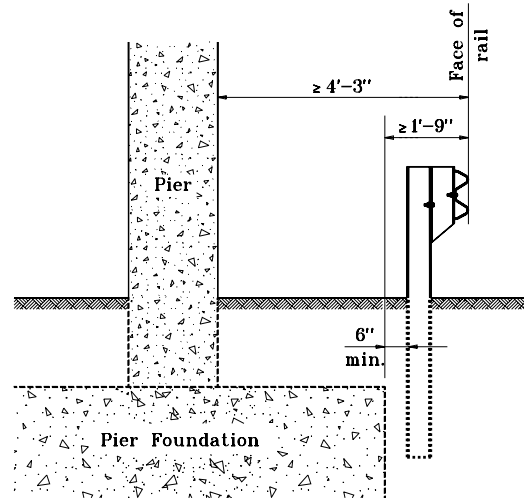


**GENERAL NOTES**

1. This drawing shall be used where W-beam guardrail is specified along the median side shoulder of a multi-lane divided roadway to shield the piers of tandem overhead structures.
2. The details on this drawing are typical guardrail layouts for shielding the piers of tandem overhead structures. The required length of guardrail shall be determined in accordance with Tables MPPG-1 and MPPG-2, and shall be as shown on the plans.
3. Dimensions and details not shown on this drawing shall be as shown on the plans.

<b>TABLE MPPG-1</b>			
Outside (Right Side) Shoulder Pier Approach Guardrail Lengths at Piers/Frame Bents $\leq 16'$ from Pavement Edge			
Design speed mph	Pier End Inside Clear Zone	Pier End Outside Clear Zone	Guardrail Pay Length (ft)
	Required Minimum Length (ft) in Advance of the Pier/Frame Bent End		
$\geq 50$	>of the rounded calculated length of need or 100	100	Required minimum length
$< 50$	>of the rounded calculated length of need or 50	50	minus 25' for GP transition

<b>TABLE MPPG-2</b>			
Outside (Right Side) Shoulder Pier Approach Guardrail Lengths at Piers/Frame Bents $> 16'$ from Pavement Edge			
Design speed mph	Pier End Inside Clear Zone	Pier End Outside Clear Zone	Guardrail pay length (ft)
	Required Minimum Length (ft) in Advance of the Pier/Frame Bent End		
$\geq 50$	>of the rounded calculated length of need or 30	100	Required minimum length
$< 50$	>of the rounded calculated length of need or 15	50	plus length to shield pier face



**Guardrail to Pier Clearance**

INDIANA DEPARTMENT OF TRANSPORTATION	
<b>MEDIAN PIER PROTECTION GUARDRAIL</b>	
SEPTEMBER 1997	
<b>STANDARD DRAWING NO.E 601-MPPG-02</b>	
	DETAILS PLACED IN THIS FORMAT 11-15-99 /s/ Anthony L. Uremovich 11-15-99 DESIGN STANDARDS ENGINEER DATE
DESIGN STANDARDS ENGINEER	/s/ Firooz Zandi 11-15-99 CHIEF HIGHWAY ENGINEER DATE ORIGINALLY APPROVED 9-01-97