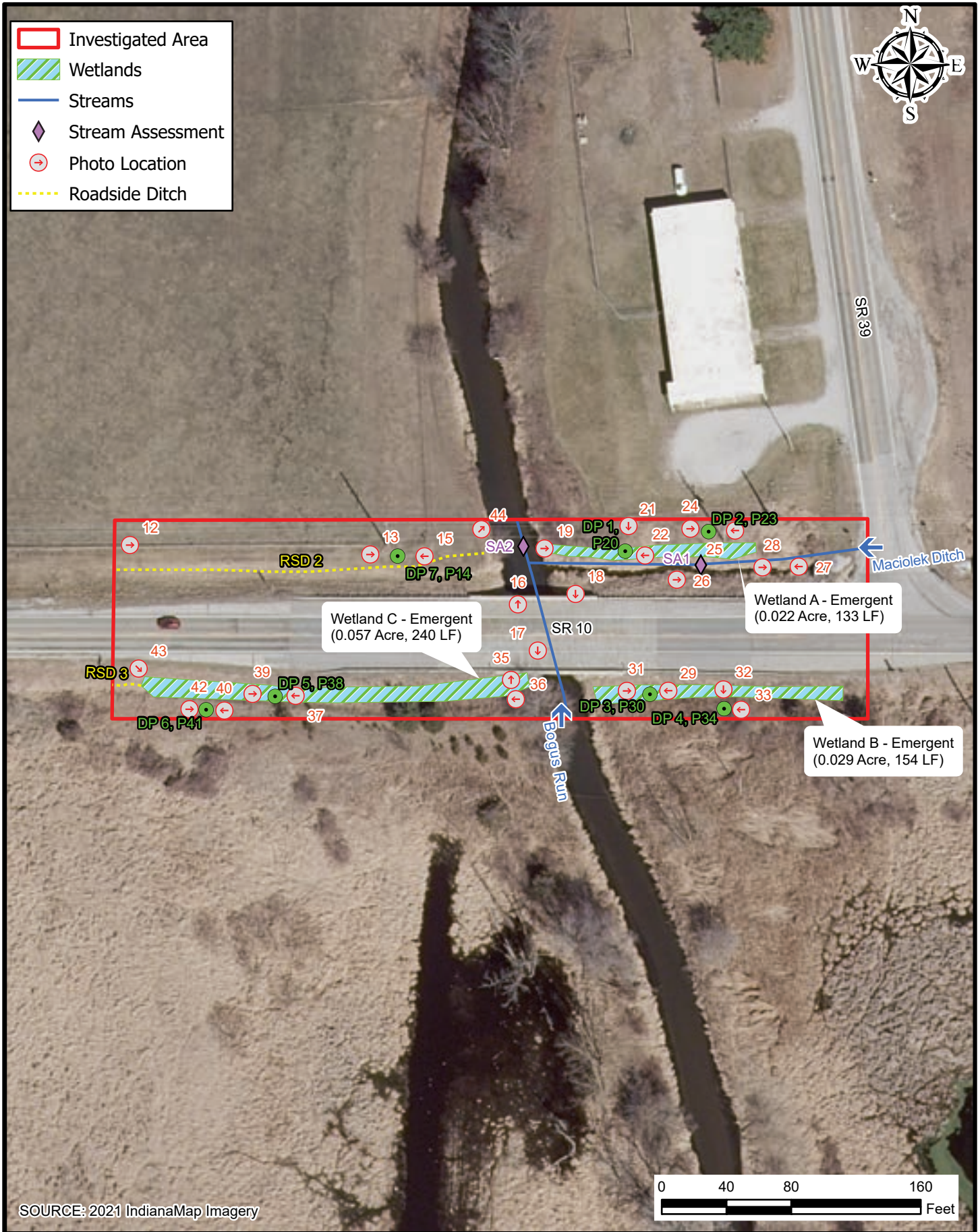


Figure 7: Water Resources and Photo Location Map (2 of 2)

INDOT LaPorte District
315 E. Boyd Blvd.
LaPorte, IN 46350

SR 10 Road Improvement, Des. No. 2100231
010-75-06886 A

Location: North Judson
Township: Wayne
County: Starke
State: Indiana

Date: 12/26/2023

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: 02/20/2024

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Preeti Samra, American Structurepoint, Inc.
9025 River Road, Suite 200 Indianapolis, IN 46240

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

American Structurepoint, Inc. was contracted by INDOT LaPorte District to perform a wetland delineation and waters investigation for the SR 10 Road Improvement project (Des. No. 2100231) located in North Judson in Wayne Township, Starke County, Indiana. The proposed overall project includes resurfacing of SR 10 throughout the project corridor; however, the investigated areas for this delineation consist of the two areas surrounding a culvert and the SR 10 bridge which would require work outside of pavement. All other work will be confined to the existing pavement. The current proposed project for CLV-29637 would consist of replacing the existing 12-inch diameter CMP with a new 12-inch diameter CMP. Riprap is anticipated to be installed at the culvert outlet. The investigated area for CLV-29637 extends approximately 50 feet north and south from the centerline of SR 10 and approximately 0.3 mile east from the intersection of CR 300 and approximately 0.06 mile west from the intersection of CR 250. The current proposed project for INDOT Structure No. 010-75-06886 A would consist of resetting the guardrail on the approaches of the existing SR 10 bridge within the project limits. The scope may include additional grading and scour protection as design progresses. The investigated area for Bridge 010-75-06886 A extends approximately 50 feet north and south from the centerline of SR 10 and approximately 50 feet west of the intersection of SR 39 and continues east to the intersection of SR 39. Three wetlands (Wetland A, Wetland B, and Wetland C) totaling 0.108 acre (527 linear feet) and two streams (Maciolek Ditch and Bogus Run) totaling 336 linear feet (0.066 acre) were delineated within the investigated areas. The wetlands and streams all directly abut Bogus Run, which flows north and appears to drain into the Kankakee River, a TNW. Therefore, these wetlands and streams are anticipated to be a jurisdictional waters of the U.S.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: Indiana County/parish/borough: Starke City: North Judson

Center coordinates of site (lat/long in degree decimal format):

Lat.: 41.215210°N Long.: 86.749067°W

Universal Transverse Mercator: 16T 521035.07E, 4562678.21N

Name of nearest waterbody: Bogus Run

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

August 8, 2023

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
Wetland A	41.215347	-86.736750	0.022 acre (133 linear feet)	Wetland	Section 404
Wetland B	41.215113	-86.736875	0.029 acre (154 linear feet)	Wetland	Section 404
Wetland C	41.215109	-86.737719	0.057 acre (240 linear feet)	Wetland	Section 404
Maciolek Ditch	41.215327	-86.736764	209 linear feet (0.024 acre)	Non-Wetland	Section 404
Bogus Run	41.215360	-86.737162	127 linear feet (0.042 acre)	Non-Wetland	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there "*may be*" waters of the U.S. and/or that there "*may be*" navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

Appendix G

Public Involvement



Sample Notice of Entry Letter
Mailed to Property Owners

RE: Des. No. 2100231
SR 10 Preventative Maintenance Project
from 0.58 mile east of the west junction of SR 39 to the east junction of SR 39
Starke County, Indiana

Notice of Entry for Survey or Investigations

August 21, 2023

Dear Property Owner,

Our information indicates that you own property near the above proposed transportation project. Representatives of the Indiana Department of Transportation will be conducting engineering and/or environmental surveys of the project area in the near future. It may be necessary for the INDOT Representatives to enter onto your property to complete this work. This is permitted by Indiana Code § 8-23-7-26. Anyone performing this type of work has been instructed to identify him or herself to you, if you are available, before they enter your property. If you no longer own this property or it is currently occupied by someone else (i.e. rental, sharecrop), please let us know the name of the new owner or occupant so that we can contact them about the survey.

Please read the attached notice to inform you of what the “Notice of Entry for Survey or Investigation” means.

The design and environmental surveys are needed for the proper planning and design of this part of the SR 10 Preventative Maintenance Project. Engineering survey work would include mapping the location of features such as trees, buildings, fences, drives, ground elevations, etc. Environmental survey work may include the identification and mapping of wetlands, architectural surveys, archaeological investigations (which may involve the survey, testing, or excavation of identified archaeological sites), and various other environmental studies. It is our sincere desire to cause you as little inconvenience as possible during this survey.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

If any problems occur, please contact the field crew or one of the following:

Michelle Greene
Principal Environmental Planner
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204
317-616-4712
Michelle.Greene@parsons.com

Matt Kohut
Project Manager
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204
317-616-1003
Matthew.Kohut@parsons.com

Shawna DeGraff
Project Manager
INDOT–LaPorte District
315 E Boyd Boulevard
LaPorte, IN, 46350
219-214-3442
SDegraff@indot.IN.gov

Please be aware that IC 8-23-7-27 and 28 provides that you may seek compensation from INDOT for damages occurring to your property (land or water) that result from INDOT's entry for the purposes mentioned above in IC 8-23-7-26. In this case, a basic procedure that may be followed is for you and/or an INDOT employee or representative to present an account of the damages to one of the above named INDOT staff. They will check the information and forward it to the appropriate person at INDOT who will contact you to discuss the situation and compensation.

In the event that property damage occurs as a result of work performed during survey, the INDOT Project Manager or Parson's Project Manager can provide you with a form to request compensation for damages. You may contact:

Shawna DeGraff
Project Manager
INDOT – LaPorte District
315 E Boyd Boulevard
LaPorte, IN, 46350
219-214-3442
SDegraff@indot.IN.gov

Matt Kohut
Project Manager
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204
317-616-1003
Matthew.Kohut@parsons.com

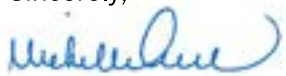
After filling out the form, you can return it to the INDOT Project Manager or Parsons Project Manager for consideration. Please contact either of the representatives above if you have questions regarding the matter, rights, and procedures.

If you are not satisfied with the compensation that INDOT determines is owed to you, Indiana Code 8-23-7-8 provides the following:

The amount of damages shall be assessed by the county agricultural extension educator of the county in which the land or water is located and two (2) disinterested residents of the county, one (1) appointed by the aggrieved party and one (1) appointed by the department. A written report of the assessment of damages shall be mailed to the aggrieved party and the department by first class United States mail. If either the department or the aggrieved party is not satisfied with the assessment of damages, either or both may file a petition, not later than fifteen (15) days after receiving the report, in the circuit or superior court of the county in which the land or water is located.

It is our desire to cause as little inconvenience as possible during our work, and we thank you in advance for your cooperation.

Sincerely,



Michelle Greene, AICP
Principal Environmental Planner
Parsons
101 W. Ohio St., Suite 2121
Indianapolis, IN 46204
Michelle.Greene@parsons.com

/mg

Attachment

Appendix H

Air Quality

SPONSOR	CONTRACT # / LEAD DES	PROJECT GROUP #	STIP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Toal Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2026	2027	2028	2029	2030
Starke County																		
Starke County	2300055	2	Init.	IR 1014	Bridge Inspections	LaPorte	0	STBG	\$224,472.83	Local Bridge Program	PE	\$111,050.92	\$0.00	\$20,151.85	\$71,057.06	\$19,842.01		
										Local Funds	PE	\$0.00	\$27,762.73	\$5,037.96	\$17,764.27	\$4,960.50		
Performance Measure Impacted: Bridge Condition																		
Location: Countywide Bridge Inspection and Inventory Program for Cycle Years 2024-2027																		
Starke County	2300191	2	Init.	IR 1014	Bridge Inspections	Seymour	0	STBG	\$279,167.41	Local Bridge Program	PE	\$118,800.00	\$0.00				\$92,400.00	\$26,400.00
										Local Funds	PE	\$0.00	\$29,700.00				\$23,100.00	\$6,600.00
Performance Measure Impacted: Bridge Condition																		
Location: Countywide Bridge Inspection and Inventory Program for Cycle Years 2028-2031																		
Indiana Department of Transportation	43351 / 2002294	2	Init.	SR 39	Small Structure Replacement	LaPorte	0	STBG	\$1,830,886.00	Bridge Construction	CN	\$1,001,600.00	\$250,400.00			\$1,252,000.00		
Performance Measure Impacted: Bridge Condition																		
Location: At 1.32 S JCT SR 8																		
Comments:Include DES 2002294, 2002295, 2300993																		
Indiana Department of Transportation	43353 / 2001996	2	Init.	SR 8	Bridge Deck Overlay	LaPorte	0	STBG	\$1,370,393.00	Bridge Construction	CN	\$932,554.40	\$233,138.60					\$1,165,693.00
Performance Measure Impacted: Bridge Condition																		
Location: Over Yellow River, 01.32 mile W SR 23																		
Comments:Include DES 2001996																		
Indiana Department of Transportation	43879 / 2100231		Init.	SR 10	HMA Overlay, Preventive Maintenance	LaPorte	1.477	STBG	\$1,713,332.07	Road Construction	CN	\$200,000.00	\$50,000.00			\$250,000.00		
Performance Measure Impacted: Pavement Condition																		
Location: 0.58 mile E of W jct of SR 39 to E jct of SR 39																		
Comments:Include DES 2100231																		
Indiana Department of Transportation	43901 / 2100225		Init.	US 30	HMA Overlay Minor Structural	LaPorte	6.789	NHPP	\$28,548,443.00	Road Construction	CN	\$21,598,400.00	\$5,399,600.00	\$26,998,000.00				
Performance Measure Impacted: Pavement Condition																		
Location: From 2.74 mile E of US 35 to 9.5 mile E of US 35																		
Comments:Include DES 2100225, 2300294																		
Indiana Department of Transportation	43906 / 2100751	2	Init.	SR 39	Bridge Deck Overlay	LaPorte	0	STBG	\$2,002,341.00	Bridge Construction	CN	\$1,406,400.00	\$351,600.00	\$1,758,000.00				
										Bridge Consulting	PE	\$20,000.00	\$5,000.00	\$25,000.00				

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Appendix I

Engineering Studies and Other Documents

From: [Mooney, Molly](#)
To: [Taylor, Matt \[US-US\]](#)
Cc: [sdegraff](#); [Graf, Jennifer \[US-US\]](#)
Subject: [EXTERNAL] RE: SR 10 PM (Des 2100231) Condition Data
Date: Friday, August 29, 2025 10:17:11 AM
Attachments: [image001.png](#)
[image002.png](#)

Matt, the latest data shows an average IRI of 114 in/mi. It has a DBA (Distress Box Area – a representation of cracking) of 1%, this was after it had been chip sealed. Average rut was 0.24 inches.

Molly Mooney, P.E.

Pavement Asset Engineer

Indiana Department of Transportation
LaPorte District

Office: (219) 325-7534

Cell: (219) 851-2246

Email: mmooney@indot.in.gov

[Find us on social media!](#)



From: Matt.Taylor@parsons.com <Matt.Taylor@parsons.com>
Sent: Friday, August 29, 2025 8:44 AM
To: Mooney, Molly <MMooney@indot.IN.gov>
Cc: DeGraff, Shawna <SDegraff@indot.IN.gov>; Jennifer.Graf@parsons.com
Subject: SR 10 PM (Des 2100231) Condition Data

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Molly,

Based on the Pavement Scoping Application this is PK 40198 (RP 44+079 to 46+028). One of the comments we received on our NEPA document review was a request that we include the existing IRI information. Could you please send the most recent Pathway data, or whatever is available, for our use, even if it was pre-chip seal?

Matt Taylor, PE*, MBA
Senior Project Manager
101 West Ohio Street, Suite 2121, Indianapolis, IN 46204
Matt.Taylor@parsons.com
Mobile: +1 317.697.2085

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Pavement Scoping Application Report

5/21/2021

ID: 40198 for model run date 7/27/2020

Pavement

Approved

Last Edited Date	1/15/2021	Work Type	HMA Overlay, Preventive Maintenance
Last Updated By	System, DTIMS	Work Category	District Pavement Project (Non-I)
Proposed FY	2026	Score	70
Pre-DES			

Pavement Project Details

Pavement Section ID	40198	State Log Date	07/27/2020
Route	SR 10	From RP + Offset	44 + 079
Created Date	1/09/2020	To RP + Offset	46 + 028
Location	0.58 mi. E. of W. Jct of SR 39 to E. Jct of SR 39	SL Measure From	40.539
		SL Measure To	42.016

Roadway Information

District	LA PORTE	Pavement Area (Sq Yd)	24,262
Subdistricts	RENSSELAER	Underdrains Present	No
County(s)	75 - Starke	Curbs Present	No
Project Length	1.48	Known ADA Deficiencies	No
Number of Through Lanes	2	Est # of Lg Culverts (>48" -20')	2
Number of Lane Miles	2.95	Est # of Sm Culverts (12-48")	3
Functional Class	5 - Major Collector		
On NHS	0 - Not on the NHS		
AADT Year	2018		
AADT	4,887		
AADT Truck	656		
Pavement Type	HMA		

Project History

Project Type	Contract #	Year	Work Type
Functional	RS-30031	2010	HMA Overlay Minor Structural
Structural			

Maintenance History

Significance	Year	Maintenance Work Type
--------------	------	-----------------------



Pavement Scoping Application Report

5/21/2021

ID: 40198 for model run date 7/27/2020

Condition Data

	Year
% Functional Cracking	0
% Structural Cracking	0
Rut inches	0
IRI (in/mile)	0
Distress Box Area	0
Concrete Length	0
Asphalt Length	0
Concrete Faulting	0
Cost Effectiveness (\$/sqyd)	0
Roadway Category	

Purpose/Need Of Project

Purpose And Need The purpose of the project is to remove the surface distresses, increase the life of the pavement, and improve the IRI to good condition, less than 70.
The existing pavement section has minor rutting, potholing/stripping, bleeding and cracking.

Own It: Alternatives

Preliminary Alternatives That Are Contemplated (Analyzed) With Costs

The existing asphalt pavement be milled 2 inches in depth and paved with 2 inches of HMA, to be determined by the pavement design. The existing inlets and small culverts within the project limits that rate poorly should be replaced. The existing guardrail should be reset. Milled centerline and shoulder corrugations should be installed.

Consequences If No Action Is Taken (Do Nothing Alternative Is Selected)

The pavement will continue to deteriorate and will require a more extensive pavement treatment.

Secondary Considerations or Goals With Costs

None

Will Further Analysis/Assessment Be Required Beyond This Form? Yes

Additional Features To be Included

RPMs	No	Centerline Rumble Stripe	Yes
Curb Ramps	No	Edgeline Rumble Stripes	Yes



Pavement Scoping Application Report

5/21/2021

ID: 40198 for model run date 7/27/2020

Estimated Total Project Costs

Phase	Amount	Comments
Right of Way Purchase	\$0	
Right of Way Services	\$0	
Preliminary Engineering 1	\$90,000	
Railroad PE 1	\$50,000	
Utilities PE	\$15,000	
Utilities CN	\$0	
Construction Total \$684,000		
Construction	\$674,000	
RR Construction	\$250,000	
Maintenance of Traffic		included in CN
Environmental Mitigation	\$10,000	
ADA	\$0	
Sidewalks/ Multi Use Paths		
Construction Engineering	\$30,000	
Other Considerations	\$0	
Total	\$1,119,000	

Maintenance of Traffic

Can this road be closed due to traffic?	Yes	Interstate Congestion Policy Waiver Required	Yes
Traffic Management Plan Required?	No		
Anticipated MOT Scheme Value	Flagging		
2			

Other Considerations

Anticipated Number of Construction Seasons To Complete	1.00
Anticipated Number of Years To Complete Design	2.00
Environmental Document Type	PCE
Environmental Factors	

Additional Anticipated Complications

Tree Clearing	No
Fish	No
Bats	No
Historical	No
Potential Hazardous Coatings	No

Additional Comments

Supporting Documents

Document Type	Document Name	Date
HydraulicReports	PK 40198 - Small Culverts.xlsx	12/23/2020 4:30:10PM
Photos	PK 4198 Pictures.pdf	1/9/2020 1:34:36PM
CostEstimates	EngRpt QtyCalc CostEst PK40198.pdf	12/23/2020 4:30:00PM
MiniScopeProjectReport	40198_Post Deliberation Project Scoping Report.pdf	4/5/2021 9:06:46PM



Pavement Scoping Application Report

5/21/2021

ID: 40198 for model run date 7/27/2020

Report Prepared By and Approved By

Title

Submitted By Asset Engineer
Concur By Scoping Engineer
Approved By SAM

Signature

Mooney, Molly
South, Paul
Benczik, Steve

1/7/2021
1/11/2021
1/15/2021

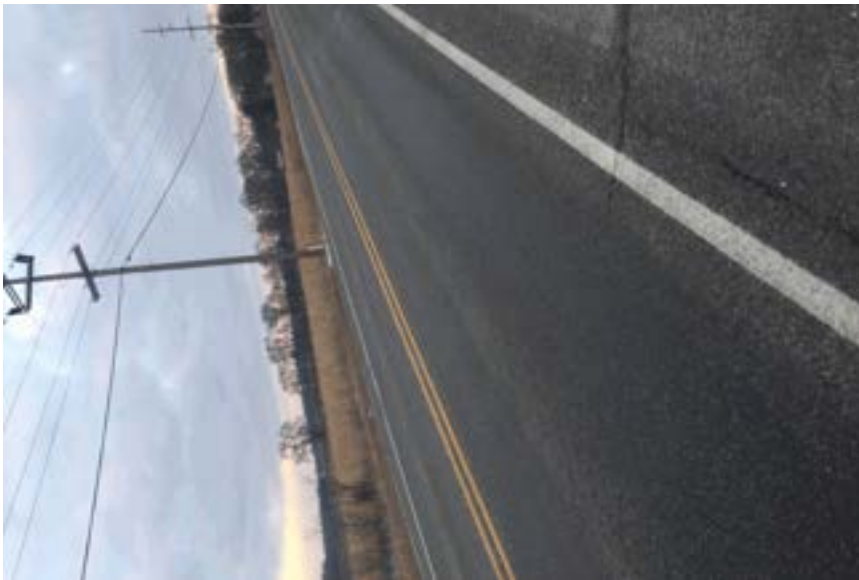
Submittal Type

Minor

Submittal Year

2026

Images



Screen Shots of the the January 24, 2024, Routine Bridge Inspection Report for
INDOT Structure Number 010-75-06886 A

888546 - LaPorte On-Call (210) | 010-75-06886 A-002960_2024 |

itams.sixense.co/itams/inspections/657e6719-156c-1224-635e-f5ab4125b098

ITAMS
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Quick Search: 010-75-06886 A
1 Asset(s) filtered

Inventory

- Assets
- Elements
- Documents
- Map
- Structures
- Tasks map
- Tasks
- Routing
- Inspections
- Critical Findings
- Audits
- Maintenance/History
- Photos
- Documents
- Scheduling
- External links
- Administration

002960 | 010-75-06886 A | 010-75-06886 A-002960_2024_Routine |

Task Info Jobs Routing Elements Critical Findings Maintenance/History Photos Documents Channel Profile Summaries

Number: 010-75-06886 A-002960_2024_Routine From: 01/24/2024 To: 01/24/2024 Photo

Asset: 010-75-06886 A - 002960


Type: Routine-NBI

Status: Approved

Team: LaPorte District Team leader: Ryan Arbour Assigned User: Amy Wines


Comments

1/27 Category asc Next



Expand all Collapse all

Search attribute


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Filters
1 Asset(s) filtered

Inventory

- Assets
- Elements
- Documents
- Map
- Structures
- Tasks map
- Tasks
- Routings
- Inspections**
- Critical Findings
- Audits
- Maintenance/History
- Photos
- Documents

002960 | 010-75-06886 A

010-75-06886 A-002960_2024_Routine

Task info

Jobs

Routings

Elements

Critical Findings

Maintenance/History

Photos

Documents

Channel Profile

Summaries

Inspection Summary

Paragraph

Routine Inspection:

This 3 span slab bridge is in overall good condition. This structure had an overlay in 2019 with superstructure patching. There is a minor spall near pier 3 in span B. There is map cracking in the wearing surface. There is a minor vertical crack near column 4 on the east side of pier 3 in the cap. Approach slabs have cracking throughout and East approach slab has center line spall.

Bridge history:

1987 New Bridge DES# 8008910, Contract B-16260

2020 Rehab A Thin Deck Overlay 1 DES# 1701065, Contract B-40315

ITAMS

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010-75-06886 A

×

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1 Asset(s) filtered

Inventory

Assets

Elements

Documents

Map

Structures

Tasks map

Tasks

Routings

Inspections

Critical Findings

Audits

Maintenance/History

Photos

Documents

Scheduling

External links

(93C) Critical Feature Inspection Date: Special Insp Date

3.2 National Bridge Inventory Condition Ratings

3.3 Scour Critical Bridge Appraisal

3.4 Hydraulic Scour Analysis Determination

3.5 Appraisal

(71) Waterway Adequacy

● 8. Bridge above approaches

Sufficiency Rating

86.1

(72) Approach Roadway Alignment

● 9. Equal to present desirable criteria

Status

● 0. Good

(84A) Bridge Ratings

● 1. Meets acceptable standards

(87) Structural Evaluation

● 7. Better than present minimum criteria

(84B) Transverse

● 1. Meets acceptable standards

(86) Deck Geometry

● 7. Better than present minimum criteria

(84C) Approach Rating

● 1. Meets acceptable standards

(88) Underclearances, Vertical & Horizontal

● 14. Not applicable

(84D) Approach Guardrail Ends

● 1. Meets acceptable standards

3.6 Inspection Summaries

Des No 2100231

Appendix I

I-8

888546 - LaPorte On-Call (210) SR10 C32.doc 010-75-06886 A-002960_2024

itams.sixense.co/taits/inspections/657e6719-156c-1224-635e-f5ab41250998

ITAMS powered by BeyondAsset

Quick Search: 010-75-06886 A 1 Asset(s) filtered

Inventory

- Assets
- Elements
- Documents
- Map
- Structures
- Tasks map
- Tasks
- Routes
- Inspections
- Critical Findings
- Audits
- Maintenance/History
- Photos
- Documents
- Scheduling
- External links
- Administration

Expand all Collapse all

Search attribute

1. Inspections-Routine

2. National Bridge Inventory Condition Ratings-Routine

3. INDOT Defined Condition Ratings-Routine

Are Barn Swallows or Nests Present? No

Are Bats or Guano Present? They Can Be Either Seen or Heard Under Structure. No

Concrete SlopeWall

No. No concrete slopeWall

Concrete SlopeWall Comments

Rip rap in place of slope wall

4. Appraisal-Routine

5. Scour Critical Bridge Appraisal-Routine

Delinquent

Culvert Summary Table

CULVERTS	CULVERT CLASS	PIPE HEIGHT	PIPE WIDTH	END SECTION RIPRAP	FLOWLINE RIPRAP	INSPECTION COMMENTS	PROPOSED WORK
CLV-010-075-44.94	Corrugated Metal Pipe	30.0 inches	30.0 inches	Yes	No	General: Pipe is starting to rust but is good Embankment Out: 7 Good End Section Out: 5 Fair Flowline Out: 7 Good Barrel Condition: 5 Fair	No work proposed
CLV-010-075-45.65	Corrugated Metal Pipe	15.0 inches	15.0 inches	No	No	General: Pipe and flow line full of debris, cannot inspect Embankment Out: 7 Good End Section Out: 0 Unknown Flowline Out: 3 Poor Barrel Condition: 0 Unknown	Culvert will be replaced with a larger corrugated metal pipe culvert 63-feet-long and 24-inches wide. Riprap will be placed at the north culvert outfall for scour protection.
CLV-010-075-45.91	Corrugated Metal Pipe	15.0 inches	15.0 inches	No	No	General: Some debris in flow line and end section of pipe, flow line not very goodwater drains slowly from pipe Embankment Out: 7 Good End Section Out: 5 Fair Flowline Out: 5 Fair Barrel Condition: 5 Fair	No work proposed
CV-010-075-45.50	Corrugated Metal Pipe	5.0 feet	5.0 feet	No	No	General: Pipe is in good condition, starting to rust at the waterline and below Culvert Rating: 7 Good	No work proposed

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800077	1800077	Starke	Bass Lake State Park/Beach
1800118	1800118C	Starke	Bass Lake
1800171	1800171OO	Starke	Bass Lake
1800327	1800327B	Starke	Bass Lake
1800343	1800343	Starke	Hamlet Town Park, Earl Leinbach Park
1800363	1800363A	Starke	Bass Lake
1800434	1800434B	Starke	Bass Lake State Beach
1800623	1800623	Starke	Sandy Acres Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.