PROJECT	DESIGNATION
0200633	0200633
CONTRACT	BRIDGE FILE
B-33539	041-82-0877E

SOUTHBOUND STRUCTURE				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
041-82-0877E	PRESTRESSED CONCRETE I-BEAM BRIDGE	1@42'-9", 1@43'-0", 2@42'-5", 1@43'-0", 2@42'-5", 1@43'-0" AND 1@42'-9" NO SKEW	OHIO RIVER OVERFLOW	℄ STRUCTURE STA. 160+18.10

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
0100482	U.S. 41 over SB Cheatam Slough
9620260	U.S. 41 over NB Cheatam Slough
0200633	U.S. 41 over SB Ohio River Overflow
0200636	U.S. 41 over NB Ohio River Overflow
0200635	U.S. 41 over SB Eagle Creek
0200634	U.S. 41 over NB Eagle Creek
1298275	U.S. 41 over SB Ohio River
1592481	Roadway Plans from Cheatam Slough to Eagle Creek

NOTE: SEE ROAD PLANS FOR REMOVAL OF EXISTING GUARDRAIL, PROPOSED GUARDRAIL, PAVEMENT MARKINGS, EROSION CONTROL MEASURES AND MAINTENANCE OF TRAFFIC DETAILS.

INDIANA DEPARTMENT OF TRANSPORTATION

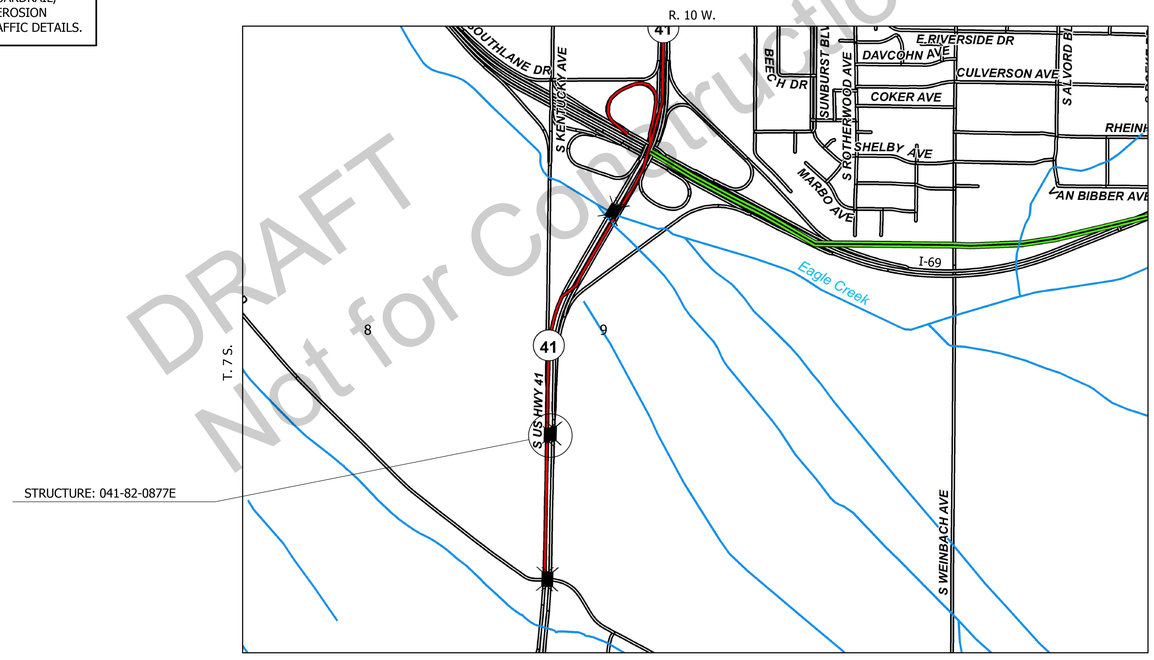
BRIDGE REHABILITATION PLANS

FOR SPANS OVER 20 FEET

U.S. 41 SB OVER OHIO RIVER OVERFLOW

PROJECT NO. 0200633

DECK RECONSTRUCTION ON STRUCTURE: 041-82-0877E, U.S 41 SB OVER OHIO RIVER OVERFLOW LOCATED APPROXIMATELY 0.82 MILES SOUTH OF THE U.S.41 AND I-69 INTERCHANGE, IN SECTIONS 8 AND 9, TOWNSHIP 7 SOUTH, RANGE 10 WEST, VANDERBURGH COUNTY, INDIANA.



SCALE: 1"=1500'

VICINITY MAP
VANDERBURGH COUNTY

TRAFFIC DATA	
A.A.D.T. (2013)	19989 V.P.D.
A.A.D.T. (2017)	21260 V.P.D.
A.A.D.T. (2037)	26720 V.P.D.
DIRECTIONAL DISTRIBUTION	100 %
TRUCKS	9 % A.A.D.T.

DESIGN DATA	
DESIGN SPEED	50 M.P.H.
POSTED SPEED	50 M.P.H.
PROJECT DESIGN CRITERIA	3R NON-FREEWAY
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE

PROJECT LOCATION SHOWN BY
VANDERBURGH COUNTY
VINCENNES DISTRICT

LATITUDE: 37°55'47" N. & LONGITUDE: 87°32'54" W.

H.U.C. = 05140202010020

R.P. 0+43

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES.

INDIANA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED 2016 TO BE USED WITH THESE PLANS.

PLANS PREPARED BY: Butler Fairman and Seufert Inc. (317)713-4615 PHONE

CERTIFIED BY: 10/31/16 DATE

APPROVED FOR LETTING: INDIANA DEPARTMENT OF TRANSPORTATION DATE

MICHAEL MALEY REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

BRIDGE FILE
041-82-0877E
DESIGNATION
0200633

SURVEY BOOK	SHEET
	1 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605

UTILITIES

COMMUNICATIONS:

AT&T
134 NW Sixth Street
Evansville, Indiana 47708
ATTN: Marc Clark
P. (812) 464-6050
E. mc3429@att.com

AT&T
134 NW Sixth Street
Evansville, Indiana 47708
ATTN: Andy Folz
P. (812) 464-6055
E. af1896@att.com

WINDSTREAM COMMUNICATIONS
5020 Smythe Drive
Evansville, Indiana 47715
ATTN: Daniel Leskinen
E. Daniel.leskinen@windstream.com
P. (812) 759-2833
P. (812) 455-9558 (CELL)

TIME WARNER CABLE
1900 N. Fares Avenue
Evansville, Indiana 47711
ATTN: Daryl Hulsey
E. daryl.hulsey@twcable.com
P. (812) 253-2755
P. (812) 305-8348 (CELL)

ELECTRIC:

KENERGY CORPORATION
ATTN: Kyle Hart
P. (270) 831-4602
E. khart@kenergycorp.com

VECTREN
Jody Chapman
1 North Main Street
Evansville, Indiana 47711
E. jwchapman@vectren.com

INDOT:

SIGNALS & LIGHTING
ATTN: Robert Horton
E. rhorton@indot.in.gov
P. (812) 699-0643
P. (812) 698-4743 (CELL)

ITS TECHNOLOGY
ATTN: Konstantin Veygman
E. kveygman@indot.in.gov
P. (317) 899-8606

WEIGHT STATION:

JACK MANN SCALES, INC.
2073 Mercer Road
Lexington, Kentucky 40511
ATTN: Larry Stagner
E. larry@jackmannscales.com
P. (859) 233-0322

KYTC
ATTN: David Cornett
E. davidp.cornett@ky.gov
P. (502) 564-4556

REVISIONS

SHEET NO.	DATE	REVISED

INDEX

NOTE: SEE ROAD PLANS FOR REMOVAL OF EXISTING GUARDRAIL, PROPOSED GUARDRAIL, PAVEMENT MARKINGS, EROSION CONTROL MEASURES AND MAINTENANCE OF TRAFFIC DETAILS.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: _____ 10/31/16
DESIGN ENGINEER DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

INDEX SHEET

HORIZONTAL SCALE	BRIDGE FILE
NONE	041-82-0877E
VERTICAL SCALE	DESIGNATION
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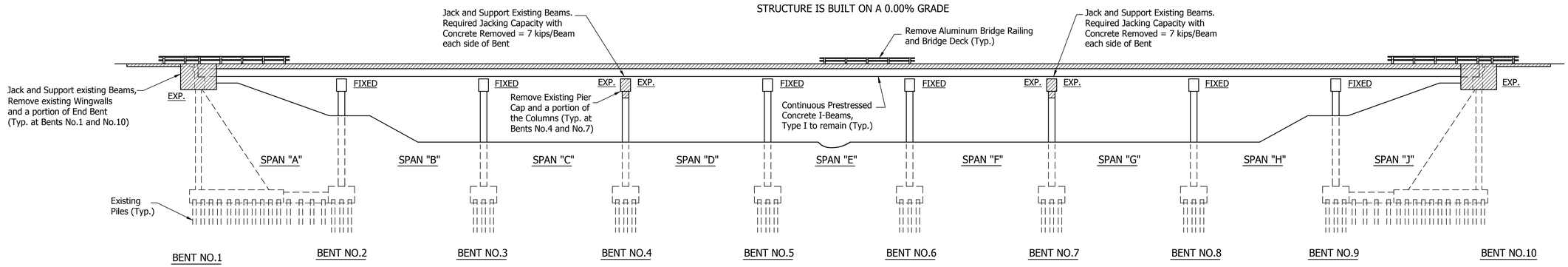
B-33539	0200633

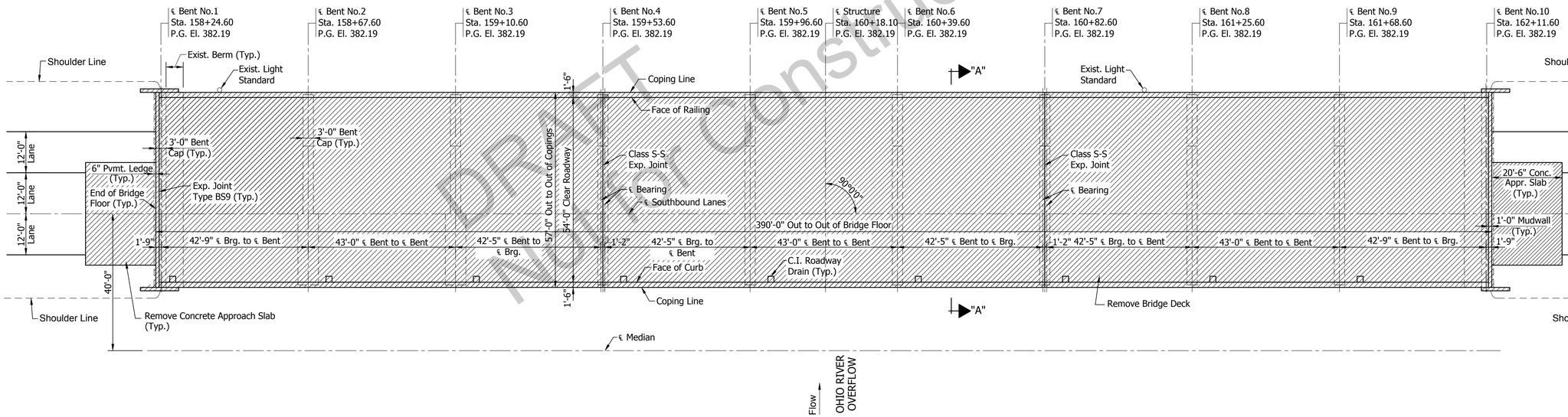
5605 SNS



ELEVATION
SOUTHBOUND STRUCTURE

Scale: 1/16"=1'-0"

Note: Hatched Areas indicate Portions to be Removed.



PLAN
SOUTHBOUND STRUCTURE

Scale: 1/16"=1'-0"

NOTE: SEE ROAD PLANS FOR REMOVAL OF EXISTING GUARDRAIL, PROPOSED GUARDRAIL, PAVEMENT MARKINGS, EROSION CONTROL MEASURES AND MAINTENANCE OF TRAFFIC DETAILS.

NOTES
See Sheet 4 for Proposed Structure General Plan.
See Sheet 5 for Section "A-A".
See Sheet 6 for General Notes and Design Data.

CONTINUOUS PRESTRESSED CONCRETE I-BEAM BRIDGE

9 SPANS: 1 AT 42'-9", 1 AT 43'-0", 2 AT 42'-5", 1 AT 43'-0", 2 AT 42'-5", 1 AT 43'-0" AND 1 AT 42'-9", NO SKEW, 54'-0" CLEAR ROADWAY ON U.S.41 SB OVER OHIO RIVER OVERFLOW

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

GENERAL PLAN EXISTING STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
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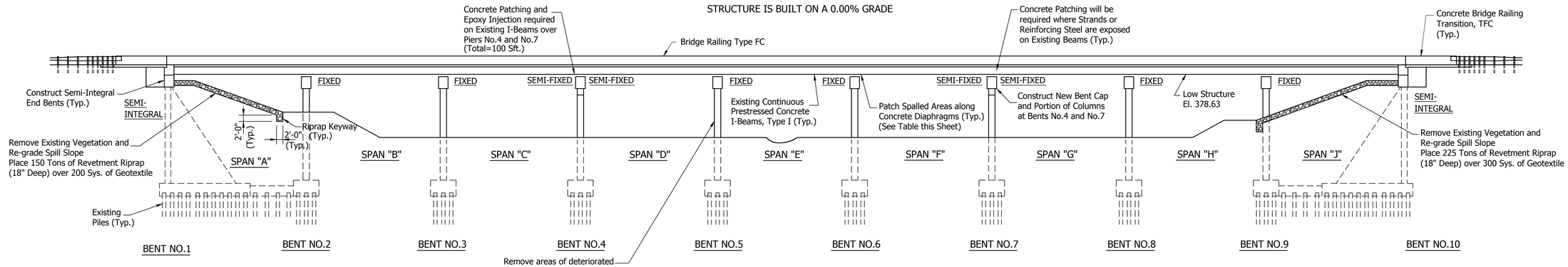
BFS NO. 5605



ELEVATION
SOUTHBOUND STRUCTURE

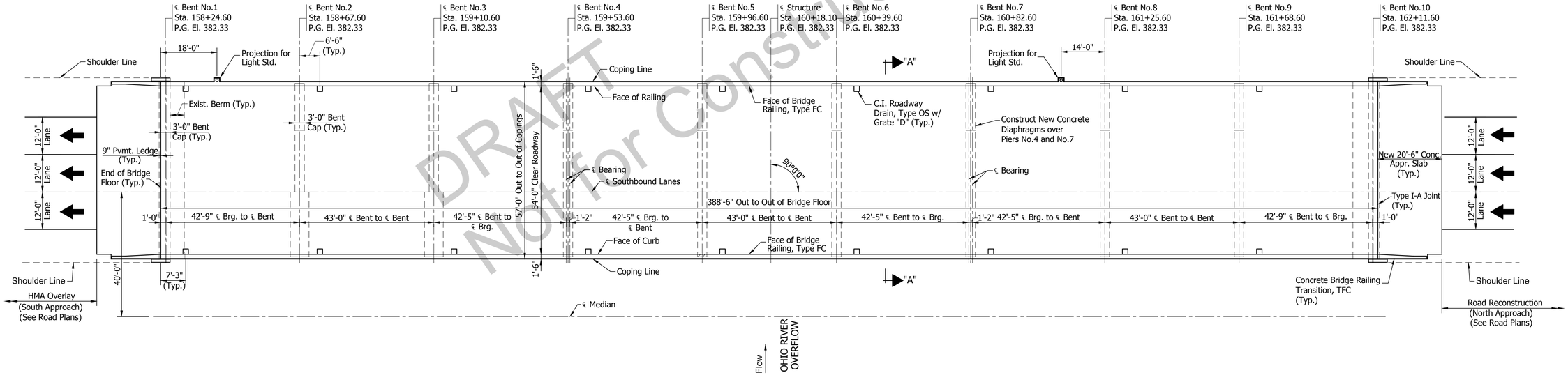
Scale: 1/16"=1'-0"

PATCHING CONCRETE STRUCTURE (SB)		
LOCATION	DIAPHRAGM QUANTITY (SFT.)	PIER QUANTITY (SFT.)
BENT NO.2	10	5
BENT NO.3	30	85
BENT NO.4	*	50
BENT NO.5	20	50
BENT NO.6	5	40
BENT NO.7	*	75
BENT NO.8	55	60
BENT NO.9	30	35

* Existing Bent Cap and Diaphragm to be Removed.

GALVANIC ANODES FOR CONCRETE PATCHING	
LOCATION	QUANTITY
DIAPHRAGMS	110 EACH
PIERS	295 EACH

Note: Install 7 Snowplowable Raised Pavement Markers (Field Verify)



NOTE: SEE ROAD PLANS FOR REMOVAL OF EXISTING GUARDRAIL, PROPOSED GUARDRAIL, PAVEMENT MARKINGS, EROSION CONTROL MEASURES AND MAINTENANCE OF TRAFFIC DETAILS.

PLAN
SOUTHBOUND STRUCTURE

Scale: 1/16"=1'-0"

NOTES

See Sheet 3 for Existing Structure General Plan.
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CONTINUOUS PRESTRESSED CONCRETE I-BEAM BRIDGE

9 SPANS: 1 AT 42'-9", 1 AT 43'-0", 2 AT 42'-5", 1 AT 43'-0", 2 AT 42'-5", 1 AT 43'-0" AND 1 AT 42'-9", NO SKEW, 54'-0" CLEAR ROADWAY ON U.S.41 SB OVER OHIO RIVER OVERFLOW

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
PROPOSED STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
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BFS NO. 5605

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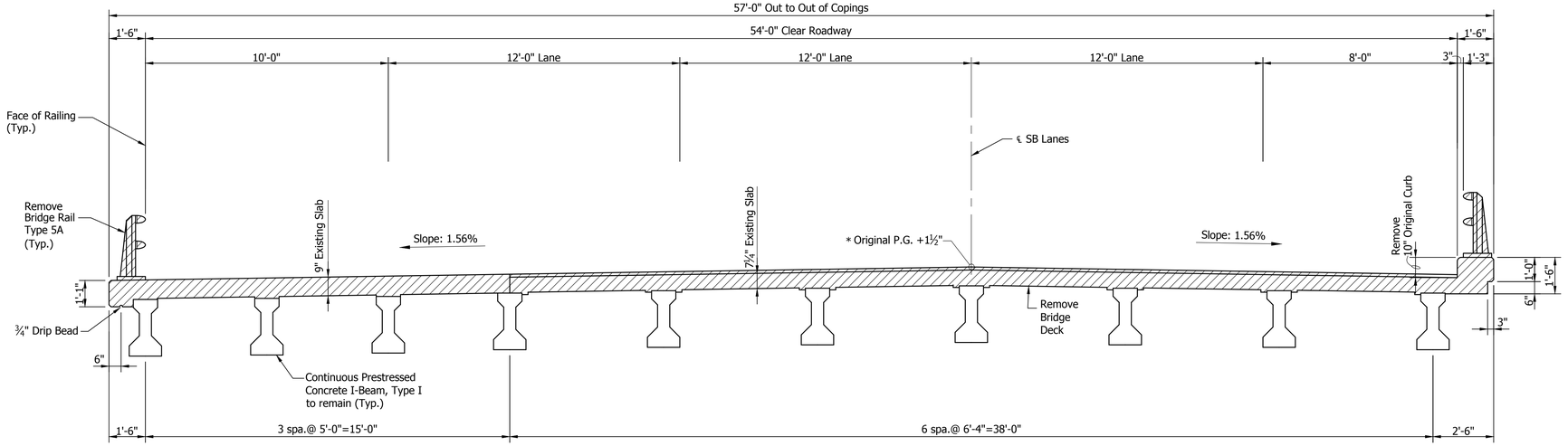
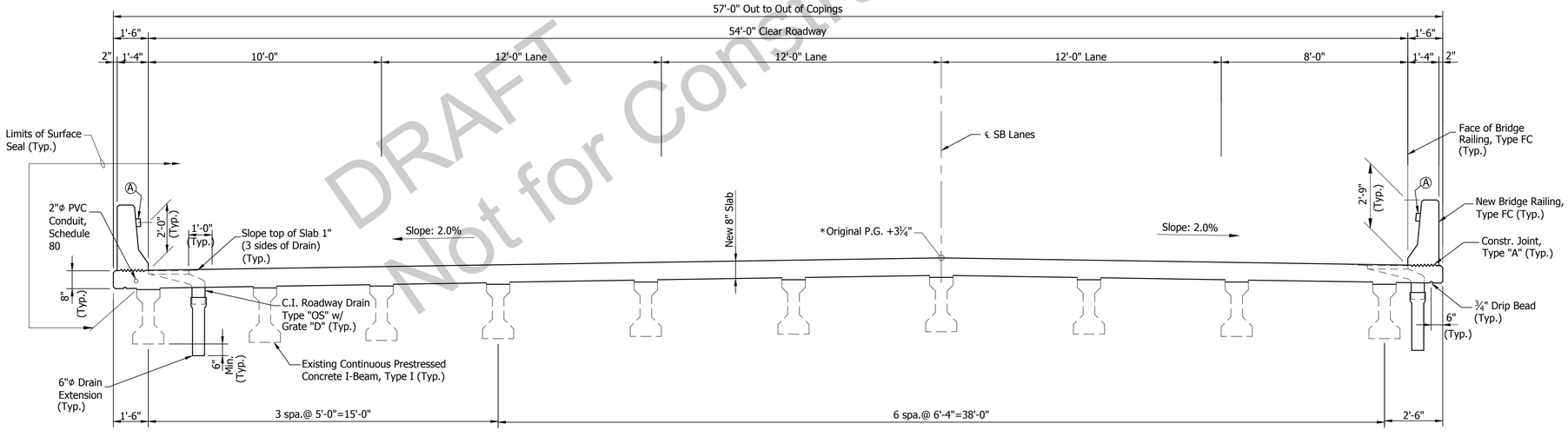


EXISTING SECTION "A-A"
SOUTHBOUND STRUCTURE
Scale: ⅜"=1'-0"

Note: Hatched Areas indicate Portions to be Removed.

Ⓐ Barrier Delineators @ 40'-0" Max. Spacing

* Original Plans dated December 13, 1968



PROPOSED SECTION "A-A"
SOUTHBOUND STRUCTURE
Scale: ⅜"=1'-0"

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: _____ 10/31/16
DESIGN ENGINEER — DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ
CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
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GENERAL NOTES

Plans for the existing structure are on file with the Indiana Department of Transportation as Structure No. 41-A-877 and Bridge Files: 41-A-877A, 41-82-877B, 41-82-877C and 41-82-877D and are available upon request.

Where new work is to be fitted to old work, the Contractor shall check all dimensions and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new part to the old.

Epoxy coated reinforcing bars shall be required in various portions of the structure unless otherwise shown.

Reinforcing bars covering shall be 2½" in top of approach slabs.

Reinforcing bars covering shall be 2-1/2" in top and 1" in bottom of floor slabs and 2" in all other areas unless noted.

Reinforcing bars shall be A.S.T.M A615, Grade 60.

Concrete shall be Class C in end bents, wingwalls, floor slab and barrier railings.

Concrete shall be Class A in all portions of the project not noted above.

Chamfer exposed corners of concrete 1" unless noted.

Surface seal shall be required on various areas of the structure as shown. Estimated quantity = 30700 Sft. (Does not include Concrete Barrier Railing Transitions).

Excavation required for placement of Aggregate for End Bent Backfill at the bridge end bents beyond the limits of Foundation Excavation Unclassified shall not be paid for directly but shall be included in the cost of the Aggregate for End Bent Backfill.

DESIGN DATA

MATERIAL DESIGN STRENGTHS:

Class "C" Concrete	F'c = 4,000 p.s.i.
Class "A" Concrete	F'c = 3,500 p.s.i.
Reinforcing Bars (Grade 60)	Fy = 60,000 p.s.i.

LIVE LOAD:

HS20-44 loading with distribution in accordance with 2002 A.A.S.H.T.O. Specifications. Load Factor = 2.17

DEAD LOAD:

Actual plus 35 pounds per square foot (composite) for future wearing surface and 15 pounds per square foot (non composite) for deck forms. Slab design with a 1/2" wearing surface and a structural depth of 7-1/2".

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6 inches outside the vertical coping form. The top overhang brackets were assumed to be located 6 inches past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

DECK FALSEWORK LOADS: Designed for 15 psf for deck forms and 2 ft. exterior walkway.

CONSTRUCTION LIVE LOAD: Designed for 20 psf extending 2 ft past the edge of coping and 75 plf vertical force applied at a distance of 6 inches outside the face of coping over a 30 ft length of the deck centered with the finishing machine.

FINISHING MACHINE LOAD: 4500 lbs. distributed over 10 feet along the coping.

WIND LOAD: Designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DATA

AASHTO LRFD Bridge Design Specifications, 6th Edition, 2012.
Seismic Zone 2
S_{D1} = 0.257
Site Class D

NOTE: SEE ROAD PLANS FOR REMOVAL OF EXISTING GUARDRAIL, PROPOSED GUARDRAIL, PAVEMENT MARKINGS, EROSION CONTROL MEASURES AND MAINTENANCE OF TRAFFIC DETAILS.

RECOMMENDED FOR APPROVAL: Michael Matel 10/31/16
DESIGN ENGINEER DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ
CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
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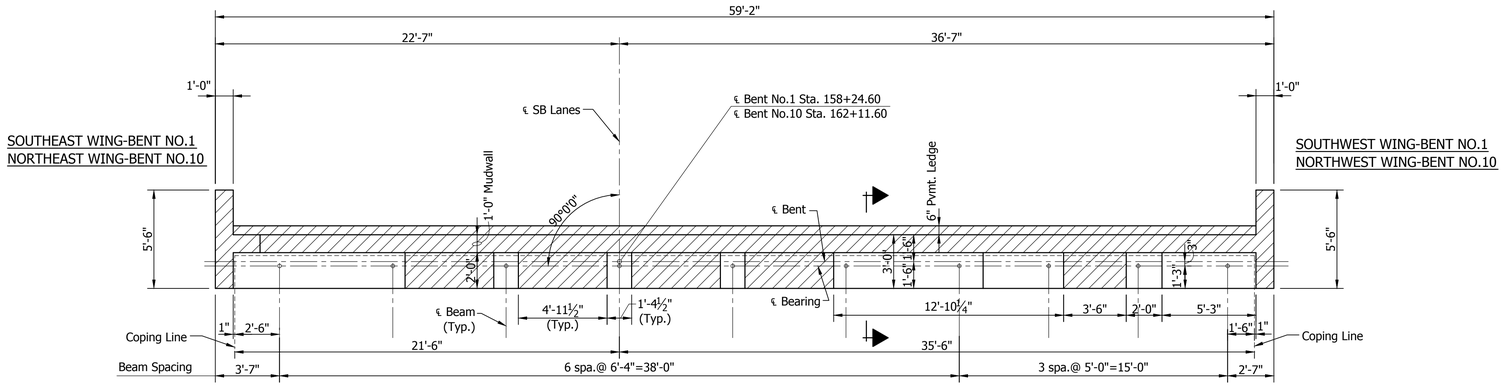
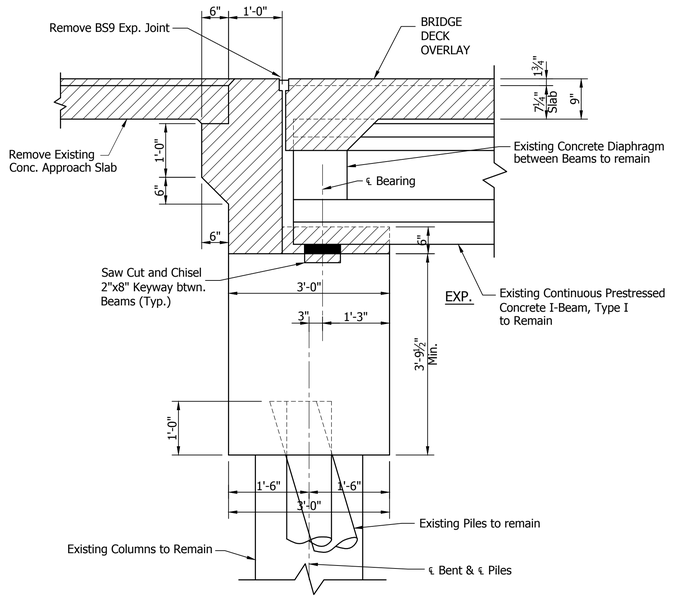


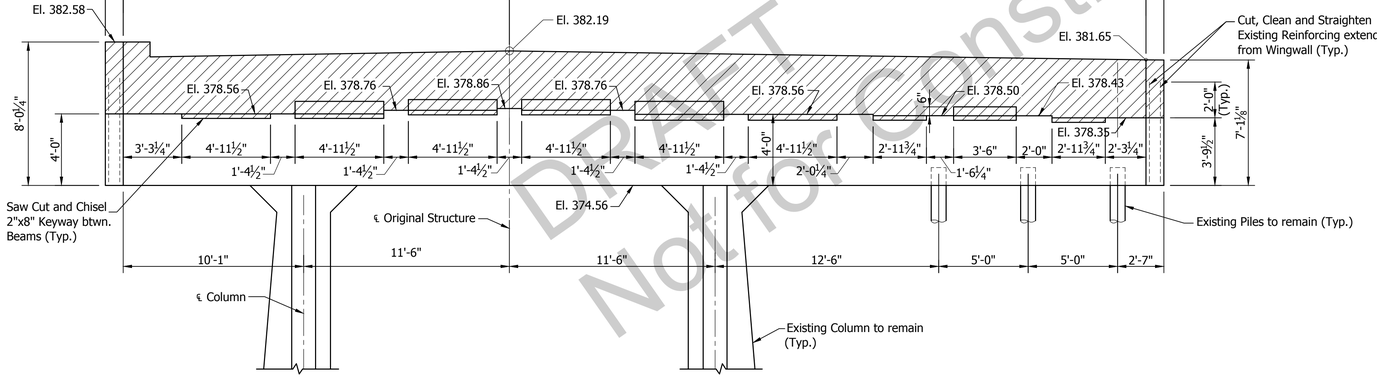
PLAN

Note: Hatched Areas indicate portions to be Removed.



SECTION AT BENTS NO.1 OR NO.10
(SHOWING REMOVALS)

Scale: ¾"=1'-0"



ELEVATION

BENT NO.1 (SHOWN)
BENT NO.10 (OPP. HAND)

Scale: ¼"=1'-0"

NOTE
See Sheets 8 and 9 for Reconstruction Details.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

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DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

BENTS NO.1 OR NO.10 DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
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AS NOTED	0200633
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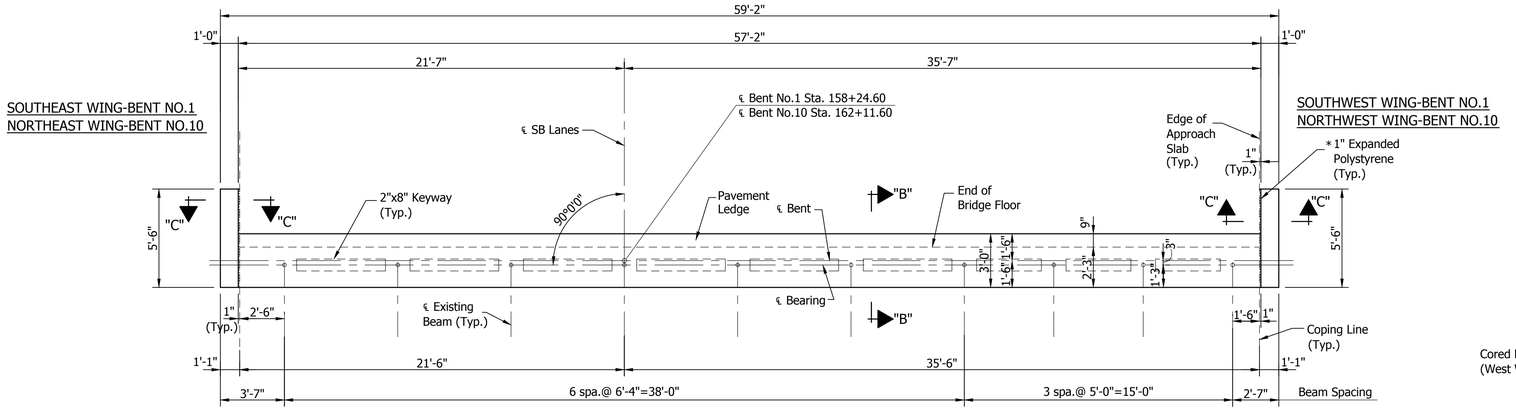
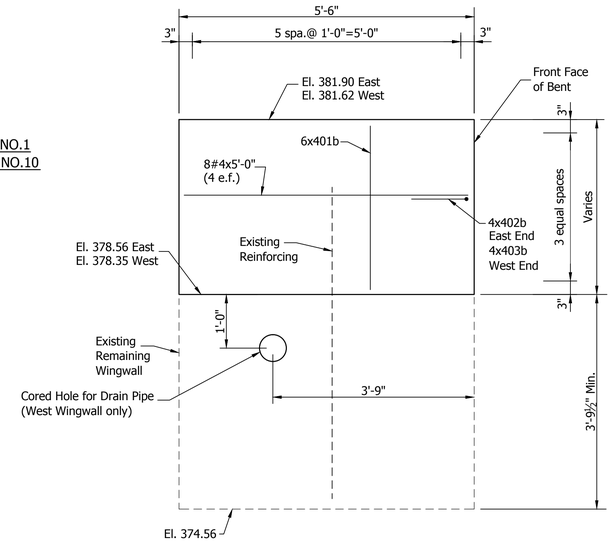
BFS NO. 5605



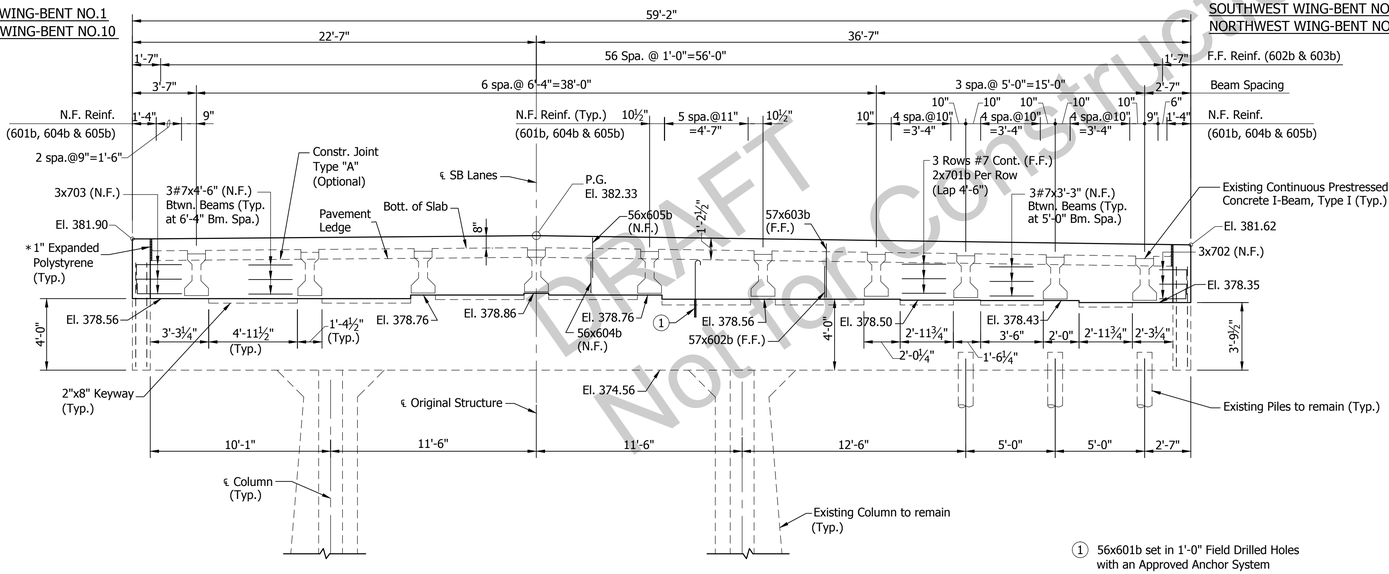
PLAN

* See Special Provisions



TYPICAL WINGWALL ELEVATION

Scale: ¾"=1'-0"

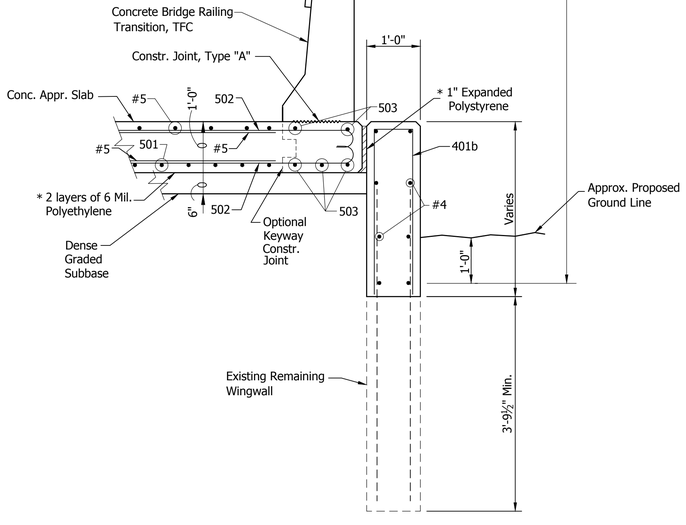


1 56x601b set in 1'-0" Field Drilled Holes with an Approved Anchor System (Min. Pullout = 26500 Lbs.)

ELEVATION

BENT NO.1 (SHOWN)
BENT NO.10 (OPP. HAND)

Scale: ¼"=1'-0"



SECTION "C-C"

Scale: ¾"=1'-0"

NOTES

See Sheet 7 for Removal Details.
See Sheet 9 for Section "B-B".
See Sheet 10 for Bar Bending Details and Bill of Materials.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

BENTS NO.1 OR NO.10 DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
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AS NOTED	0200633

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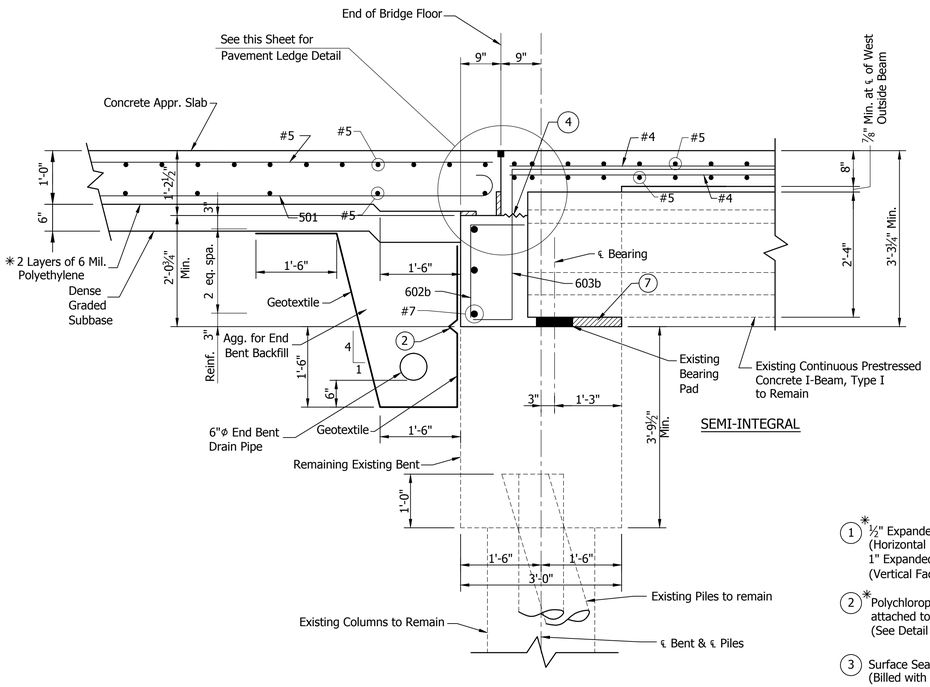
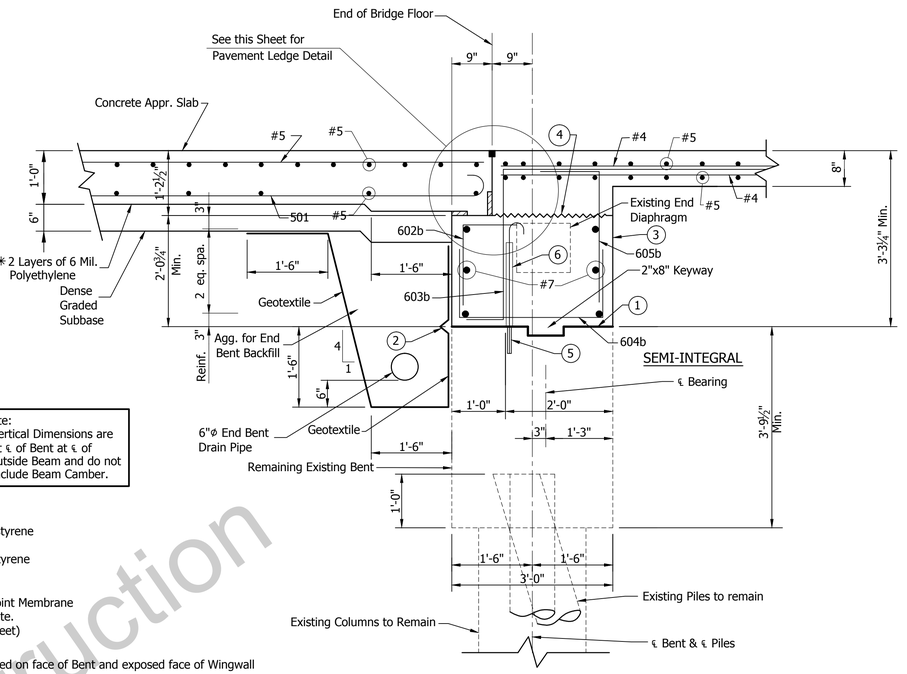


SECTION "B-B"
AT BEAMS
Scale: ¾"=1'-0"

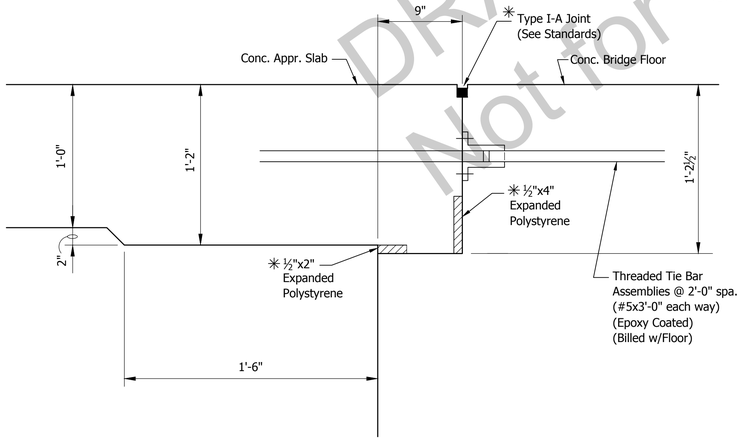


SECTION "B-B"
BETWEEN BEAMS
Scale: ¾"=1'-0"

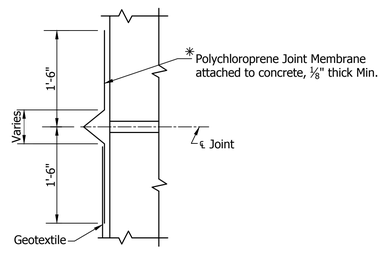
Note:
Vertical Dimensions are at ℄ of Bent at ℄ of outside Beam and do not include Beam Camber.

1. ✳½" Expanded Polystyrene (Horizontal Face) 1" Expanded Polystyrene (Vertical Face)
2. ✳Polychloroprene Joint Membrane attached to concrete. (See Detail this Sheet)
3. Surface Seal required on face of Bent and exposed face of Wingwall (Billed with Floor)
4. Optional Constr. Joint, Type "A"
5. 601b set in 1'-0" Field Drilled Holes with an Approved Anchor System (Min. Pullout = 26500 Lbs.)
6. ✳PVC Pipe Sleeve, 4" Dia. Schedule 40 Top of Sleeve to be Sealed before Concrete is Poured.
7. ✳Expanded Polystyrene cut to clear Bearing Pad by ½".

✳See Special Provisions



PAVEMENT LEDGE DETAIL
Not to Scale



JOINT MEMBRANE DETAIL
Not to Scale

NOTES
See Sheet 7 for Removal Details.
See Sheet 10 for Bar Bending Details and Bill of Materials.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ
CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

BENTS NO.1 OR NO.10 DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
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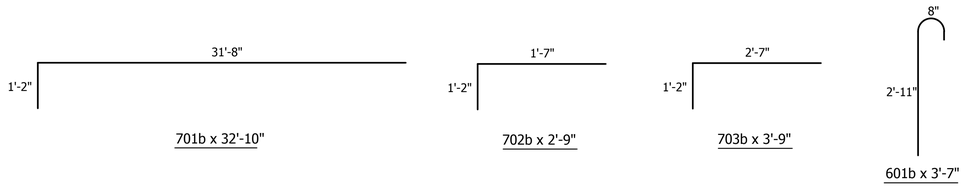
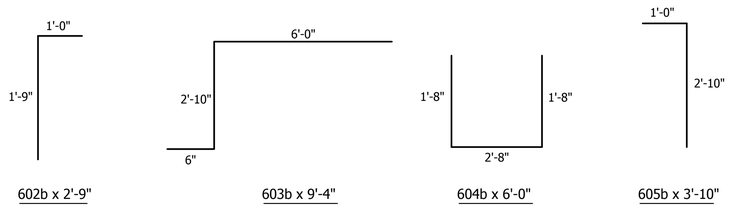
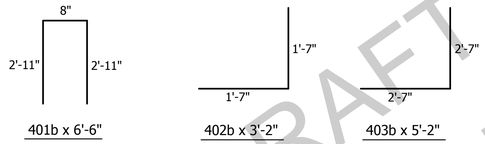
BFS NO. 5605







BAR BENDING DETAILS

Not to Scale

BILL OF MATERIALS
BENT NO.1
(BENT NO.10 SAME)
SOUTHBOUND STRUCTURE

REINFORCING BARS

Mark or Size	No. of Bars	Length (Ft.)	Weight (Lbs.)
701b	6	32'-10"	
702b	3	2'-9"	
703b	3	3'-9"	
#7	18	4'-6"	
#7	9	3'-3"	
Total #7 (Epoxy Coated)			668
601b	56	3'-7"	
602b	57	2'-9"	
603b	57	9'-4"	
604b	56	6'-0"	
605b	56	3'-10"	
Total #6 (Epoxy Coated)			2163
401b	12	6'-6"	
402b	4	3'-2"	
403b	4	5'-2"	
#4	16	5'-0"	
Total #4 (Epoxy Coated)			128
Total Steel (Epoxy Coated)			2959

MISCELLANEOUS

Field Drilled Holes in Concrete	56 Each
Cored Hole in Concrete	1 Each
Geotextile	55 Sys.
Aggregate for End Bent Backfill	15 Cys.
6"ø End Bent Drain Pipe (Includes 90° Elbow)	72 Lft.

⊕ A.S.T.M. A615, Grade 60

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

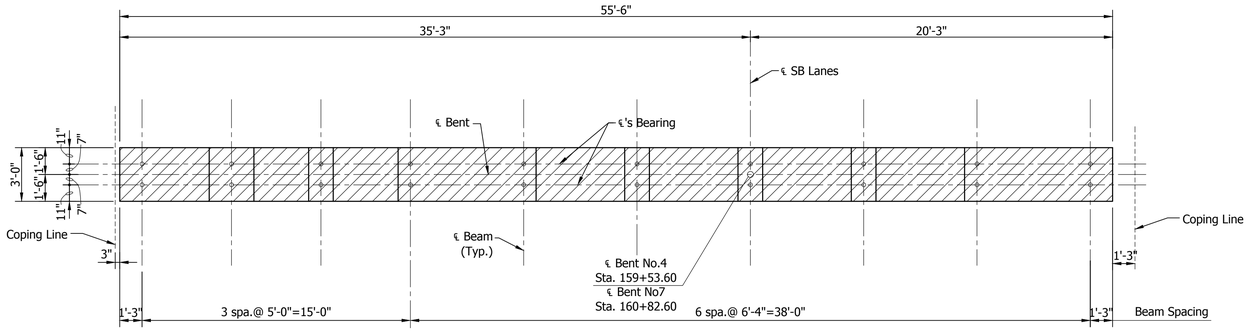
CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

BENTS NO.1 OR NO.10 DETAILS
SOUTHBOUND STRUCTURE

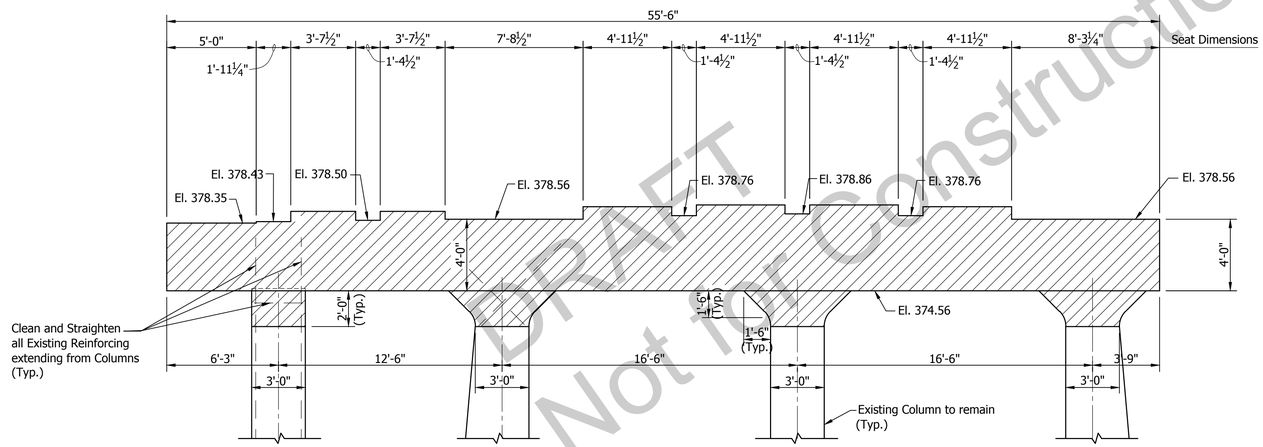
HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	10 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605



CAP PLAN

Note: Hatched Areas indicate portions to be Removed.



ELEVATION

BENT NO.4 OR NO.7

Scale: ¼"=1'-0"

NOTE

See Sheet 12 for Reconstruction Details.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

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DESIGNED: C. OBRIEN DRAWN: D. SHEETZ

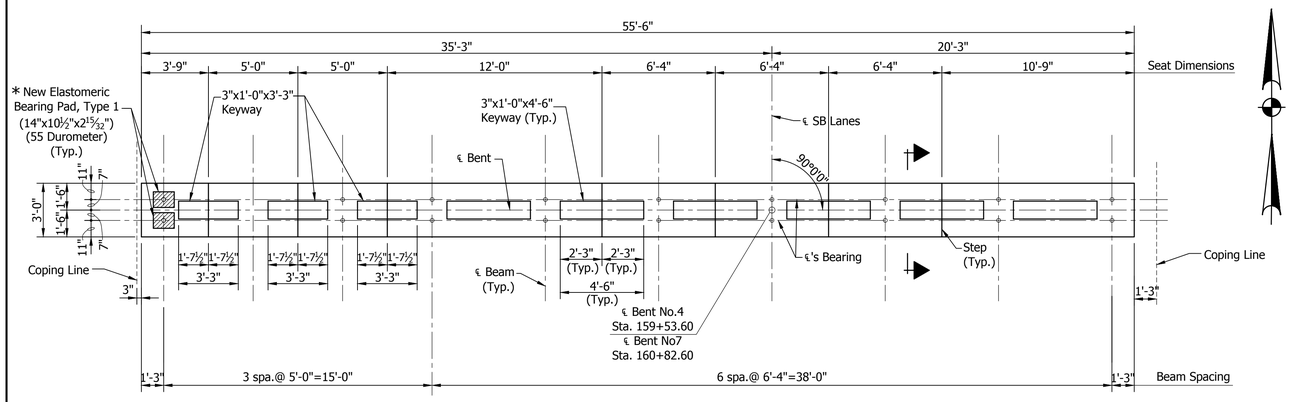
CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

BENTS NO.4 OR NO.7 DETAILS
SOUTHBOUND STRUCTURE

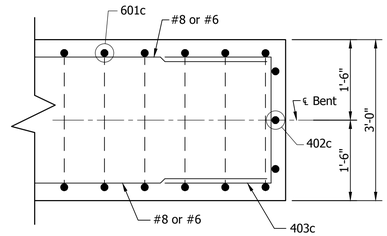
HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	11 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605

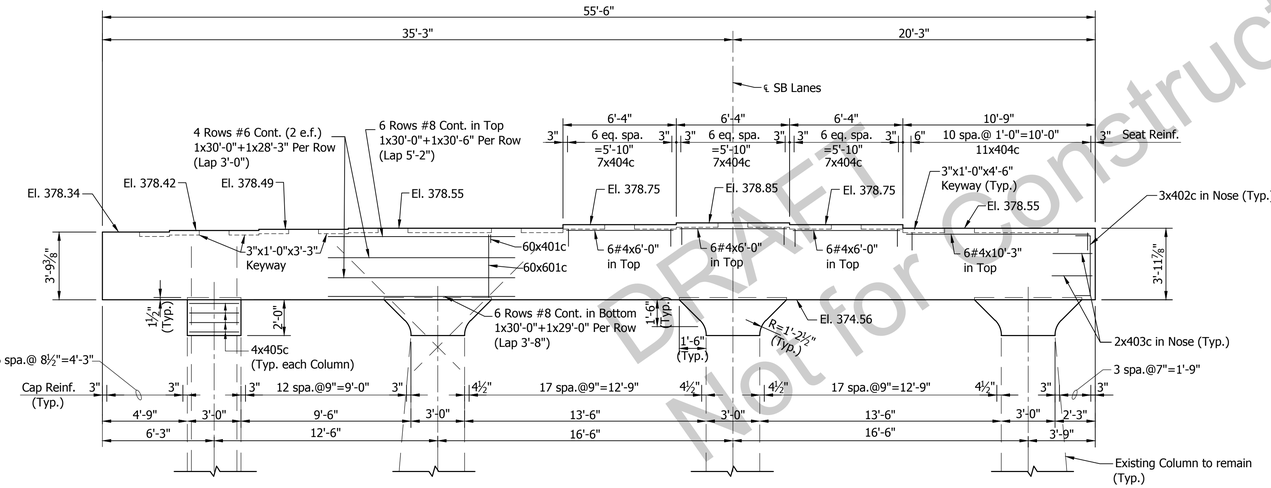


CAP PLAN

* See Special Provisions

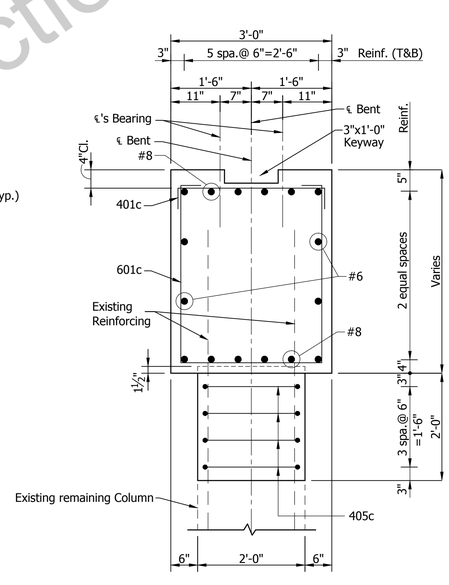


PLAN-SECTION
CAP NOSE
Scale: ¾"=1'-0"

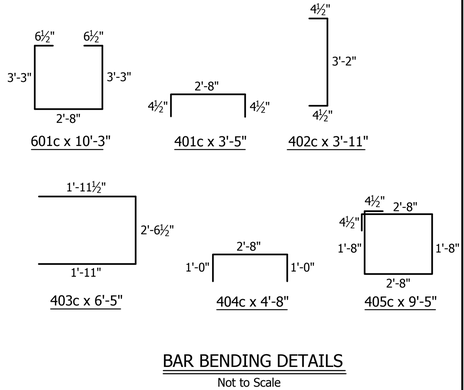


ELEVATION

BENT NO.4 OR NO.7
Scale: ¼"=1'-0"



SECTION AT BENT NO.4 OR NO.7
(SHOWING RECONSTRUCTION)
Scale: ¾"=1'-0"



BAR BENDING DETAILS
Not to Scale

BILL OF MATERIALS
BENT NO.4
(BENT NO.7 SAME)
SOUTHBOUND STRUCTURE

REINFORCING BARS

Mark or Size	No. of Bars	Length (Ft.)	Weight (Lbs.)
#8	6	30'-6"	
#8	12	30'-0"	
#8	6	29'-0"	
Total #8 (Plain)			1914
601c	60	10'-3"	
#6	4	30'-0"	
#6	4	28'-3"	
Total #6 (Plain)			1274
401c	60	3'-5"	
402c	6	3'-11"	
403c	4	6'-5"	
404c	32	4'-8"	
405c	16	9'-5"	
#4	6	10'-3"	
#4	18	6'-0"	
Total #4 (Plain)			483
Total Steel (Plain)			3671

CONCRETE

Class "A" in Substructure	27.2 Cys.

⊕ A.S.T.M. A615, Grade 60

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER 10/31/16 DATE

DESIGNED: C. OBRIEN DRAWN: D. SHEETZ

CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

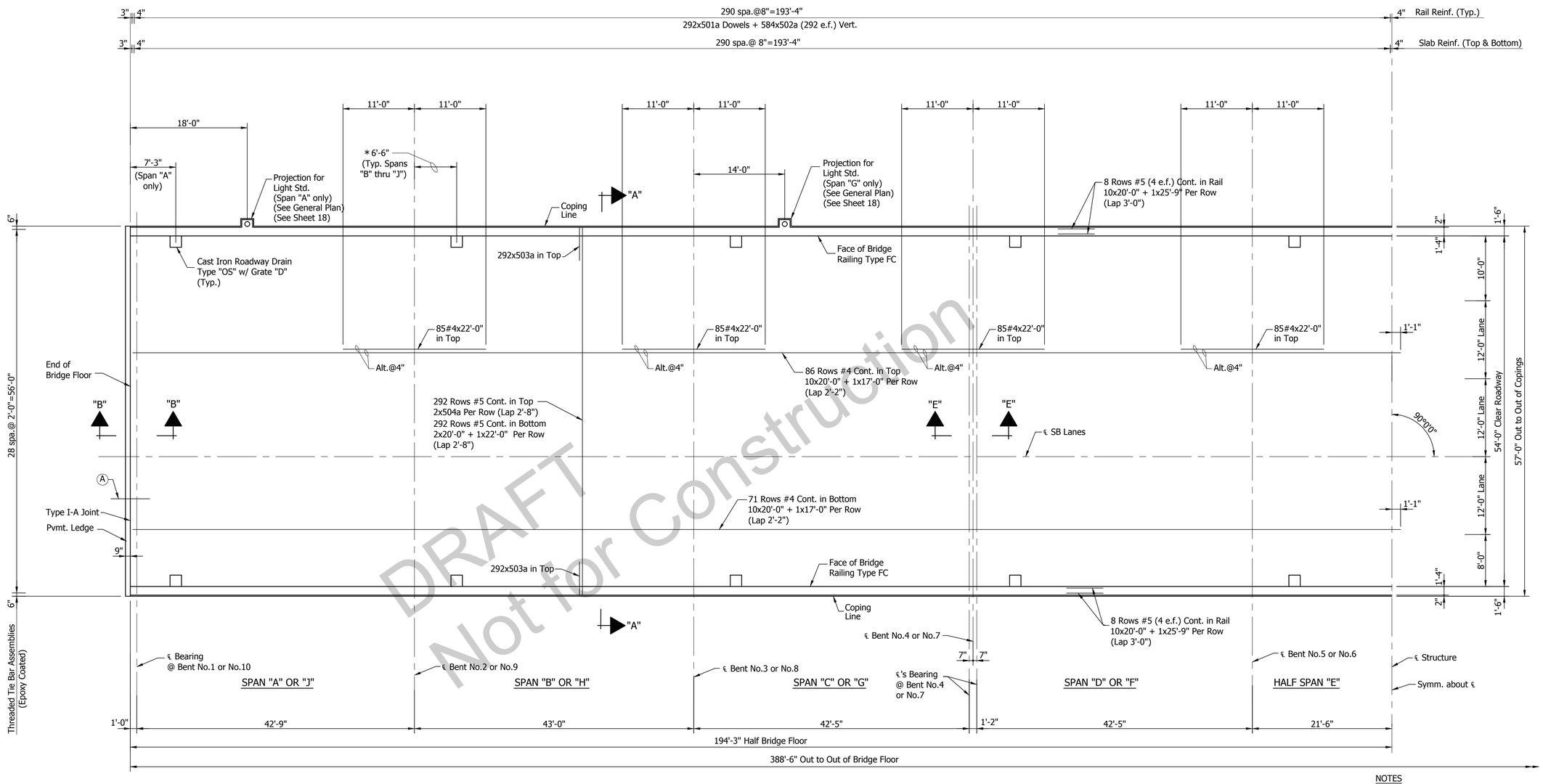
BENTS NO.4 OR NO.7 DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633

SURVEY BOOK	SHEET
	12 OF 20
CONTRACT	PROJECT
B-33539	0200633

5605

BFS NO.



Ⓐ 29-Threaded Tie Bar Assemblies (#5x3'-0" each way) (Epoxy Coated)

∗ Drain Locations for Spans "F" thru "J" are located as shown on the General Plan. (See Sheet 4)

PLAN
SPANS "A", "B", "C", "D" AND HALF SPAN "E" (SHOWN)
HALF SPAN "E", SPANS "F", "G", "H" AND "J" (ROTATE 180°)

Scale: ⅛"=1'-0"

NOTES
See Sheet 9 for Section "B-B".
See Sheet 14 for Section "A-A" and Additional Notes.
See Sheet 15 for Section "E-E" and Sequence of Pours.
See Sheet 16 for Concrete Dead Load Deflection Diagram and Screed Plan.
See Sheet 17 for Screed Elevations.
See Sheet 18 for Bar Bending Details and Bill of Materials.

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INDIANA
DEPARTMENT OF TRANSPORTATION

FLOOR DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	13 OF 20
CONTRACT	PROJECT
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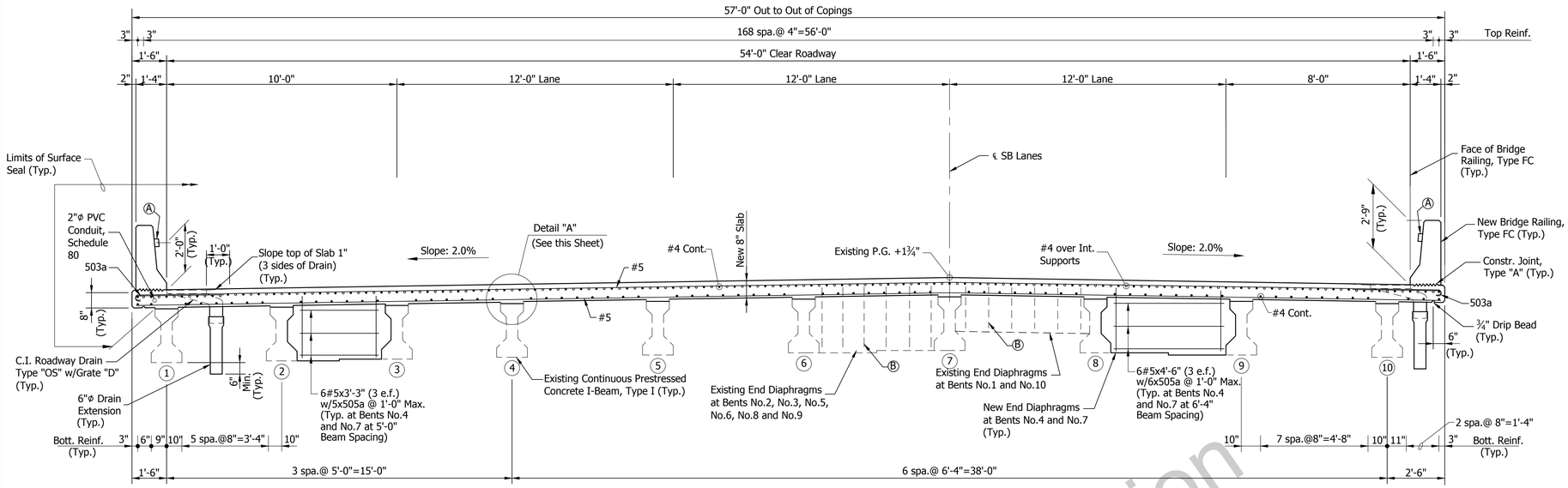


SECTION "A-A"
SOUTHBOUND STRUCTURE

Scale: ⅜"=1'-0"

Ⓐ Barrier Delineators @ 40'-0" Max. Spacing

Ⓑ Clean and Straighten Existing Diaphragm Reinforcing as required extending into New Bridge Deck (Typ.)

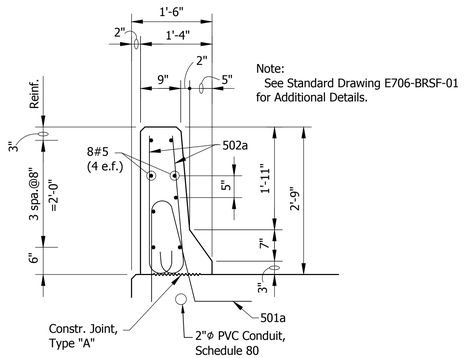
FLOOR NOTES

Concrete forms shall not be blocked against the end of beams in making any pours adjacent to the beam spans.

Suitable restraint shall be provided to prevent the rotation of the outside beams from construction loads such as finishing machines, forms, etc.

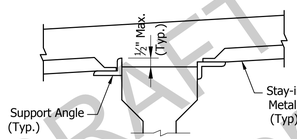
The top reinforcing in the slab shall be securely tied down to the slab forms and/or the beams to prevent lifting during concrete placement.

The Contractor shall have the option of using permanent metal deck forms in lieu of removable deck forms.



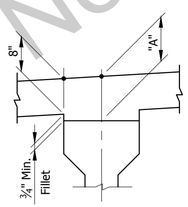
TYPICAL SECTION THRU
BRIDGE RAILING TYPE FC

Scale: ¾"=1'-0"



FILLET TREATMENT FOR
PRESTRESSED CONCRETE BEAM

Not to Scale



DETAIL "A"

Not to Scale

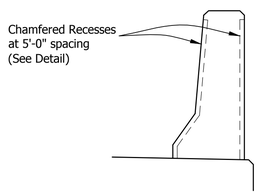
BEAM NO.	"A" INCHES
①	8⅜"
②	9⅛"
③	9⅜"
④	9⅞"
⑤	10⅛"
⑥	10⅜"
⑦	10¾"
⑧	10⅜"
⑨	10⅛"
⑩	9⅞"

SAW CUT ALTERNATE

FORMED ALTERNATE

RECESS DETAILS

Not to Scale



TYPICAL RAIL SECTION

Not to Scale

NOTES

See Sheet 16 for Concrete Dead Load Deflection Diagram and Screed Plan.
See Sheet 17 for Screed Elevations.
See Sheet 18 for Bar Bending Details and Bill of Materials.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER 10/31/16 DATE

DESIGNED: C. OBRIEN DRAWN: D. SHEETZ

CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

FLOOR DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	14 OF 20
CONTRACT	PROJECT
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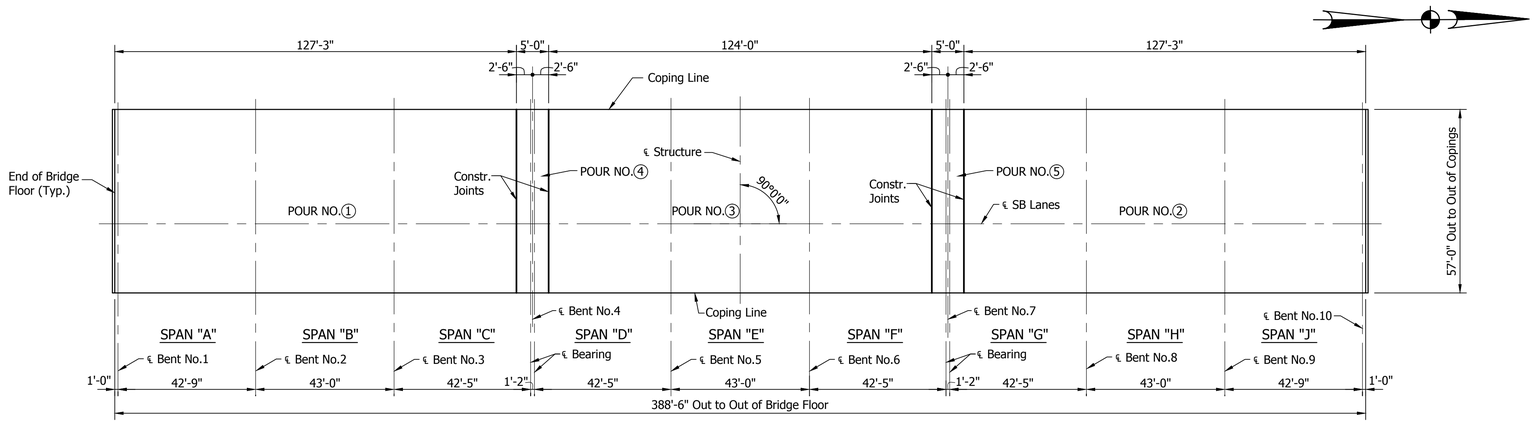
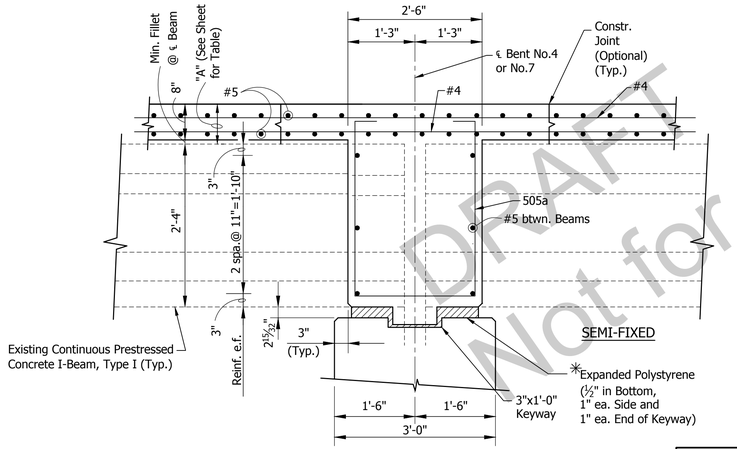


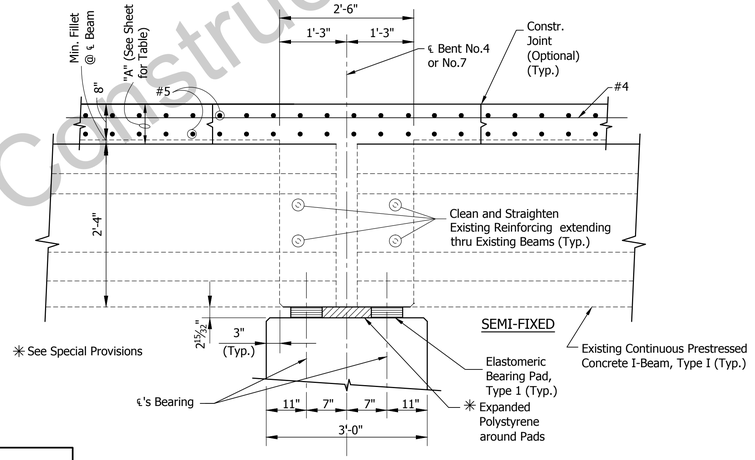
Note: Pour Numbers indicate Sequence of Pours, Pours over Interior Supports shall be made last to reduce the effect of the Slab Dead Load in the Negative Moment Area. Pours No.4 and No.5 will include the Diaphragm at the Support and shall be held to a 5'-0" Lengths.

SEQUENCE OF POURS DIAGRAM

Not to Scale



SECTION "E-E" BETWEEN BEAMS



SECTION "E-E" AT BEAMS

Note:
Vertical Dimensions are at ℄ of Pier at ℄ of each Beam and do not include Beam Camber.

BENTS NO. 4 AND NO.7

Not to Scale

NOTE

See Sheet 18 for Bar Bending Details and Bill of Materials.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

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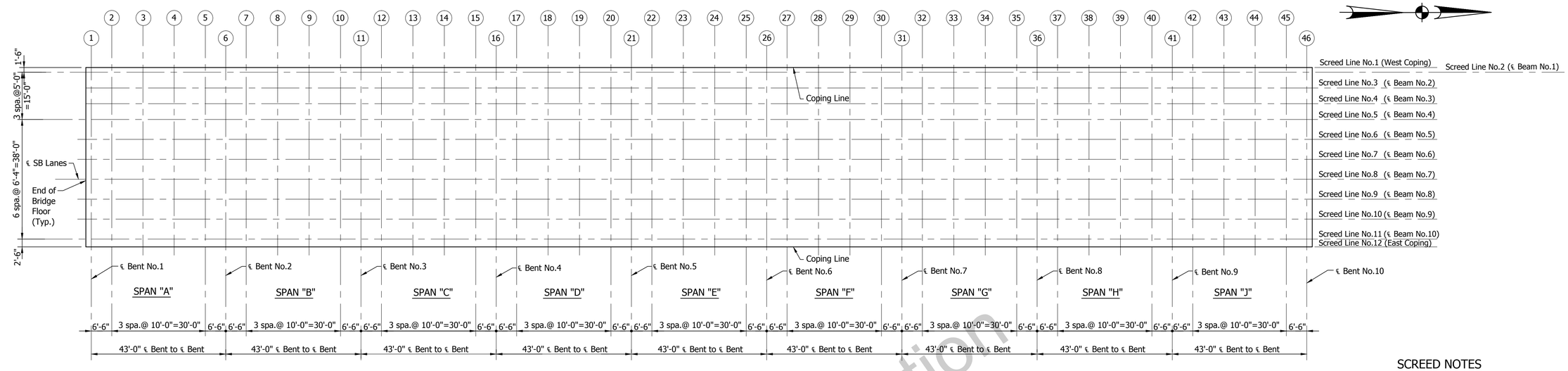
INDIANA
DEPARTMENT OF TRANSPORTATION

FLOOR DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	15 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605



SCREED PLAN

Scale: 1/16"=1'-0"

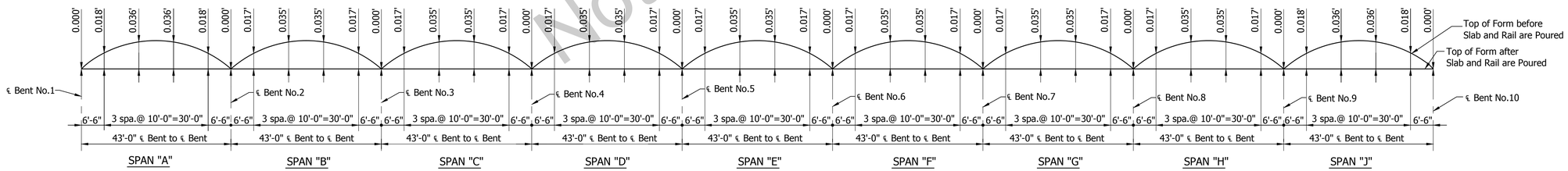
SCREED NOTES

Screed elevations will be given for setting screeds and coping forms so that the slab and copings will be at the required elevations after all the concrete has been poured.

Take elevations at the screed and coping points on top of adjacent beams, subtract these elevations from the given elevations and use resulting dimensions as the height for setting the screed or coping forms above that point. This dimension remains unchanged regardless of how much or what order the concrete is poured.

No concrete shall be poured until the above operation is completed.

Do not set screeds or coping forms by leveling.



AVERAGE CONCRETE DEAD LOAD DEFLECTION DIAGRAM

Scale: 1/16"=1'-0"

NOTE
See Sheet 17 for Screed Tables.

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

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CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

FLOOR DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	16 OF 20
CONTRACT	PROJECT
B-33539	0200633

BRS NO. 5605

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		Point:	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23
SCREED LINE	1	Top of Coping Form	381.620	381.640	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655
		Top of Exterior Beam																							
		Top of Beam to Top of Coping																							
	2	Top of Screed	381.650	381.670	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685
		Top of Beam																							
		Top of Beam to Top of Screed																							
	3	Top of Screed	381.750	381.770	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785
		Top of Beam																							
		Top of Beam to Top of Screed																							
	4	Top of Screed	381.850	381.870	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885
		Top of Beam																							
		Top of Beam to Top of Screed																							
	5	Top of Screed	381.950	381.970	381.985	381.985	381.970	381.950	381.965	381.985	381.985	381.965	381.950	381.965	381.985	381.985	381.965	381.950	381.965	381.985	381.985	381.965	381.950	381.965	381.985
		Top of Beam																							
		Top of Beam to Top of Screed																							
	6	Top of Screed	382.075	382.095	382.115	382.115	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110
		Top of Beam																							
		Top of Beam to Top of Screed																							
	7	Top of Screed	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240
		Top of Beam																							
		Top of Beam to Top of Screed																							
	8	Top of Screed	382.330	382.350	382.365	382.365	382.350	382.330	382.345	382.365	382.365	382.345	382.330	382.350	382.365	382.365	382.350	382.330	382.345	382.365	382.365	382.350	382.330	382.350	382.365
		Top of Beam																							
		Top of Beam to Top of Screed																							
	9	Top of Screed	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240
		Top of Beam																							
		Top of Beam to Top of Screed																							
	10	Top of Screed	382.075	382.095	382.115	382.115	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110
		Top of Beam																							
		Top of Beam to Top of Screed																							
	11	Top of Screed	381.950	381.970	381.985	381.985	381.970	381.950	381.965	381.985	381.985	381.970	381.950	381.970	381.985	381.985	381.970	381.950	381.970	381.985	381.985	381.970	381.950	381.970	381.985
		Top of Beam																							
		Top of Beam to Top of Screed																							
	12	Top of Coping Form	381.900	381.920	381.935	381.935	381.920	381.900	381.915	381.935	381.935	381.915	381.900	381.920	381.935	381.935	381.920	381.900	381.915	381.935	381.935	381.920	381.900	381.920	381.935
		Top of Exterior Beam																							
		Top of Beam to Top of Coping																							

		Point:	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46
SCREED LINE	1	Top of Coping Form	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.635	381.620	381.635	381.655	381.655	381.640	381.620
		Top of Exterior Beam																							
		Top of Beam to Top of Coping																							
	2	Top of Screed	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.665	381.650	381.665	381.685	381.685	381.670	381.650
		Top of Beam																							
		Top of Beam to Top of Screed																							
	3	Top of Screed	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.765	381.750	381.765	381.785	381.785	381.770	381.750
		Top of Beam																							
		Top of Beam to Top of Screed																							
	4	Top of Screed	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.865	381.850	381.865	381.885	381.885	381.870	381.850
		Top of Beam																							
		Top of Beam to Top of Screed																							
	5	Top of Screed	381.985	381.965	381.950	381.965	381.985	381.985	381.965	381.950	381.965	381.985	381.985	381.965	381.950	381.965	381.985	381.985	381.965	381.950	381.970	381.985	381.985	381.970	381.950
		Top of Beam																							
		Top of Beam to Top of Screed																							
	6	Top of Screed	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.115	382.115	382.095	382.075
		Top of Beam																							
		Top of Beam to Top of Screed																							
	7	Top of Screed	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.24	382.220	382.205
		Top of Beam																							
		Top of Beam to Top of Screed																							
	8	Top of Screed	382.365	382.350	382.330	382.350	382.365	382.365	382.345	382.330	382.350	382.365	382.365	382.350	382.330	382.345	382.365	382.365	382.345	382.330	382.350	382.365	382.365	382.350	382.330
		Top of Beam																							
		Top of Beam to Top of Screed																							
	9	Top of Screed	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.240	382.220	382.205	382.220	382.240	382.24	382.220	382.205
		Top of Beam																							
		Top of Beam to Top of Screed																							
	10	Top of Screed	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.110	382.110	382.095	382.075	382.095	382.115	382.115	382.095	382.075
		Top of Beam																							
		Top of Beam to Top of Screed																							
	11	Top of Screed	381.985	381.970	381.950	381.970	381.985	381.985	381.970	381.950	381.970	381.985	381.985	381.970	381.950	381.970	381.985	381.985	381.965	381.950	381.970	381.985	381.985	381.970	381.950
		Top of Beam																							
		Top of Beam to Top of Screed																							
	12	Top of Coping Form	381.935	381.920	381.900	381.920	381.935	381.935	381.915	381.900	381.920	381.935	381.935	381.920	381.900	381.915	381.935	381.935	381.915	381.900	381.920	381.935	381.935	381.920	381.900
		Top of Exterior Beam																							
		Top of Beam to Top of Coping																							

MICHAEL MATEL REGISTERED No. 860186 STATE OF INDIANA PROFESSIONAL ENGINEER

RECOMMENDED FOR APPROVAL: DESIGN ENGINEER 10/31/16 DATE

DESIGNED: C. OBRIEN DRAWN: D. SHEETZ

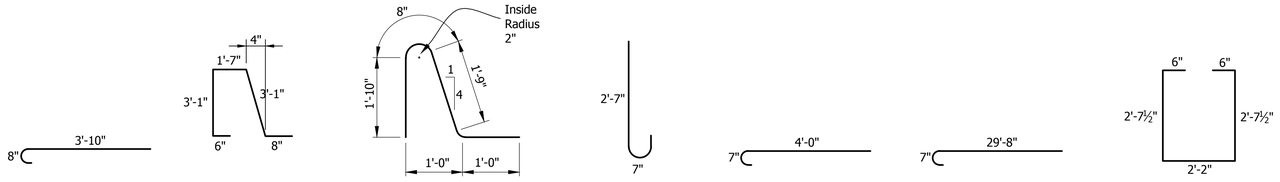
CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

FLOOR DETAILS
SOUTHBOUND STRUCTURE

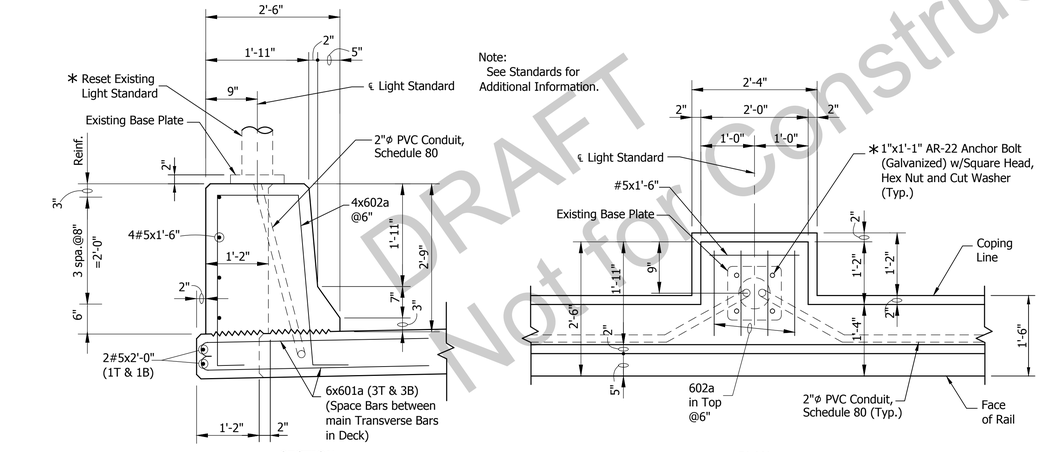
HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	17 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605



BAR BENDING DETAILS

Not to Scale



DETAILS AT LIGHT STANDARD

Not to Scale

BILL OF MATERIALS
SUPERSTRUCTURE
SPANS "A" THRU "J"
SOUTHBOUND STRUCTURE

REINFORCING BARS

Mark or Size	No. of Bars	Length (Ft.)	Weight (Lbs.)
601a	12	4'-6"	
602a	8	8'-11"	
Total #6 (Epoxy Coated)			188
501a	1168	5'-6"	
502a	2336	3'-2"	
503a	1168	4'-7"	
504a	1168	30'-3"	
505a	102	8'-5"	
#5	32	25'-9"	
#5	584	22'-0"	
#5	1488	20'-0"	
#5	72	4'-6"	
#5	36	3'-3"	
#5	4	2'-0"	
#5	8	1'-6"	
Total #5 (Epoxy Coated)			103526
#4	680	22'-0"	
#4	3140	20'-0"	
#4	314	17'-0"	
Total #4 (Epoxy Coated)			55509
⊕ Total Steel (Epoxy Coated)			159223

CONCRETE

Class "C" in Superstructure	
Pour No.1	203.7 Cys.
Pour No.2	203.7 Cys.
Pour No.3	181.7 Cys.
Pour No.4	17.9 Cys.
Pour No.5	17.9 Cys.
Total Class "C" in Superstructure	624.9 Cys.
Class "C" in Railing	74.7 Cys.

MISCELLANEOUS

Barrier Delineators	22 Each
Threaded Tie Bar Assemblies (#5x3'-0" each way) (Epoxy Coated)	58 Each
Surface Seal	28440 Sft.
Cast Iron Roadway Drains Type "OS-D"	18 Each
6"ϕ Drain Pipe Casting Extension	18 Each
⊗ 2"ϕ PVC Conduit, Schedule 80	515 Lft.

⊕ A.S.T.M. A615, Grade 60

⊗ Includes Approach Slabs

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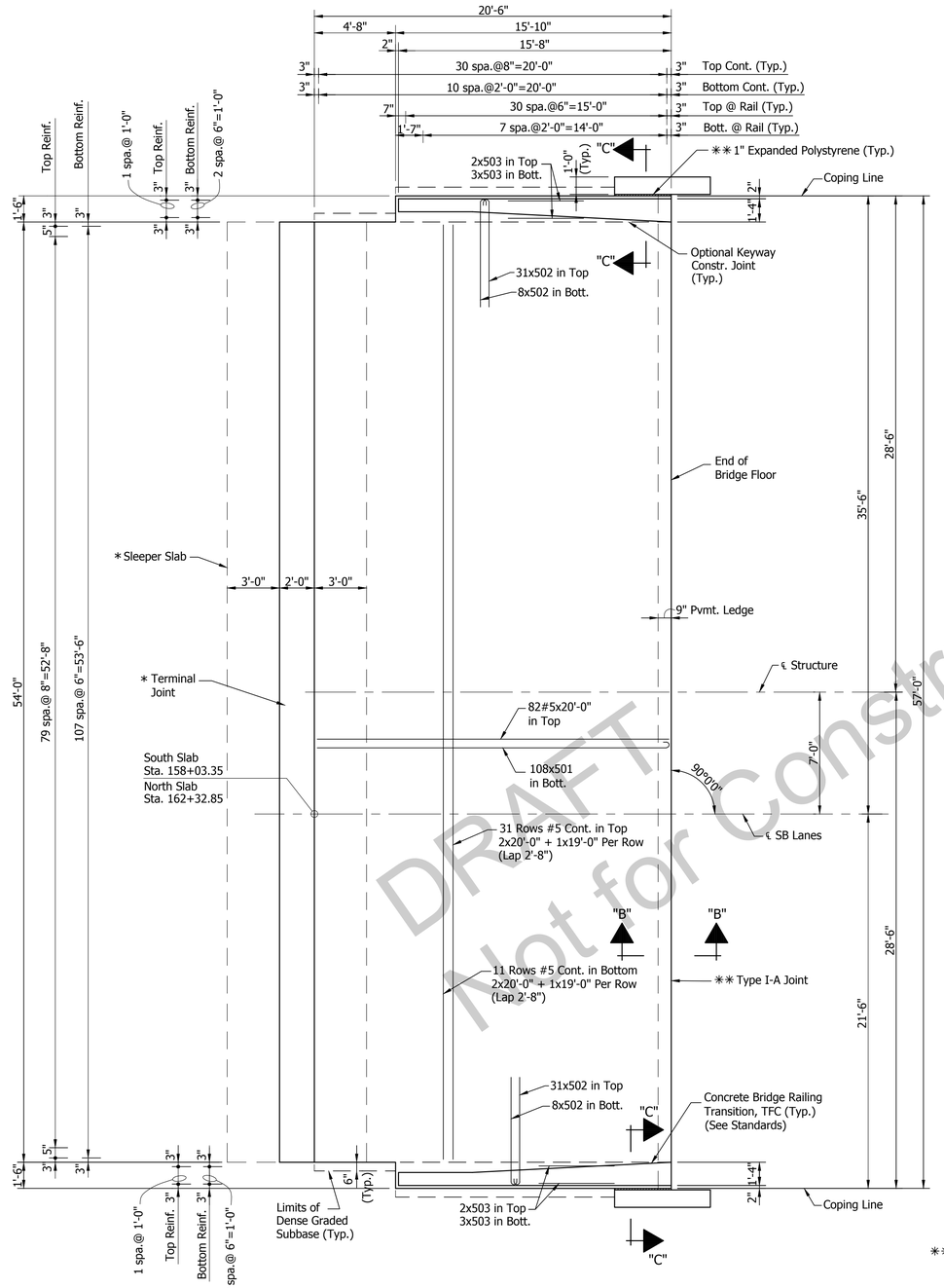
CHECKED: B. WRIGHT CHECKED: M. MATEL

INDIANA DEPARTMENT OF TRANSPORTATION

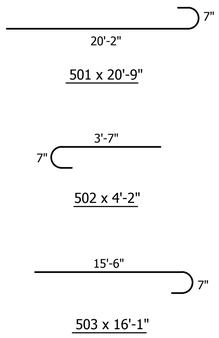
FLOOR DETAILS SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET 18 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605



PLAN
SOUTH APPROACH SLAB (SHOWN)
NORTH APPROACH SLAB (OPP. HAND)
Scale: ¼"=1'-0"



BAR BENDING DETAILS
Not to Scale

BILL OF MATERIALS
SOUTH APPR. SLAB
NORTH APPR. SLAB
(SAME UNLESS NOTED)
SOUTHBOUND STRUCTURE

REINFORCING BARS

Mark or Size	No. of Bars	Length (Ft.)	Weight (Lbs.)
501	108	20'-9"	
502	78	4'-2"	
503	10	16'-1"	
#5	166	20'-0"	
#5	42	19'-0"	
⊕ Total Steel (Epoxy Coated)			7139

CONCRETE

Reinforced Concrete Bridge Approach (12")	129 Sys.

MISCELLANEOUS

Dense Graded Subbase	21 Cys.
Concrete Bridge Railing	
Transition, TFC	2 Each
⌀ Surface Seal	1130 Sft.
* Terminal Joint	54 Lft.

⊕ A.S.T.M. A615, Grade 60
⌀ Does Not Include Bridge Railing Transition
* North Approach Only (See Note)

*Note: Sleeper Slab and Terminal Joint required at North Approach Slab only if Concrete Pavement Option is used.

NOTES
See Sheet 8 for Section "C-C".
See Sheet 9 for Section "B-B".

** See Special Provisions

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DESIGNED: D. SHEETZ DRAWN: D. SHEETZ
CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

APPROACH SLAB DETAILS
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	041-82-0877E
VERTICAL SCALE	DESIGNATION
AS NOTED	0200633
SURVEY BOOK	SHEET
	19 OF 20
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BFS NO. 5605

STRUCTURE QUANTITIES

ITEM	CONCRETE				DENSE GRADED SUBBASE	REINF. CONC. BRIDGE APPR. 12"	CONC. RAILING, FC	REINF. BARS (PLAIN)	REINF. BARS (EPOXY COATED)	EST. WEIGHT STR. STEEL	2"ϕ PVC CONDUIT, SCHEDULE 80	FIELD DRILLED HOLES IN CONCRETE	CAST IRON DRAIN TYPE "OS-D"	6"ϕ DRAIN PIPE CASTING EXTENSION	EST. AREA SURFACE SEAL	CONCRETE BRIDGE RAILING TRANSITION TFC	6"ϕ END BENT DRAIN PIPE	AGGREGATE FOR END BENT BACKFILL	GEOTEXTILE	CORED HOLE IN CONCRETE	THREADED TIE BAR ASSEMBLIES (EPOXY COATED)	BARRIER DELINEATORS	*TERMINAL JOINT
	CLASS C IN SUPERSTR.	CLASS C IN SUBSTR.	CLASS B IN FOOTING	CLASS A IN SUBSTR.																			
	CYS.	CYS.	CYS.	CYS.	CYS.	SYS.	CYS.	LBS.	LBS.	LBS.	LFT.	EACH	EACH	EACH	SFT.	EACH	LFT.	CYS.	SYS.	EACH	EACH	EACH	LFT.
SUPERSTRUCTURE																							
Spans "A" thru "J"	624.9						74.7		159223		515		18	18	28440						58	22	
SUBSTRUCTURE																							
Bent No.1									2959			56					72	15	55	1			
Bent No.4				27.2				3671															
Bent No.7				27.2				3671															
Bent No.10									2959			56					72	15	55	1			
APPROACH SLABS																							
South					21	129			7139						1130	2							
North					21	129			7139						1130	2							54
BARRIER RAIL TRANSITIONS																							
South									1102														
North									1102														
TOTALS	624.9			54.4	42	258		⊕7342	⊕181623		515	112	18	18	30700	4	144	30	110	2	58	22	54

⊕ A.S.T.M. A615, Grade 60

*Note: Sleeper Slab and Terminal Joint required at North Approach Slab only if Concrete Pavement Option is used.

RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER 10/31/16 DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. MATEL CHECKED: M. MATEL

INDIANA
DEPARTMENT OF TRANSPORTATION

BRIDGE SUMMARY
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
NONE	041-82-0877E
VERTICAL SCALE	DESIGNATION
NONE	0200633
SURVEY BOOK	SHEET
	20 OF 20
CONTRACT	PROJECT
B-33539	0200633

BFS NO. 5605